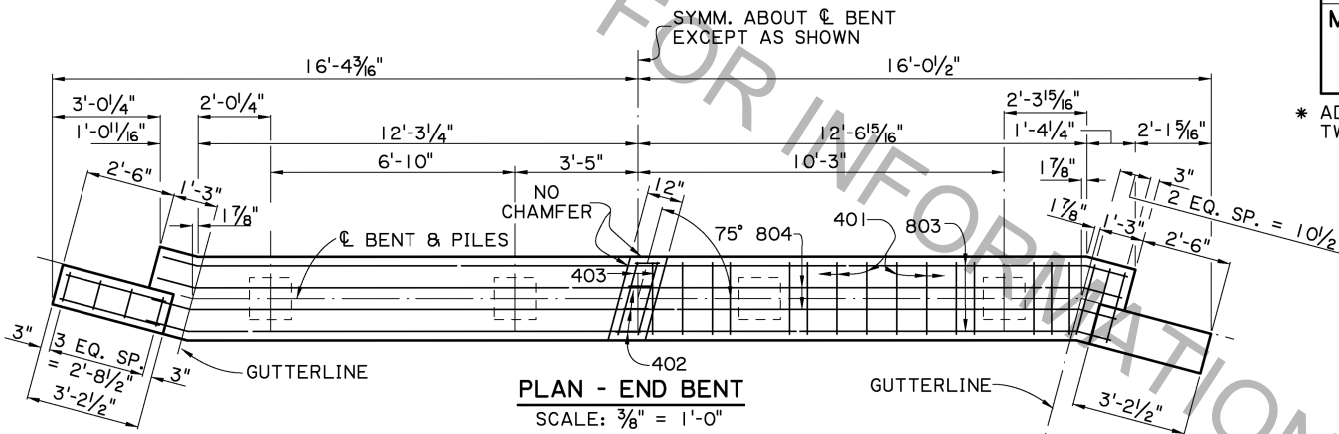


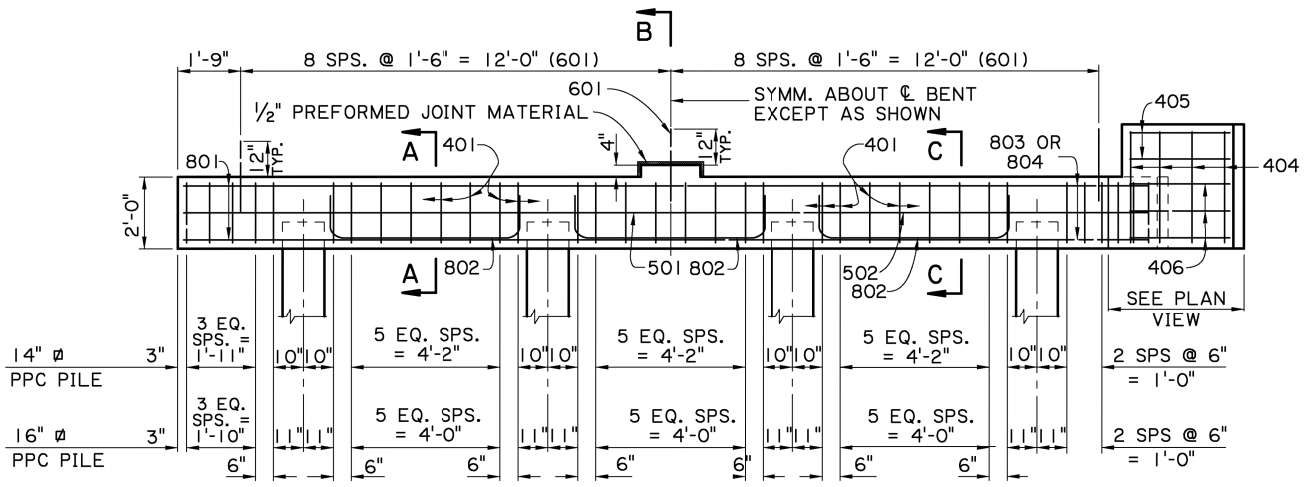
PLAN - INTERMEDIATE BENT

SCALE 3/8" = 1'-0"



PLAN - END BENT

SCALE: 3/8" = 1'-0"

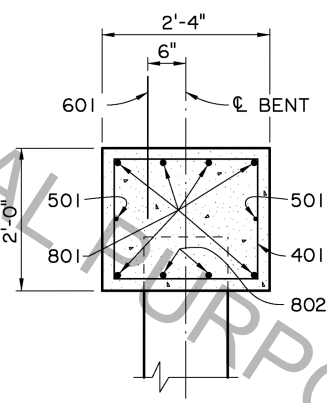
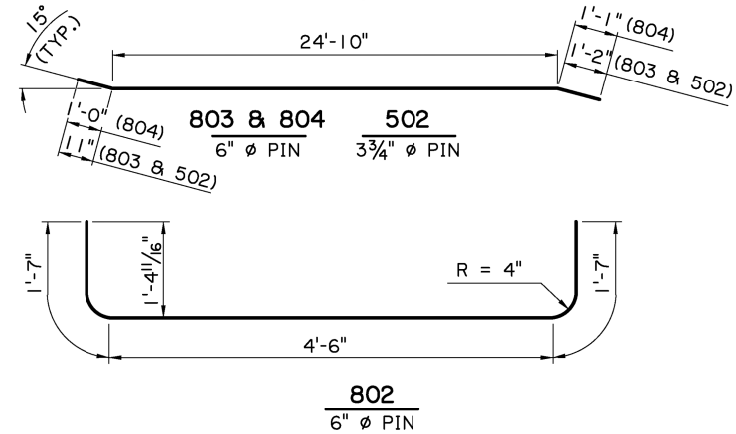
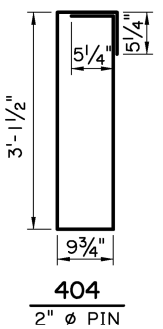
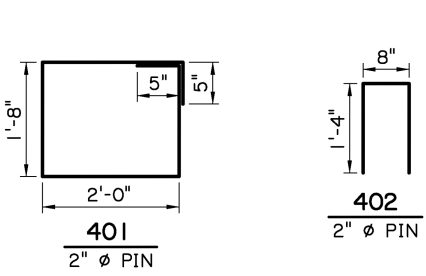


HALF ELEVATION - INTERMEDIATE BENT

SCALE 3/8" = 1'-0"

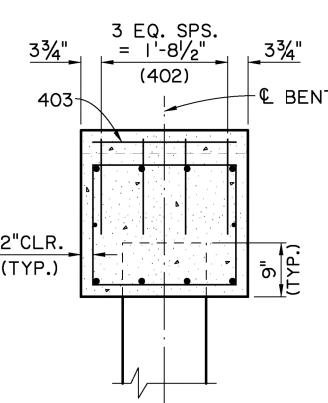
HALF ELEVATION - END BENT

SCALE 3/8" = 1'-0"



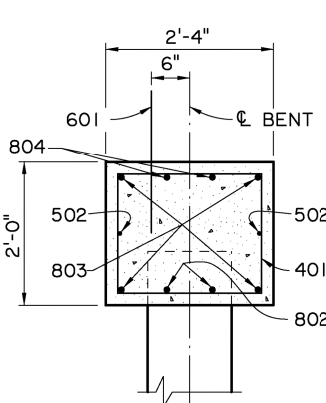
SECTION A-A

SCALE: 3/4" = 1'-0"



SECTION B-B

SCALE: 3/4" = 1'-0"



SECTION C-C

SCALE: 3/4" = 1'-0"

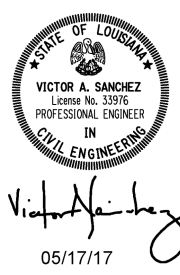
ESTIMATED QUANTITIES (ONE INTER. BENT)				
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
801	6	27'-2"	163'-0"	LONGIT. IN CAP
802	6	7'-8"	46'-0"	LONGIT. IN CAP
TOTAL NO. 8 BARS = 209'-0" = 558 LBS.				
601	17	2'-0"	34'-0"	DOWELS
TOTAL NO. 6 BARS = 34'-0" = 51 LBS.				
501	2	27'-2"	54'-4"	LONGIT. IN CAP
TOTAL NO. 5 BARS = 54'-4" = 57 LBS.				
401	34	8'-2"	277'-8"	STIRRUPS IN CAP
402	4	3'-4"	13'-4"	STIRRUPS IN RISER
403	2	2'-1"	4'-2"	LONGIT. IN RISER
TOTAL NO. 4 BARS = 295'-2" = 197 LBS.				
* TOTAL DEFORMED REINFORCING STEEL = 863 LBS.				
* CLASS A1 CONCRETE = 4.59 CU. YDS.				
MAX. PILE LOAD: SERVICE DEAD LOAD = 18 TONS				
SERVICE LIVE LOAD = 30 TONS				
FACTORED TOTAL LOAD = 67 TONS				

\* ADD 51 LBS. OF REINFORCING STEEL (17-601 DOWELS) WHEN TWO FIXED ENDS OCCUR ON THE SAME BENT.

ESTIMATED QUANTITIES (ONE END BENT)				
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
802	6	7'-8"	46'-0"	LONGIT. IN CAP
803	4	26'-11"	107'-8"	LONGIT. IN CAP
804	2	26'-11"	53'-10"	LONGIT. IN CAP
TOTAL NO. 8 BARS = 207'-6" = 554 LBS.				
601	17	2'-0"	34'-0"	DOWELS
TOTAL NO. 6 BARS = 34'-0" = 51 LBS.				
502	2	26'-11"	53'-10"	LONGIT. IN CAP
TOTAL NO. 5 BARS = 53'-10" = 56 LBS.				
401	36	8'-2"	294'-0"	STIRRUPS IN CAP
402	4	3'-4"	13'-4"	STIRRUPS IN RISER
403	2	2'-1"	4'-2"	LONGIT. IN RISER
404	8	8'-9"	70'-0"	STIRRUPS IN WINGWALL
405	8	2'-10"	22'-8"	LONGIT. IN WINGWALL
406	12	4'-0"	48'-0"	LONGIT. IN WINGWALL
TOTAL NO. 4 BARS = 452'-2" = 302 LBS.				
TOTAL DEFORMED REINFORCING STEEL = 963 LBS.				
* CLASS A1 CONCRETE = 5.38 CU. YDS.				
MAX. PILE LOAD: SERVICE DEAD LOAD = 18 TONS				
SERVICE LIVE LOAD = 30 TONS				
FACTORED TOTAL LOAD = 67 TONS				

\* ADD 51 LBS. OF REINFORCING STEEL (17-601 DOWELS) WHEN TWO FIXED ENDS OCCUR ON THE SAME BENT.

16" # PPC PILES USED FOR ESTIMATING PURPOSES ONLY. (ADD 0.04 CU. YDS. OF CLASS A1 CONCRETE PER BENT WHEN 14" # PPC PILES ARE USED.)



NOTES:

CONSTRUCTION SPECIFICATIONS: LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION, WITH 2008 & 2009 INTERIMS.

DESIGN LOAD: LIVE LOAD IS HL-93, AND LADV-11 (LOUISIANA DESIGN VEHICLE LIVE LOAD 2011).

STRUCTURAL CONCRETE: ALL CONCRETE SHALL BE CLASS A1. EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER UNLESS OTHERWISE NOTED. ALL EXPOSED FACES OF WINGWALLS AND ENDS OF CAPS SHALL RECEIVE A SURFACE FINISH AS PER SUBSECTION 805.08 OF THE STANDARD SPECIFICATIONS, EXCEPT WHEN SPECIFIED ELSEWHERE IN THE PLANS.

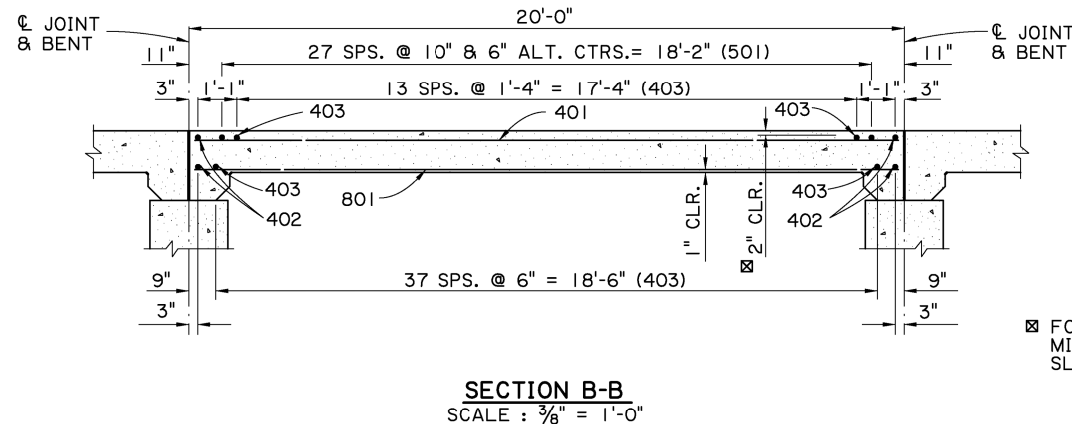
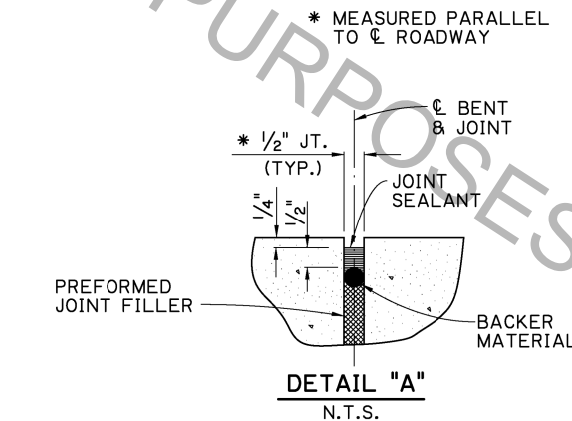
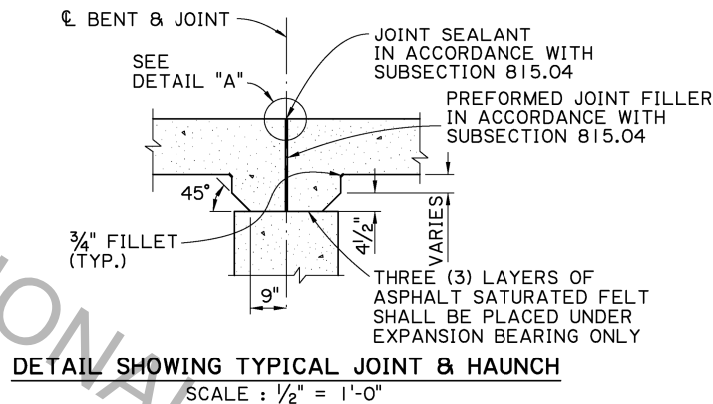
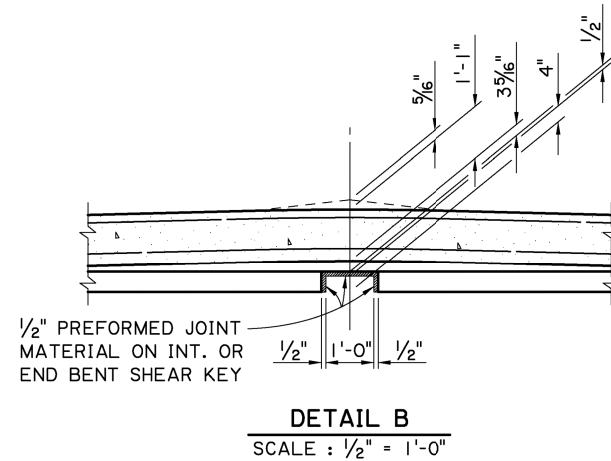
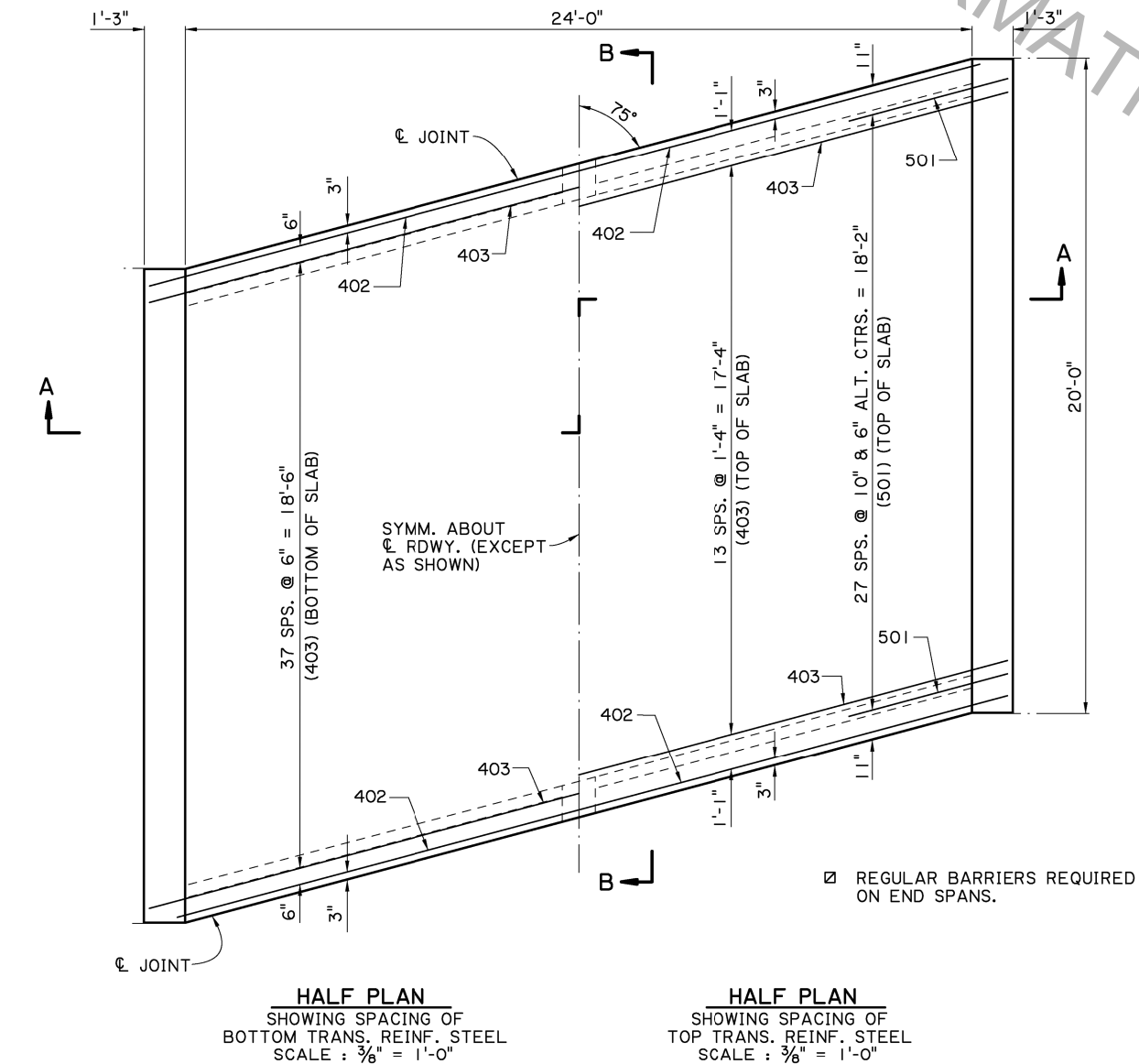
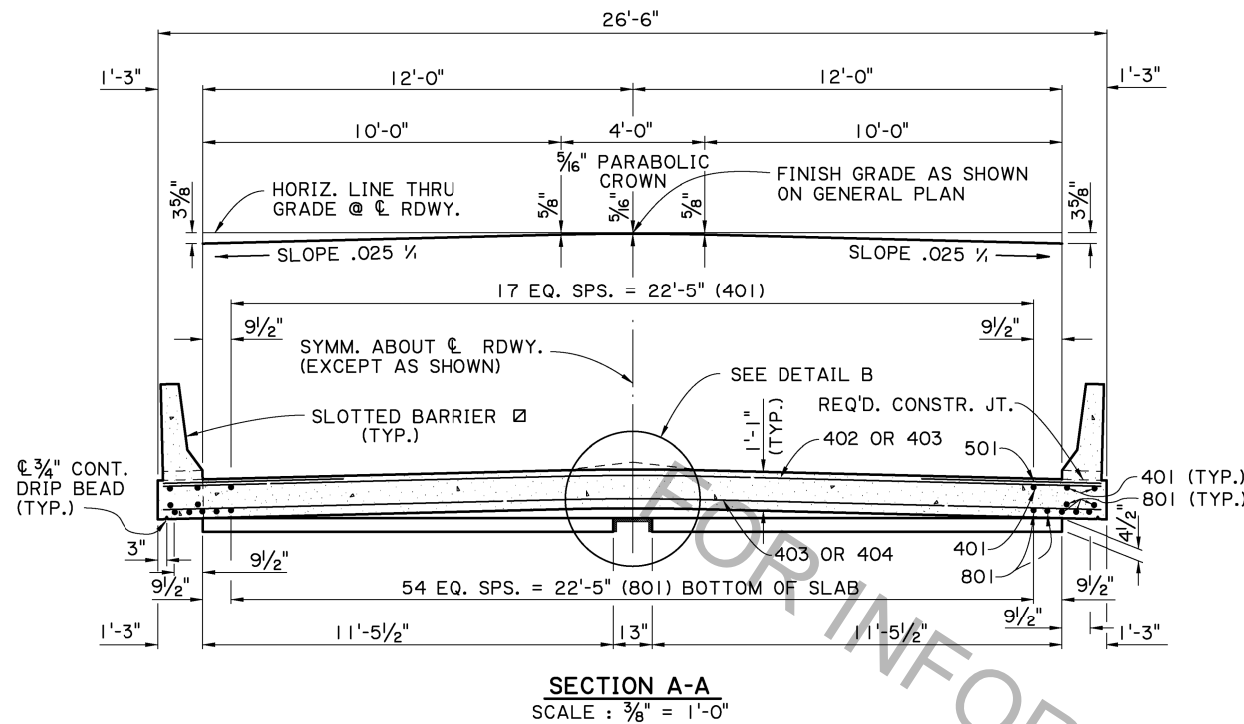
REINFORCING STEEL: ALL REINFORCING SHALL BE GRADE 60. DIMENSIONS RELATING TO FABRICATION ARE OUT TO OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE NOTED. DOWELS (601 BARS) SHALL BE PROVIDED AT ALL FIXED BEARINGS AND APPROACH SLAB BEARINGS (SEE GENERAL PLAN). ALL EXPOSED ENDS OF DOWELS SHALL BE WRAPPED WITH TWO LAYERS OF 15 LB. ASPHALT SATURATED FELT. CLOSE FITTING TUBES OF COMPRESSIBLE MATERIAL NOT LESS THAN 3/16" THICK MAY BE SUBSTITUTED.

PRECAST CONCRETE PILES: FOR DETAILS SEE STANDARD DETAIL BD.2.5.1.0.01 (CS-216). EXTERIOR PILES ARE TO BE BATTERED OUTWARD AT 1 1/2" ON 12 IN THE LONGITUDINAL DIRECTION OF THE BENT, WHEN NOTED ON THE GENERAL PLAN.

PREFORMED JOINT MATERIAL: PREFORMED JOINT MATERIAL SHALL BE IN ACCORDANCE WITH SECTION 815.04 OF THE STANDARD SPECIFICATIONS.

BASIS OF PAYMENT: ALL MATERIALS SHALL BE PAID FOR UNDER "BRIDGE SUPERSTRUCTURE AND SUBSTRUCTURE" ACCORDING TO THE SPECIFICATIONS.

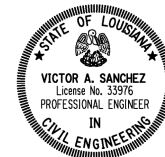
SHEET NUMBER	1	OF	1
DESIGNED	B. DELATTE	CHECKED	J. NAKHLEH
DETAILS	D. HYMEH	CHECKED	J. NAKHLEH
REVIEWED	05/17/17	SERIES #	1
DATE	05/17/17	BY	
REVISION OR CHANGE	ORDER DESCRIPTION	NO.	
BENTS			
REINFORCED CONCRETE PILE BENTS			
24'-0" CLEAR ROADWAY			
75° CROSSING TWO WAY TANGENT			
DOTD BRIDGE DESIGN			
PSS-75-24-20SL			



ESTIMATED QUANTITIES (ONE SPAN)				
BAR NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION	
801	67	19'-7"	1312'-1"	LONGIT. BOT. OF SLAB
TOTAL NO. 8 BARS = 1312'-1" = 3503 LBS.				
501	56	5'-0"	280'-0"	TRANS. TOP OF SLAB
TOTAL NO. 5 BARS = 280'-0" = 292 LBS.				
401	22	19'-7"	430'-10"	LONGIT. TOP OF SLAB
402	4	26'-2"	104'-8"	TRANS. TOP & BOT. OF SLAB
403	52	27'-1"	1408'-4"	TRANS. TOP & BOT. OF SLAB
TOTAL NO. 4 BARS = 1943'-10" = 1298 LBS.				
TOTAL DEFORMED REINFORCING STEEL = 5120 LBS.				
CLASS A1 CONCRETE = 22.08 CU. YDS.				
CONCRETE RAILING (BARRIER TYPE) = 40.00 LIN. FT.				

**SPAN NOTES:**  
CONSTRUCTION SPECIFICATIONS:  
LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.  
DESIGN SPECIFICATIONS:  
AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION, WITH 2008 & 2009 INTERIMS.  
DESIGN LOAD:  
THE BRIDGE DECK IS DESIGNED FOR A FUTURE WEARING COURSE OF 19 PSF. THE LIVE LOAD IS HL-93, AND LADV-11 (LOUISIANA DESIGN VEHICLE LIVE LOAD 2011).  
STRUCTURAL CONCRETE:  
ALL CONCRETE SHALL BE CLASS A1. EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER UNLESS OTHERWISE NOTED. ALL BARRIER RAIL SURFACES ARE TO RECEIVE A CLASS 3 SPECIAL FINISH.  
REINFORCING STEEL:  
ALL REINFORCING SHALL BE GRADE 60; DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, DIMENSIONS RELATING TO FABRICATION ARE OUT TO OUT OF BARS, UNLESS OTHERWISE NOTED. ALL REINFORCING BARS SHALL BE PLACED TO PROVIDE A MINIMUM COVER OF ONE INCH FROM THE SURFACE OF THE DRAIN HOLES TO THE FACE OF THE BARS.  
GUARD RAIL:  
REFER TO THE GENERAL PLAN FOR GUARD RAIL REQUIREMENTS. PROVIDE HOLES FOR GUARD RAIL CONNECTIONS ACCORDING TO STANDARD PLAN BD.1.1.1.0.01(IGR-200) ON ALL FOUR BRIDGE RAIL ENDS.  
BASIS OF PAYMENT:  
ALL MATERIAL SHALL BE PAID FOR UNDER "BRIDGE SUPERSTRUCTURE AND SUBSTRUCTURE" ACCORDING TO THE SPECIFICATIONS.

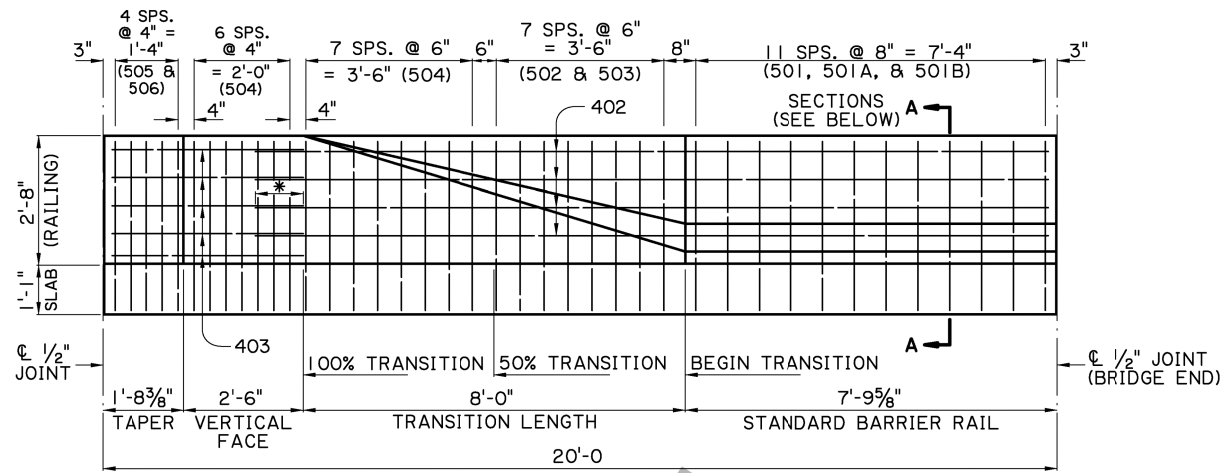
AS-DESIGNED RATING		
VEHICLE	RATING FACTOR	NOTES
HL-93 (INV)	1.451	—
HL-93 (OPR)	1.881	—
LADV-11 (INV)	1.116	MAGNIFICATION FACTOR = 1.3



Victor A. Sanchez  
05/17/17

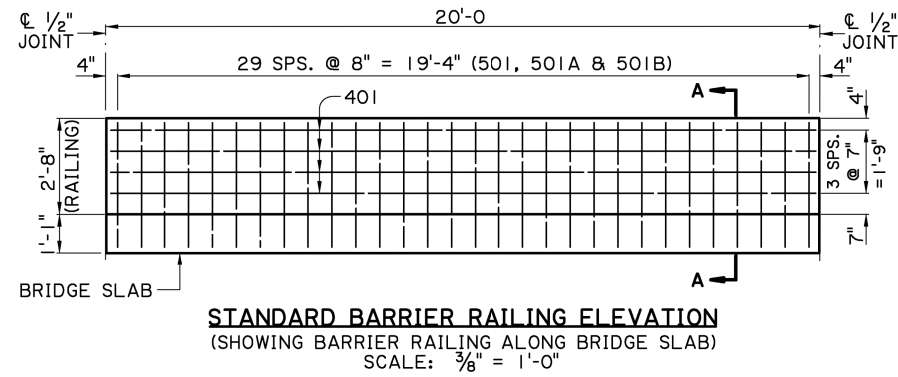
FOR BRIDGES IN DISTRICT 04 & 05, MINIMUM CONCRETE COVER IN TOP OF SLAB SHALL BE 2 1/2".



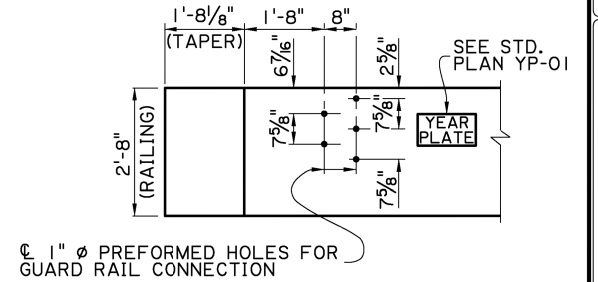


\* 1'-0" (MIN.)  
SPLICE

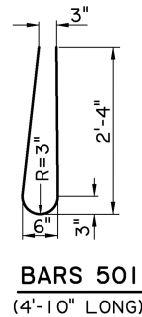
**BARRIER RAILING TRANSITION ELEVATION**  
(SHOWING BARRIER RAILING AT END OF BRIDGE)  
SCALE: 1/2" = 1'-0"



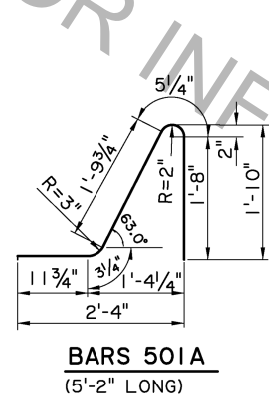
**STANDARD BARRIER RAILING ELEVATION**  
(SHOWING BARRIER RAILING ALONG BRIDGE SLAB)  
SCALE: 3/8" = 1'-0"



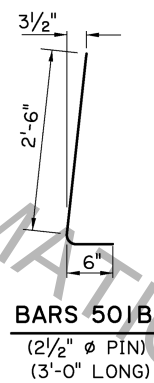
**GUARD RAIL CONNECTION DETAIL**  
(FOR GUARD RAIL DETAILS,  
SEE STANDARD PLAN BD.1.1.1.0.01(GR-200).  
SCALE: 1/2" = 1'-0"



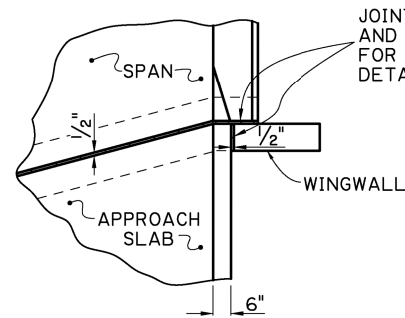
**BARS 501**  
(4'-10" LONG)



**BARS 501A**  
(5'-2" LONG)

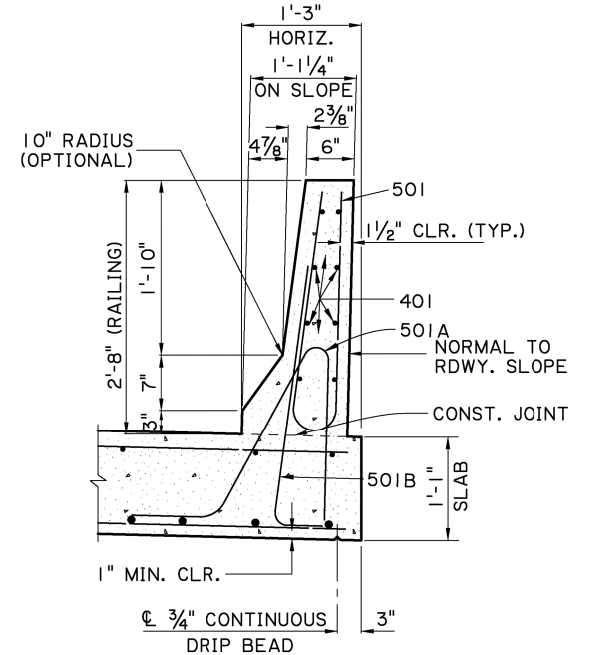


**BARS 501B**  
(2 1/2" Ø PIN)  
(3'-0" LONG)

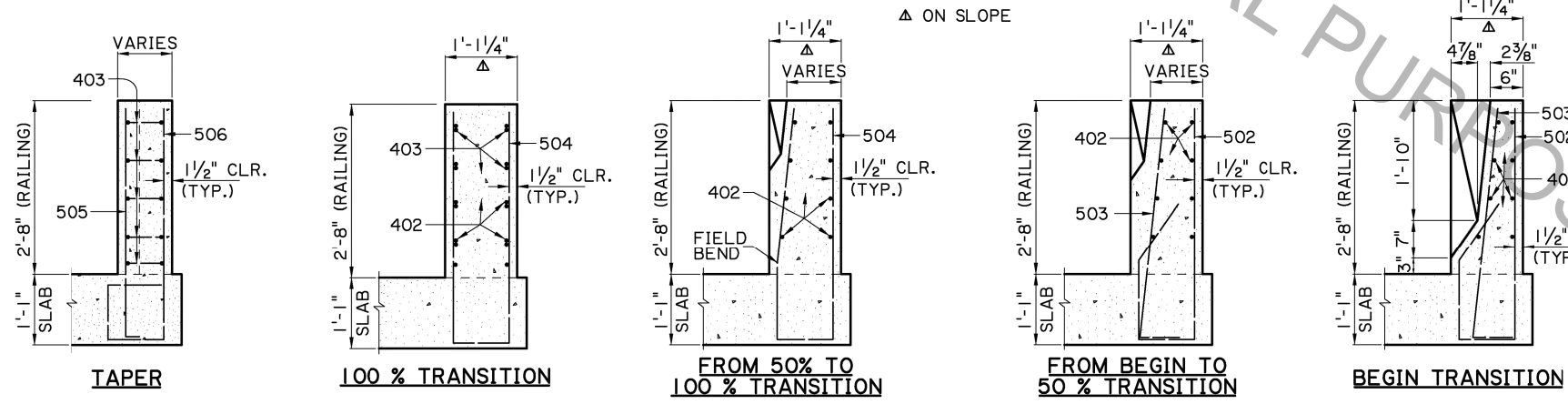
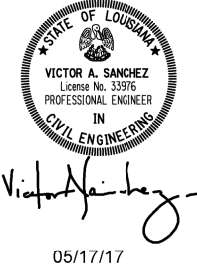


**JOINT DETAIL**  
SCALE: 3/8" = 1'-0"

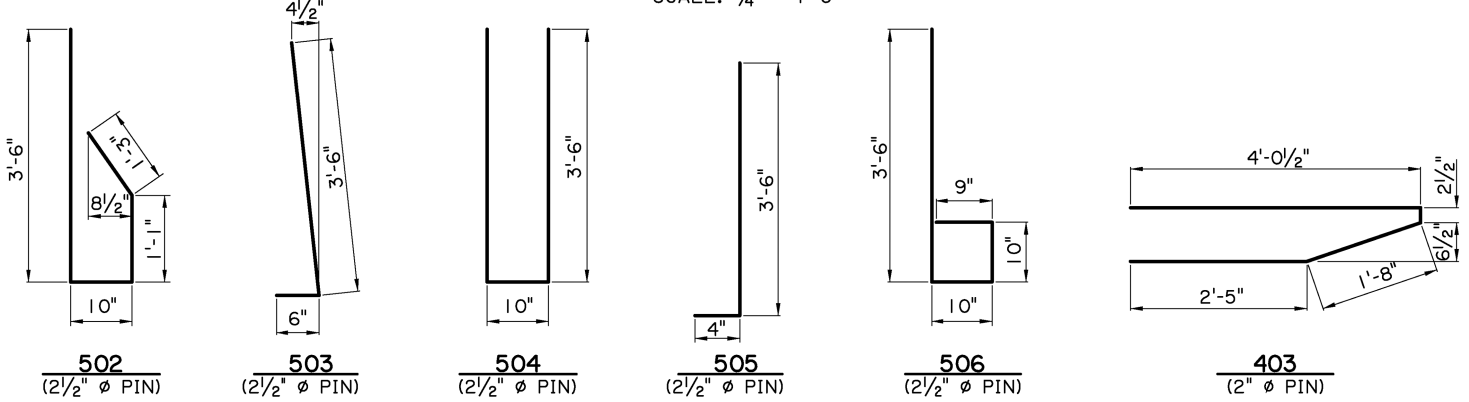
JOINT SEALANT, BACKER MATERIAL,  
AND PREFORMED JOINT FILLER.  
FOR DETAILS SEE SPAN SHEET,  
DETAIL "A".



**SECTION A-A**  
SCALE: 1" = 1'-0"

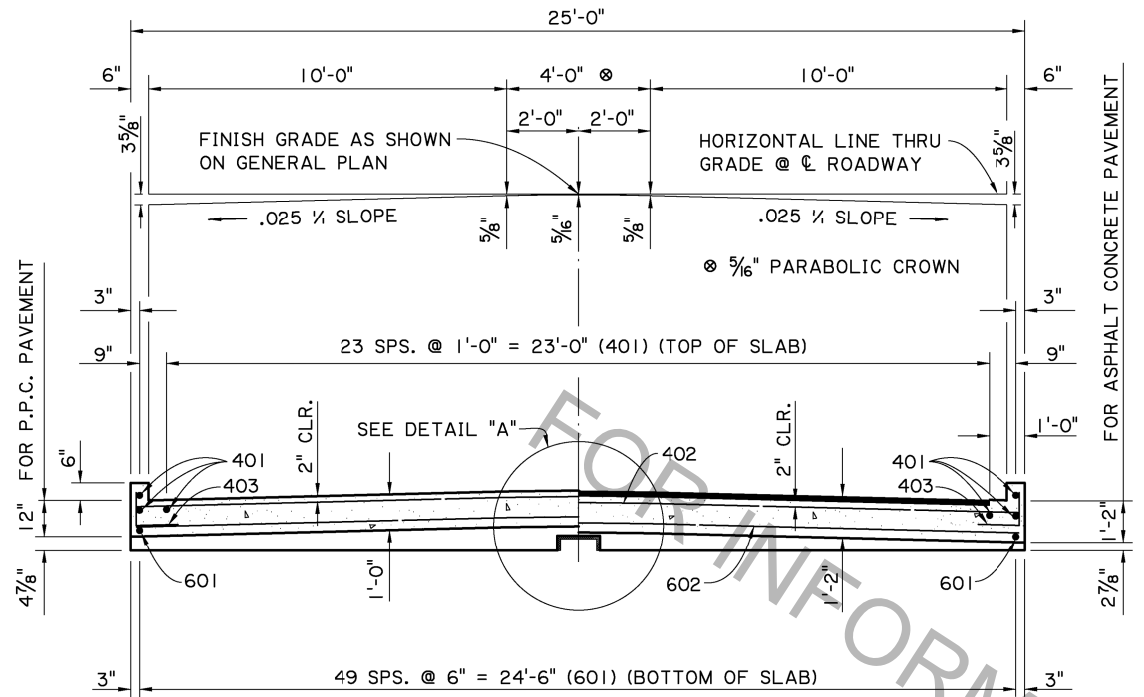


**BARRIER RAILING TRANSITION SECTIONS**  
SCALE: 3/4" = 1'-0"

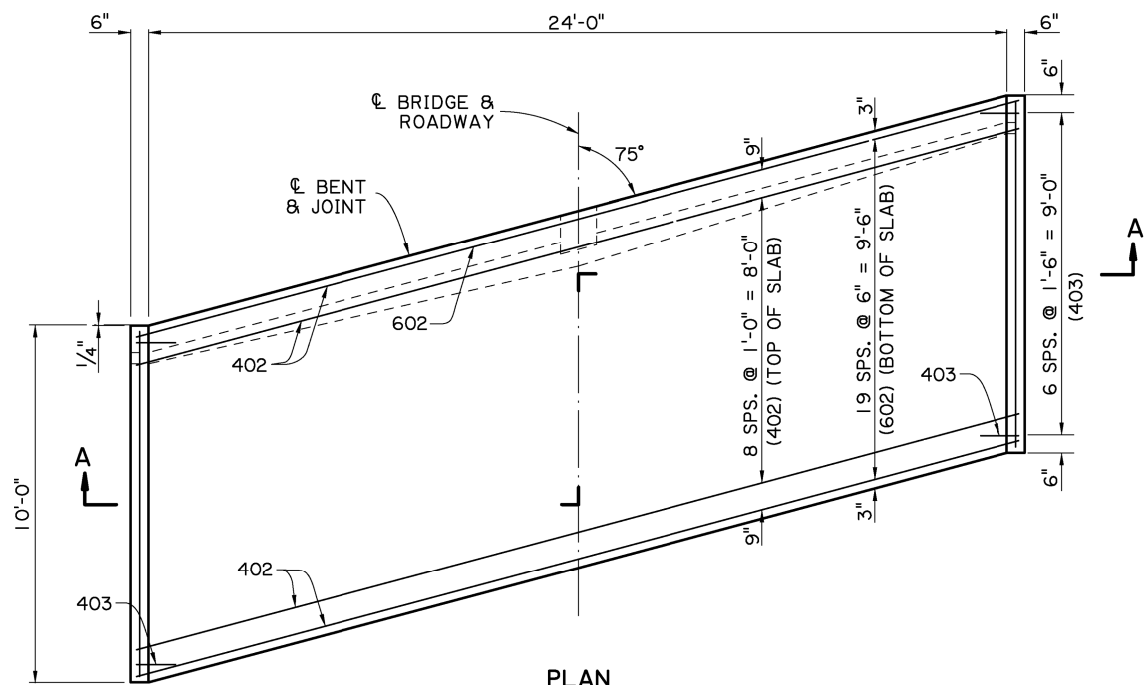


**BARRIER RAILING TRANSITION SCHEMATIC**  
SCALE: 3/8" = 1'-0"

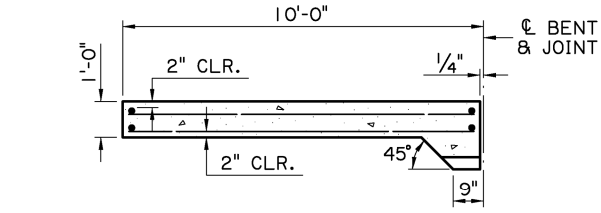
SHEET NUMBER	3 OF 11
DESIGNED	B. DELATTE
CHECKED	J. NAKHLEH
CONTROL SECTION	D. HYMEL
REVIEWED	J. NAKHLEH
STATE	05/17/17
PROJECT	SS-75-24-20SL
BY	
DATE	
REVISION OR CHANGE ORDER DESCRIPTION	
SPAN (2 OF 2) 20'-0" CONCRETE BARRIER 24'-0" CLEAR ROADWAY 75° CROSSING TWO WAY TANGENT DOTD DOTD BRIDGE DESIGN	



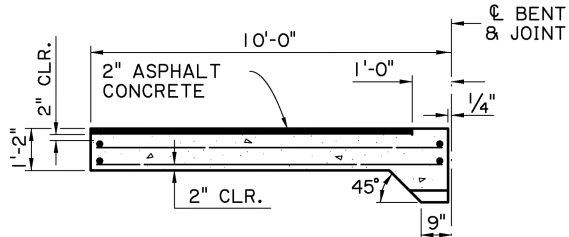
SECTION A-A  
SCALE: 3/8" = 1'-0"



PLAN  
SCALE: 3/8" = 1'-0"

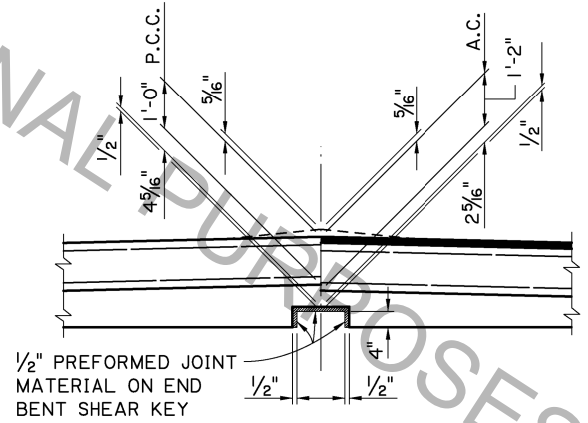


(FOR PORTLAND CEMENT CONCRETE ROADWAY PAVEMENT)

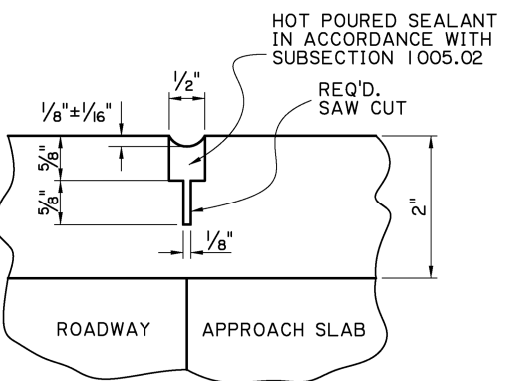


(FOR ASPHALT CONCRETE ROADWAY PAVEMENT)

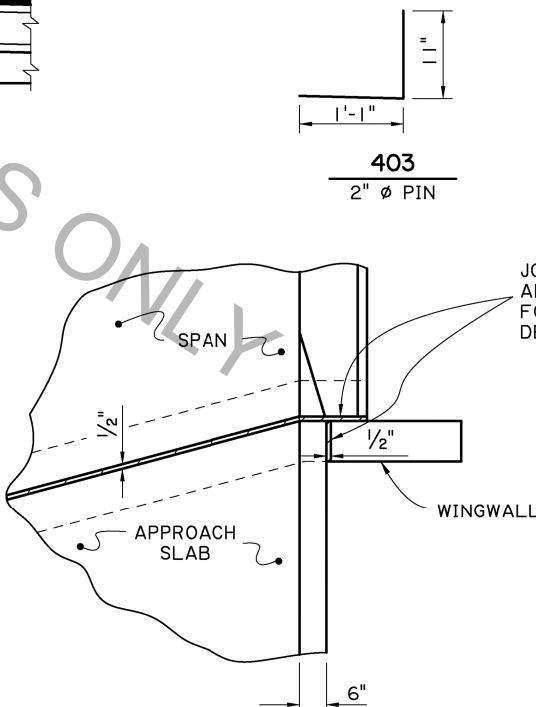
SECTION ALONG CL ROADWAY  
SCALE: 3/8" = 1'-0"



DETAIL A  
SCALE: 1/2" = 1'-0"



SAWING & SEALING  
JOINT DETAIL  
N.T.S.



JOINT DETAIL  
SCALE: 3/8" = 1'-0"

ESTIMATED QUANTITIES (ONE SLAB)				
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
601	50	9'-7"	479'-2"	LONGIT. BOT. OF SLAB
602	20	25'-6"	510'-0"	TRANSV. BOT. OF SLAB
TOTAL NO. 6 BARS = 989'-2" = 1,486 LBS.				
401	28	9'-7"	268'-4"	LONGIT. TOP OF SLAB & CURB
402	11	25'-6"	280'-6"	TRANSV. TOP OF SLAB
403	14	2'-0"	28'-0"	DOWELS IN CURB
TOTAL NO. 4 BARS = 576'-10" = 385 LBS.				
TOTAL DEFORMED REINFORCING STEEL = 1,871 LBS.				
CONCRETE APPROACH SLAB = 27.78 SQ. YDS.				
ASPHALTIC CONCRETE = 2.5 TONS				
SAW CUT AND SEAL = 24 LIN. FT.				

- TO BE PAID FOR UNDER ITEM 'CONCRETE APPROACH SLABS'.
- REQUIRED WHEN APPROACH SLAB IS ADJACENT TO ASPHALT CONCRETE PAVEMENT.

APPROACH SLAB NOTES:

**CONSTRUCTION SPECIFICATIONS:** LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

**DESIGN SPECIFICATIONS:** AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION, WITH 2008 & 2009 INTERIMS.

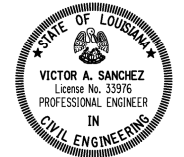
**STRUCTURAL CONCRETE:** ALL CONCRETE SHALL BE CLASS A1. EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER, UNLESS OTHERWISE NOTED.

**REINFORCING STEEL:** ALL REINFORCING STEEL SHALL BE GRADE 60. DIMENSIONS RELATING TO THE FABRICATION ARE OUT-TO-OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE NOTED.

**BEDDING MATERIAL:** FOR DETAILS OF BEDDING MATERIAL AND UNDERDRAINS, SEE STANDARD DETAIL BD.2.10.1.0.07.

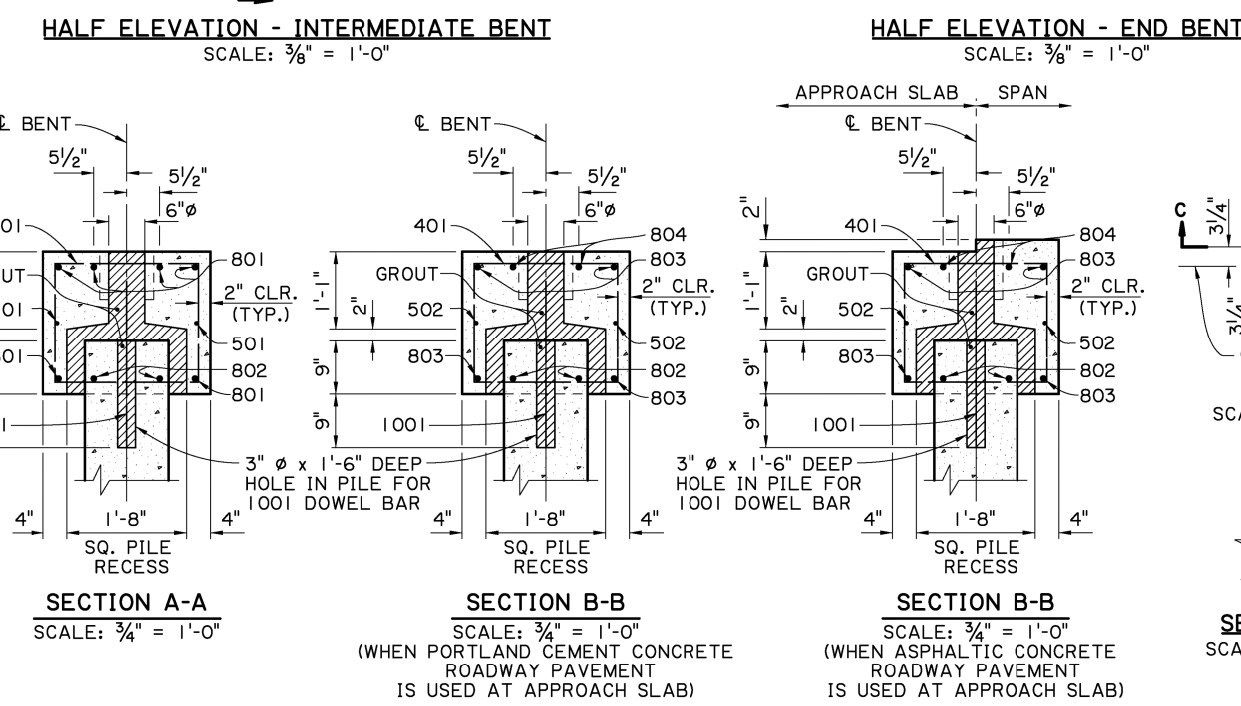
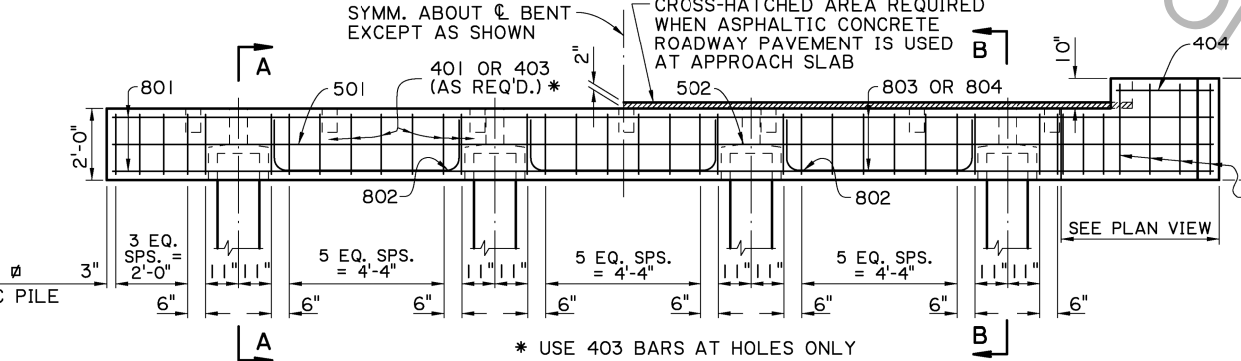
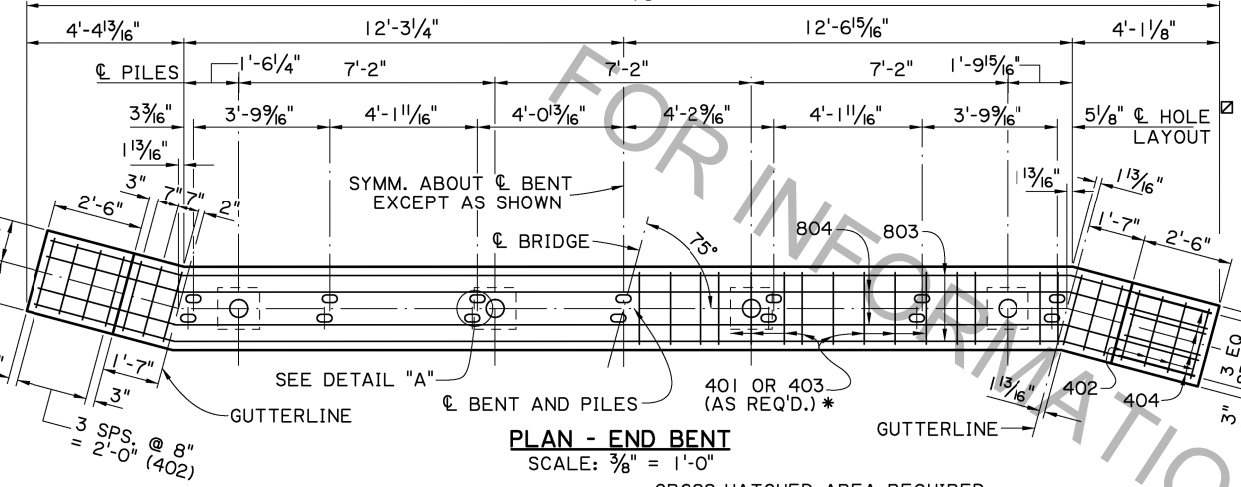
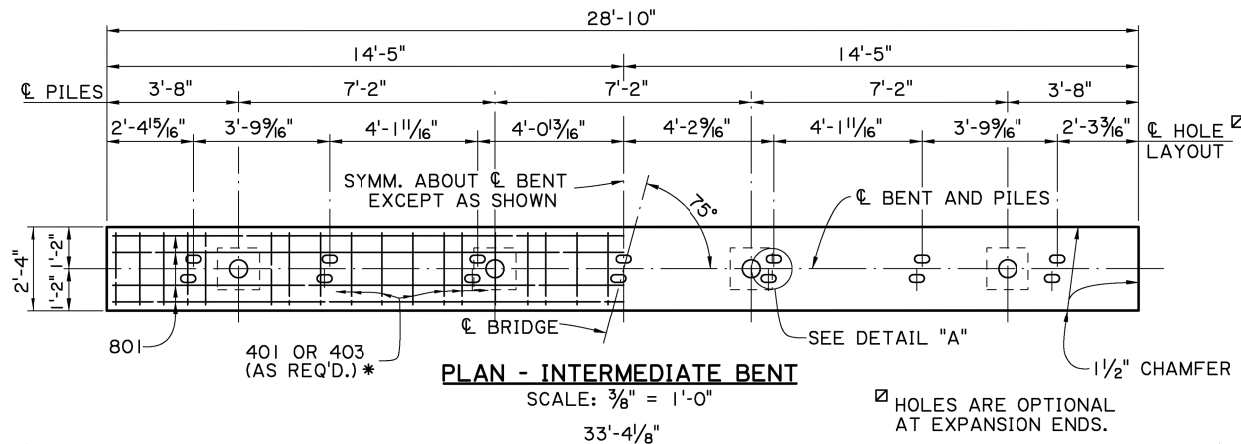
**SAWING & SEALING:** THE ASPHALT CONCRETE SHALL BE SAW CUT AT THE END OF THE CONCRETE APPROACH SLAB THE ENTIRE ROADWAY WIDTH AND SEALED. COST TO BE INCLUDED WITH CONCRETE APPROACH SLABS.

**BASIS OF PAYMENT:** ALL MATERIAL SHALL BE PAID FOR UNDER 'CONCRETE APPROACH SLABS' ACCORDING TO THE SPECIFICATIONS.



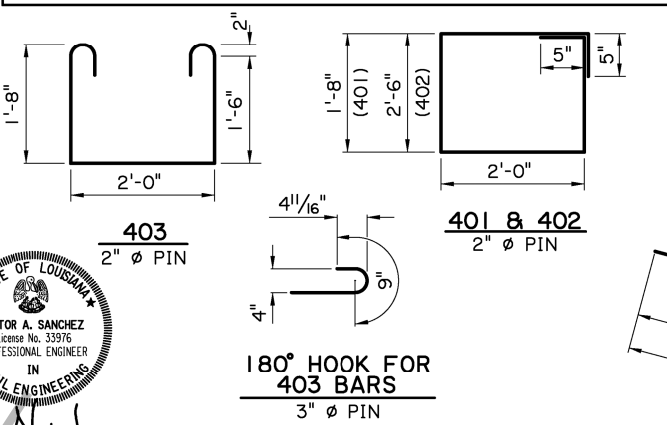
Victor A. Sanchez  
05/17/17





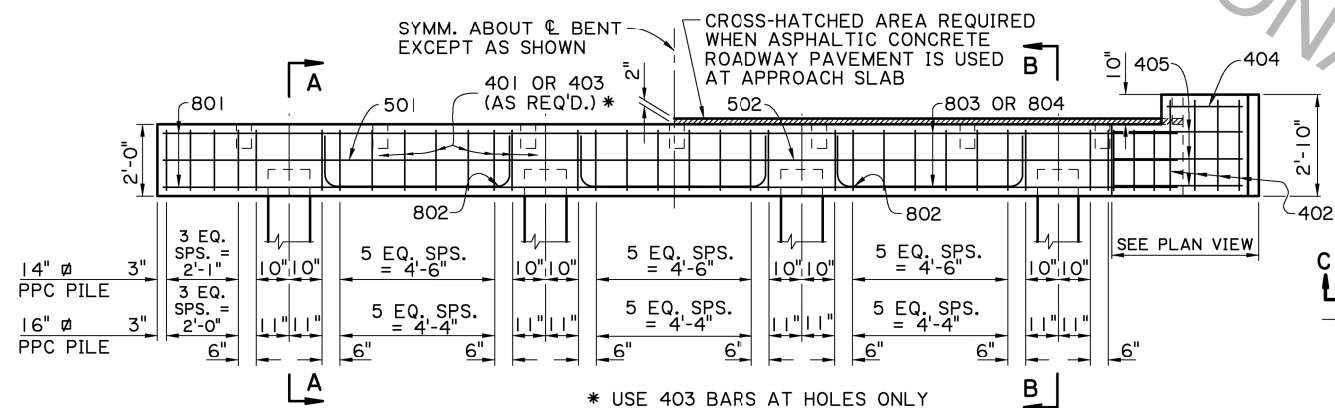
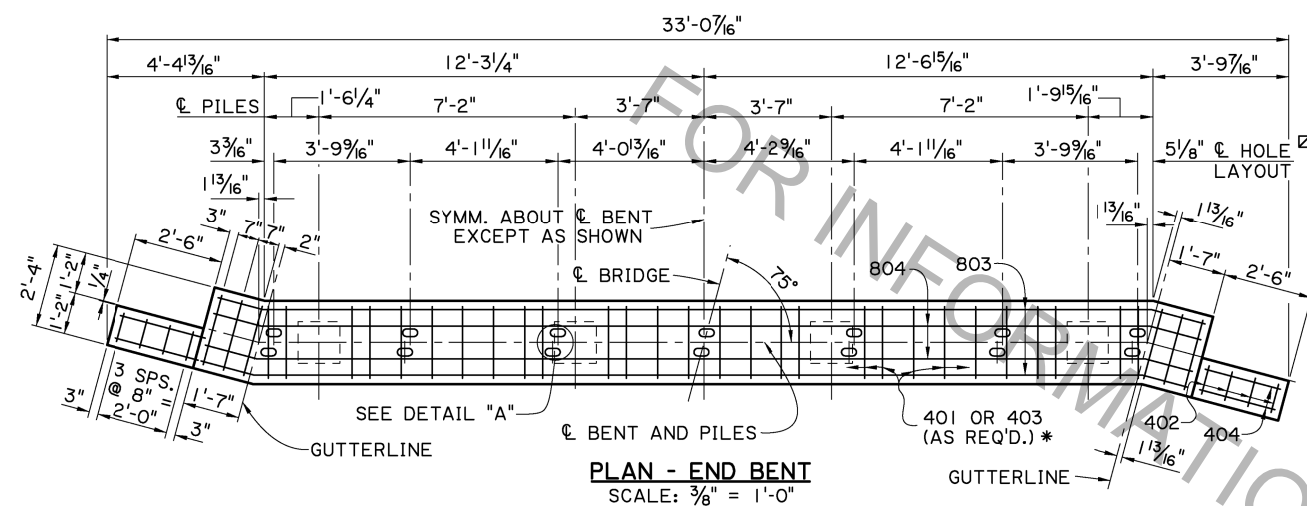
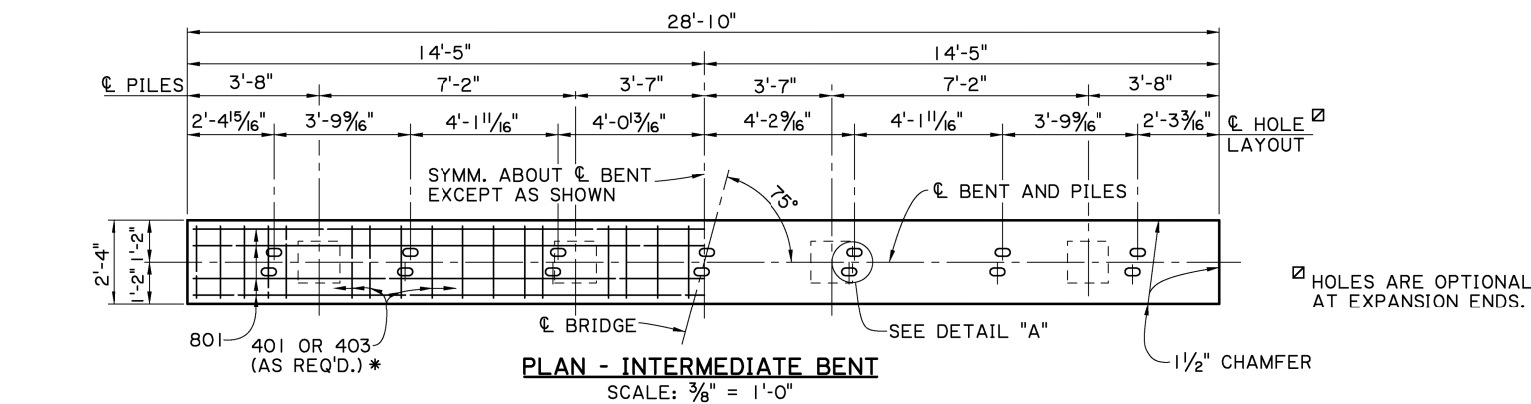
ESTIMATED QUANTITIES (ONE INTER. BENT)				
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
1001	4	2'-4"	9'-4"	DOWELS IN PILES
TOTAL NO. 10 BARS = 9'-4" = 40 LBS.				
801	6	28'-6"	171'-0"	LONGIT. IN CAP
802	6	7'-10"	47'-0"	LONGIT. IN CAP BTW. PILES
TOTAL NO. 8 BARS = 218'-0" = 582 LBS.				
501	2	28'-6"	57'-0"	LONGIT. IN CAP
TOTAL NO. 5 BARS = 57'-0" = 59 LBS.				
401	30	8'-2"	245'-0"	STIRRUPS IN CAP
403	4	6'-6"	26'-0"	STIRRUPS IN CAP
TOTAL NO. 4 BARS = 271'-0" = 181 LBS.				
TOTAL DEFORMED REINFORCING STEEL = 862 LBS.				
TOTAL CLASS P1 CONCRETE = 4.64 CU. YDS.				
MAX. PILE LOAD: SERVICE DEAD LOAD = 17 TONS				
SERVICE LIVE LOAD = 34 TONS				
FACTORED TOTAL LOAD = 72 TONS				
TOTAL GROUT FOR PILE RECESSES = 0.28 CU. YDS.				

ESTIMATED QUANTITIES (ONE END BENT)				
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
1001	4	2'-4"	9'-4"	DOWELS IN PILES
TOTAL NO. 10 BARS = 9'-4" = 40 LBS.				
802	6	7'-10"	47'-0"	LONGIT. IN CAP BTW. PILES
803	4	32'-8"	130'-8"	LONGIT. IN CAP
804	2	32'-8"	65'-4"	LONGIT. IN CAP
TOTAL NO. 8 BARS = 243'-0" = 649 LBS.				
502	2	32'-8"	65'-4"	LONGIT. IN CAP
TOTAL NO. 5 BARS = 65'-4" = 68 LBS.				
401	30	8'-2"	245'-0"	STIRRUPS IN CAP
402	8	9'-10"	78'-8"	STIRRUPS IN WINGWALL
403	4	6'-6"	26'-0"	STIRRUPS IN CAP
404	8	2'-2"	17'-4"	LONGIT. IN WINGWALL
TOTAL NO. 4 BARS = 367'-0" = 245 LBS.				
TOTAL DEFORMED REINFORCING STEEL = 1002 LBS.				
TOTAL CLASS P1 CONCRETE = 5.72 CU. YDS.				
MAX. PILE LOAD: SERVICE DEAD LOAD = 17 TONS				
SERVICE LIVE LOAD = 34 TONS				
FACTORED TOTAL LOAD = 72 TONS				
TOTAL GROUT FOR PILE RECESSES = 0.28 CU. YDS.				



ALTERNATE BENT NOTES:

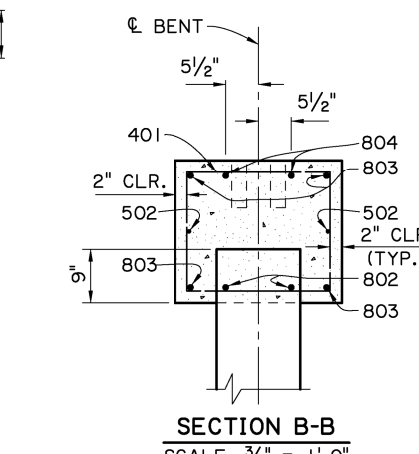
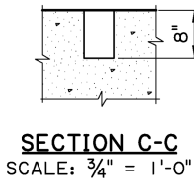
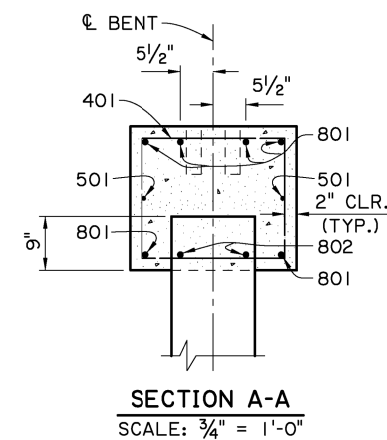
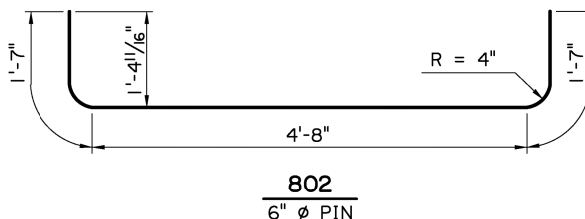
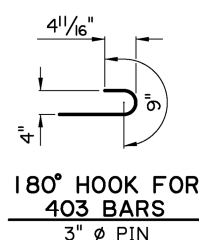
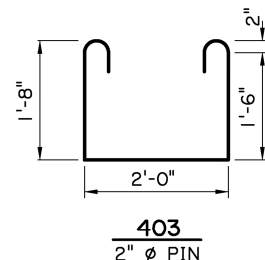
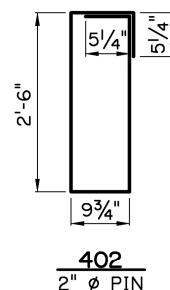
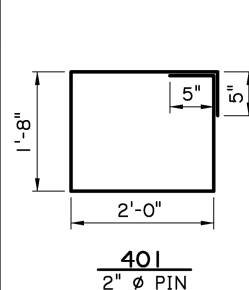
CONSTRUCTION SPECIFICATIONS: LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.  
DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION, WITH 2008 & 2009 INTERIMS.  
DESIGN LOAD: LIVE LOAD IS HL-93, AND LADV-11 (LOUISIANA DESIGN VEHICLE LIVE LOAD 2011).  
STRUCTURAL CONCRETE: ALL CONCRETE SHALL BE CLASS P1. STEEL SIDE FORMS AND STEEL OR CONCRETE BOTTOM FORMS SHALL BE USED FOR PRECAST COMPONENTS. EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER UNLESS OTHERWISE NOTED. ALL SURFACES SHALL RECEIVE A CLASS 1 ORDINARY SURFACE FINISH UPON REMOVAL OF THE FORMS. ALL EXPOSED FACES OF WINGWALLS AND ENDS OF CAPS SHALL RECEIVE A CLASS 3 SPECIAL FINISH.  
REINFORCING STEEL: ALL REINFORCING SHALL BE GRADE 60. DIMENSIONS RELATING TO FABRICATION ARE OUT TO OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE NOTED.  
GROUT: THE GROUT SHALL BE AN APPROVED FLOWABLE NON-SHRINK GROUT LISTED ON AML. THE GROUT SHALL BE TESTED FOR ACCEPTANCE PRIOR TO USAGE. SURFACES SHALL BE THOROUGHLY SATURATED WITH WATER BY FLOODING THE VOID FOR APPROXIMATELY 5 MINUTES IMMEDIATELY BEFORE THE GROUT IS PLACED. ONLY POTABLE WATER SHALL BE USED FOR SATURATION AND MIXING PURPOSES.  
PRECAST UNITS: THE PLANS FOR AN ONGOING OPERATION OF FABRICATING FACILITIES SHALL BE APPROVED BY THE DEPARTMENT. EACH UNIT SHALL HAVE THE FABRICATOR'S MARK AND UNIQUE NUMBER, MEETING THE APPROVAL OF THE ENGINEER, STAMPED OR SCRIBED IN THE PLASTIC CONCRETE. ALL UNITS SHALL BE HELD AT THE PLANT FOR A MINIMUM OF 10 DAYS AFTER CASTING. THE CONCRETE SHALL REACH A MINIMUM STRENGTH OF 3,000 PSI BEFORE HANDLING IS PERMITTED. THE LIFTING INSERTS SHALL BE 1" TYPE S INSERTS AS MANUFACTURED BY DAYTON-SUPERIOR CORPORATION OR AN APPROVED EQUAL. EACH INSERT SHALL HAVE A MINIMUM LOAD CAPACITY OF 10,000 POUNDS. FOUR INSERTS WITH 1" Ø x 5" LONG COIL BOLTS SHALL BE PLACED IN THE TOP OF THE UNITS AND LOCATED AT A DISTANCE 21% OF ITS LENGTH (+/- 6") FROM EACH END AND 6" FROM THE EDGES. INSERT HOLES SHALL BE GROUT FILLED AFTER PLACEMENT OF THE UNIT. AT THE CONTRACTOR'S OPTION, A SLING OF SUFFICIENT CAPACITY MAY BE USED FOR LIFTING, PROVIDED THE SAME PICKUP LOCATIONS FROM THE ENDS ARE USED.  
PRECAST CONCRETE PILES: PILES SHALL BE FABRICATED ACCORDING TO STANDARD DETAIL BD.2.5.1.0.01(CS-216). CENTROID OF THE PILE AT CUTOFF ELEVATION SHALL NOT VARY FROM THE PLAN LOCATION BY MORE THAN 3" MEASURED EITHER PERPENDICULAR OR PARALLEL TO THE CENTERLINE OF BENT. IF THE CENTROID OF A PILE IS OUTSIDE THESE LIMITS BUT WITHIN THE ACCURACY OF DRIVING REQUIRED BY THE SPECIFICATIONS, A BENT CAP SHALL BE PROVIDED ACCORDING TO THE CAST-IN-PLACE ALTERNATE. EXTERIOR PILES ARE TO BE BATTERED OUTWARD A 1/2 ON 12 IN THE LONGITUDINAL DIRECTION OF THE BENT, WHEN NOTED ON THE GENERAL PLAN.  
BASIS OF PAYMENT: ALL MATERIALS SHALL BE PAID FOR UNDER "BRIDGE SUPERSTRUCTURE AND SUBSTRUCTURE" ACCORDING TO THE SPECIFICATIONS.



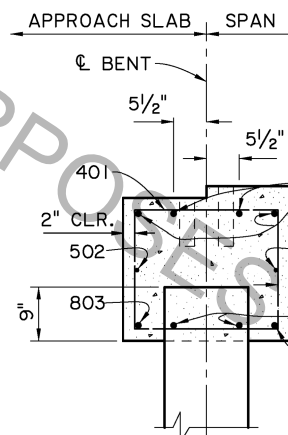
HALF ELEVATION - INTERMEDIATE BENT  
SCALE:  $\frac{3}{8}" = 1'-0"$

HALF ELEVATION - END BENT  
SCALE:  $\frac{3}{8}" = 1'-0"$

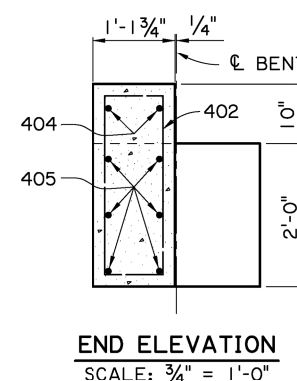
AS-DESIGNED RATING		
VEHICLE	RATING FACTOR	NOTES
HL-93 (INV)	1.438	—
HL-93 (OPR)	1.864	—
LADV-11 (INV)	1.106	MAGNIFICATION FACTOR = 1.3



SCALE:  $\frac{1}{4}'' = 1'-0''$   
(WHEN PORTLAND CEMENT CONCRETE ROADWAY  
PAVEMENT IS USED AT APPROACH SLAB)



**SECTION B-B**  
**SCALE:  $\frac{3}{4}" = 1'-0"$**   
**(WHEN ASPHALTIC CONCRETE ROADWAY PAVEMENT IS USED AT APPROACH SLAB)**



END ELEVATION  
SCALE:  $\frac{3}{4}" = 1'-0"$

ESTIMATED QUANTITIES (ONE INTER. BENT)				
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
801	6	28'-6"	171'-0"	LONGIT. IN CAP
802	6	7'-10"	47'-0"	LONGIT. IN CAP BTW. PILES
TOTAL NO. 8 BARS =			218'-0"	= 582 LBS.
501	2	28'-6"	57'-0"	LONGIT. IN CAP
TOTAL NO. 5 BARS =			57'-0"	= 59 LBS.
401	30	8'-2"	245'-0"	STIRRUPS IN CAP
403	4	6'-6"	26'-0"	STIRRUPS IN CAP
TOTAL NO. 4 BARS =			271'-0"	= 181 LBS.
TOTAL DEFORMED REINFORCING STEEL =				822 LBS.
TOTAL CLASS A1 CONCRETE =				4.79 CU. YDS.
MAX. PILE LOAD:		SERVICE DEAD LOAD = 17 TONS		
		SERVICE LIVE LOAD = 34 TONS		
		FACTORED TOTAL LOAD = 72 TONS		

☒ 16" Ø PPC PILES USED FOR ESTIMATING PURPOSES ONLY. (ADD 0.04 CU. YDS. OF CLASS A1 CONCRETE PER BENT WHEN 14" Ø PPC PILES ARE USED.)

ESTIMATED QUANTITIES (ONE END BENT)				
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
802	6	7'-10"	47'-0"	LONGIT. IN CAP BTW. PILES
803	4	27'-8"	110'-8"	LONGIT. IN CAP
804	2	27'-8"	55'-4"	LONGIT. IN CAP
<b>TOTAL NO. 8 BARS = 213'-0" = 569 LBS.</b>				
502	2	27'-8"	55'-4"	LONGIT. IN CAP
<b>TOTAL NO. 5 BARS = 55'-4" = 58 LBS.</b>				
401	30	8'-2"	245'-0"	STIRRUPS IN CAP
402	8	7'-6"	60'-0"	STIRRUPS IN WINGWALL
403	4	6'-6"	26'-0"	STIRRUPS IN CAP
404	4	2'-2"	8'-8"	LONGIT. IN WINGWALL
405	12	3'-11"	47'-0"	LONGIT. IN WINGWALL
<b>TOTAL NO. 4 BARS = 386'-8" = 258 LBS.</b>				
<b>TOTAL DEFORMED REINFORCING STEEL = 885 LBS.</b>				
<b>⊗ TOTAL CLASS A1 CONCRETE = 5.24 CU. YDS.</b>				
<b>MAX. PILE LOAD: SERVICE DEAD LOAD = 17 TONS</b>				
<b>SERVICE LIVE LOAD = 34 TONS</b>				
<b>FACTORED TOTAL LOAD = 72 TONS</b>				

⊗ 16" ∅ PPC PILES USED FOR ESTIMATING PURPOSES ONLY. (ADD 0.04 CU. YDS. OF CLASS A1 CONCRETE PER BENT WHEN 14" ∅ PPC PILES ARE USED.) ADD 0.20 CU. YDS. OF CLASS A1 CONCRETE PER BENT WHEN ASPHALTIC CONCRETE ROADWAY PAVEMENT IS USED AT APPROACH SLAB.

ALTERNATE BENT NOTES:

CONSTRUCTION SPECIFICATIONS: LATEST APPROVED LOUISIANA  
STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES,  
SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION, WITH 2008 & 2009 INTERIMS.

DESIGN LOAD: LIVE LOAD IS HL-93, AND LADV-11 (LOUISIANA DESIGN VEHICLE LIVE LOAD 2011).

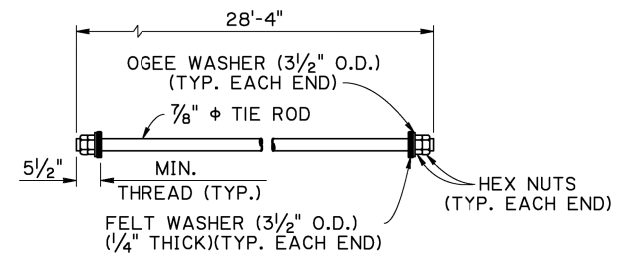
**STRUCTURAL CONCRETE:** ALL CONCRETE SHALL BE CLASS A1. EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER UNLESS OTHERWISE NOTED. ALL EXPOSED FACES OF WINGWALLS AND ENDS OF CAPS SHALL RECEIVE A SURFACE FINISH AS PER SUBSECTION 805.08 OF THE STANDARD SPECIFICATIONS, EXCEPT WHEN SPECIFIED ELSEWHERE IN THE PLANS.

REINFORCING STEEL: ALL REINFORCING SHALL BE GRADE 60. DIMENSIONS RELATING TO FABRICATION ARE OUT TO OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE NOTED.

PRECAST CONCRETE PILES: FOR DETAILS OF PILES SEE STANDARD  
DETAIL BD.2.5.1.0.01(CS-216). EXTERIOR PILES ARE TO BATTERED  
OUTWARD AT 1 1/2 ON 12 IN THE LONGITUDINAL DIRECTION OF THE  
BENT. WHEN NOTED ON THE GENERAL PLAN.

BASIS OF PAYMENT: ALL MATERIALS SHALL BE PAID FOR UNDER "BRIDGE SUPERSTRUCTURE AND SUBSTRUCTURE" ACCORDING TO THE SPECIFICATIONS.





THE NUTS & WASHERS FOR THE TIE ROD SHALL BE ZINC COATED AND THE EXPOSED ENDS TO THE TIE RODS SHALL BE PAINTED WITH AN APPROVED COATING. AS A FINAL OPERATION THE CONTRACTOR SHALL BE REQUIRED TO TORQUE THE INSTALLED TIE ROD TO 170 FT. LBS. JUST PRIOR TO PAINTING. ALL EXPOSED ENDS SHALL BE PAINTED WITH AN APPROVED COATING AFTER STRESSING. ONE (1) MECHANICAL SPLICE MAY BE USED IN SPLICING THE  $\frac{7}{8}$ "  $\phi$  TIE ROD. THE SPLICE SHALL DEVELOP AT LEAST 125% OF THE SPECIFIED YIELD STRENGTH OF THE TIE ROD IN TENSION. THE MECHANICAL SPLICE SHALL BE ZINC COATED OR PAINTED WITH AN APPROVED COLD GALVANIZING REPAIR COMPOUND FROM AML PRIOR TO PLACING THE TIE ROD IN THE STRUCTURE.

SCALE:  $\frac{3}{8}" = 1'-0"$

N.T.S.

SCALE:  $\frac{3}{8}" = 1'-0"$


SCALE: 1" = 1'-0"

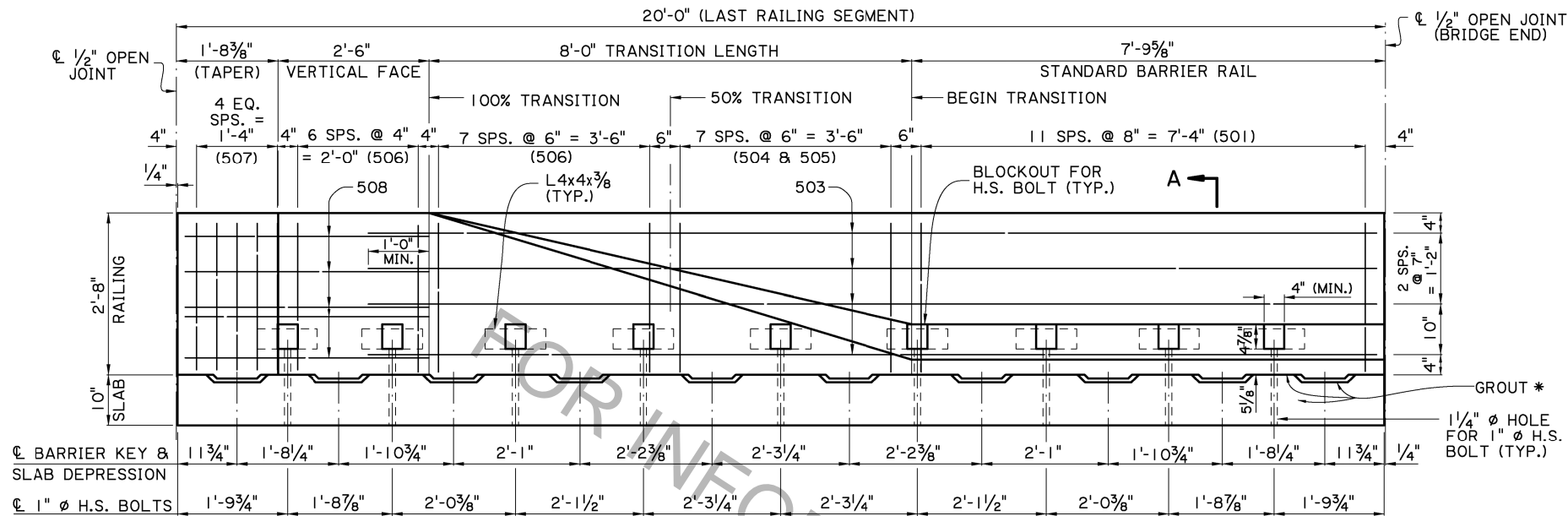
SCALE:  $\frac{3}{4}" = 1'-0"$

SCALE:  $\frac{3}{4}" = 1'-0"$

SCALE: 1" = 1'-0"

SCALE:  $\frac{3}{8}" = 1'-0"$

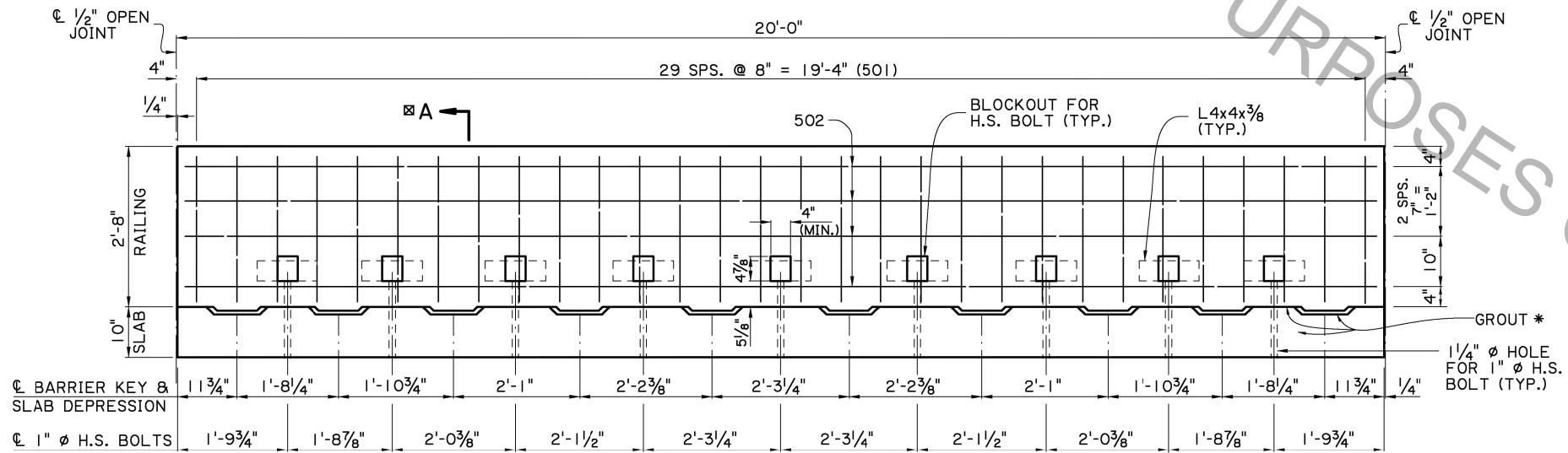
	ALTERNATE SPAN (1 OF 4)		DESIGNED BY	B. DELATTE	PARISH
	20'-0" PRECAST CONCRETE SLAB SPAN		CHECKED BY	J. NAKHLEH	
	24'-0" CLEAR ROADWAY		DETAILED BY	D. HYMEL	CONTROL SECTION
	75' CROSSING TWO WAY TANGENT		CHECKED BY	J. NAKHLEH	
			REVIEWED BY	05/17/17	STATE PROJECT
			SERIES #	7 OF 11	
STANDARD DETAIL		NO.	DATE	REVISION OR CHANGE ORDER DESCRIPTION	
DOTD BRIDGE DESIGN		PSS-75-24-20SL			
DOTD		SHEET NUMBER			



FOR SECTION A-A & TRANSITION SECTIONS  
SEE ALTERNATE SPAN (3 OF 4)

**PRECAST BARRIER RAILING TRANSITION ELEVATION**  
(SHOWING BARRIER RAILING AT END OF BRIDGE)  
SCALE: 3/4" = 1'-0"

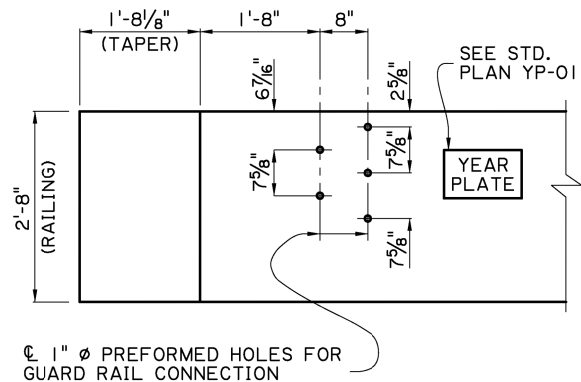
\* PLACE OR INJECT NON-SHRINK GROUT AS REQUIRED IN BETWEEN SLAB DEPRESSIONS TO FILL ALL VOIDS AND GAPS FOR FULL EVEN BEARING OF THE BARRIER ON THE SLAB. SEE NOTE 3, SHEET 9 OF 11.



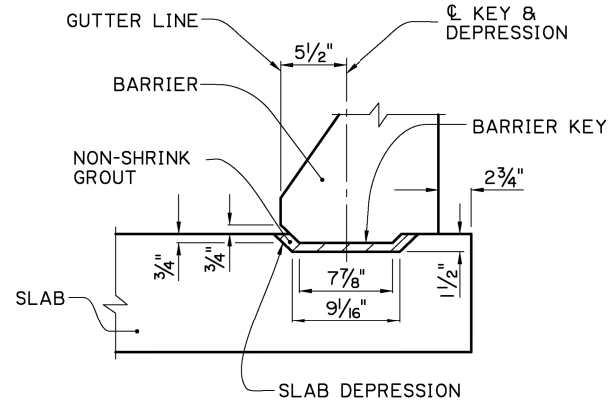
**STANDARD PRECAST BARRIER RAILING ELEVATION**  
(SHOWING BARRIER RAILING ALONG BRIDGE SLAB)  
SCALE: 3/4" = 1'-0"



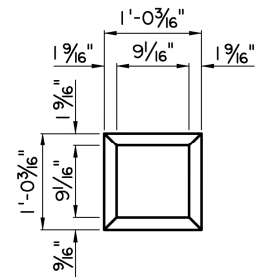
05/17/17



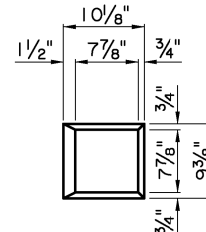
**GUARD RAIL CONNECTION DETAIL**  
(FOR GUARD RAIL DETAILS,  
SEE STANDARD PLAN BD.1.1.1.0.01 (GR-200).  
SCALE: 3/4" = 1'-0"



**ELEVATION**  
SCALE: 1 1/2" = 1'-0"



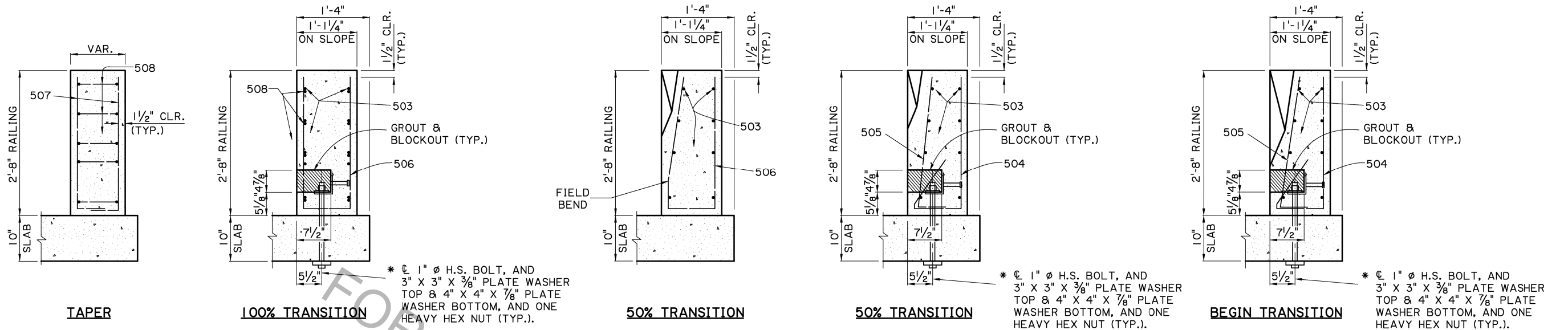
**PLAN-DEPRESSION**  
SCALE: 1" = 1'-0"



**PLAN-KEY**  
SCALE: 1" = 1'-0"

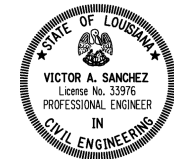
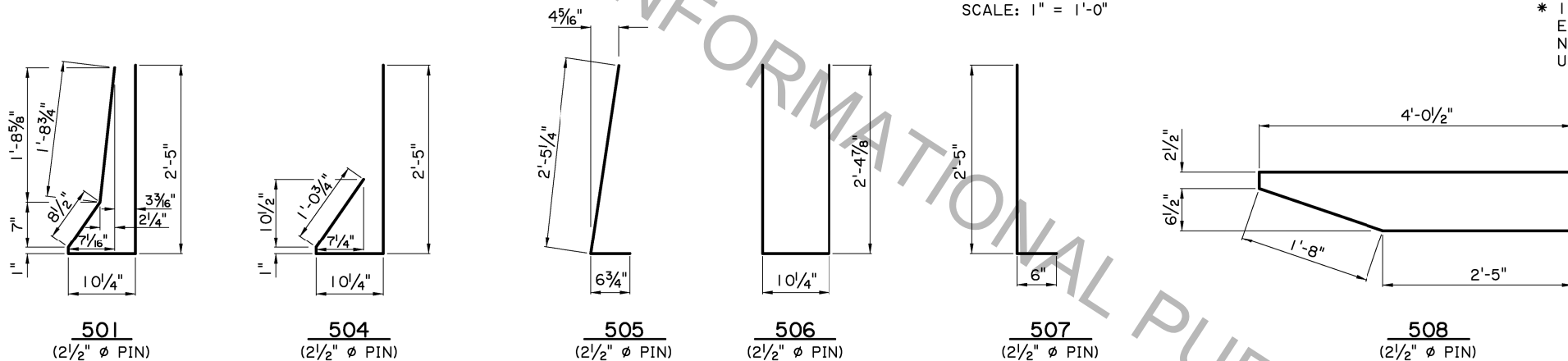
**BARRIER KEY AND  
PANEL DEPRESSION DETAILS**





**BARRIER RAILING TRANSITION SECTIONS**

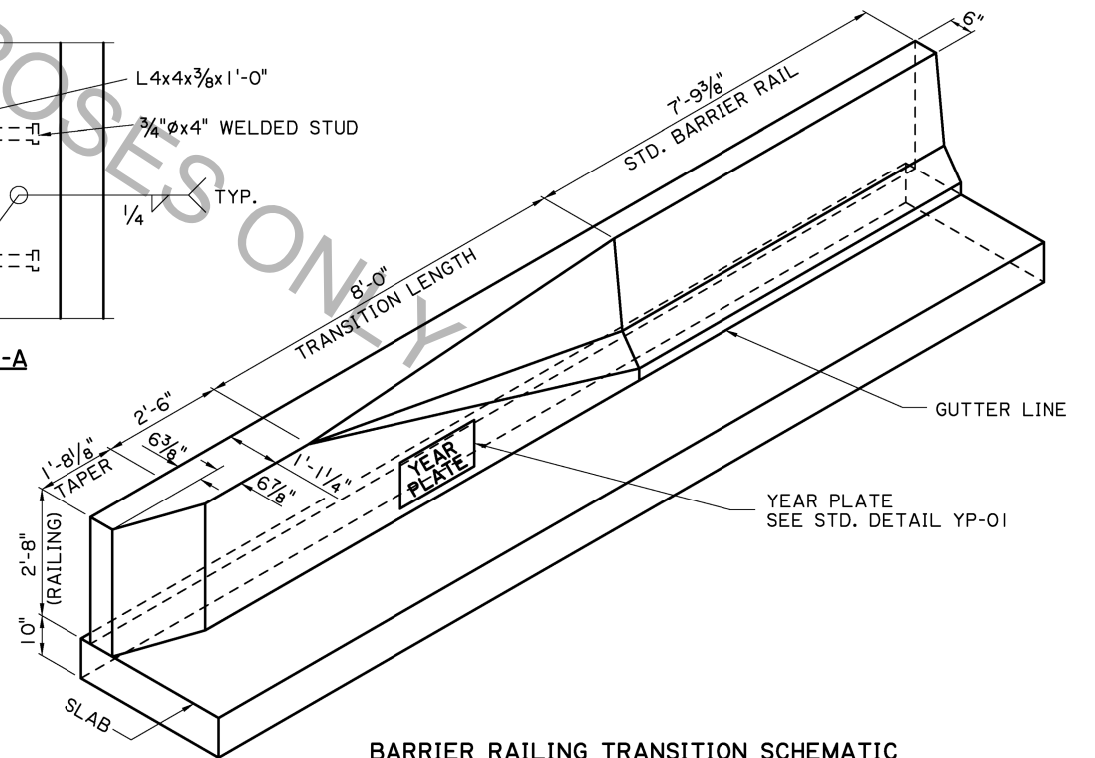
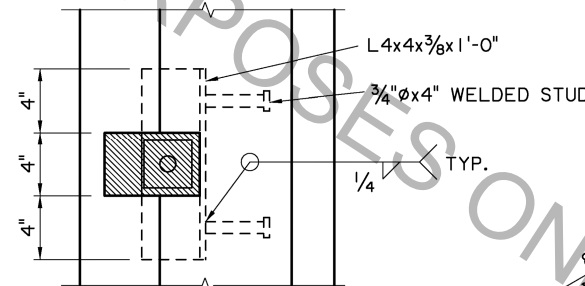
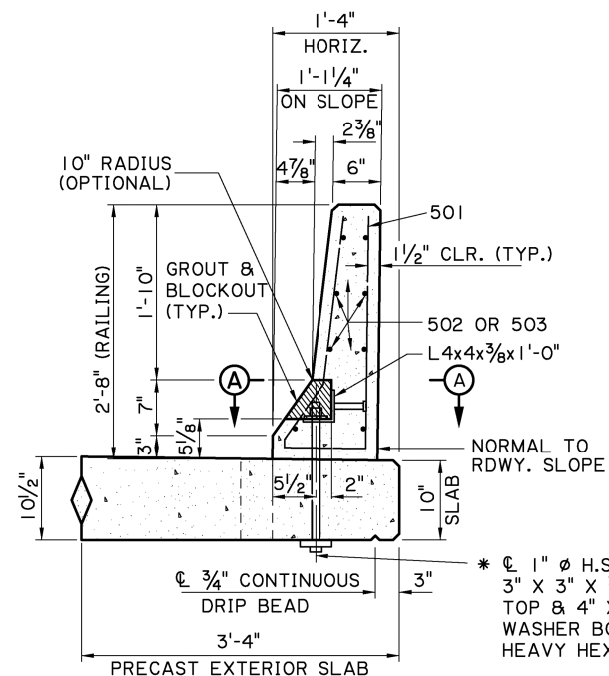
SCALE: 1" = 1'-0"

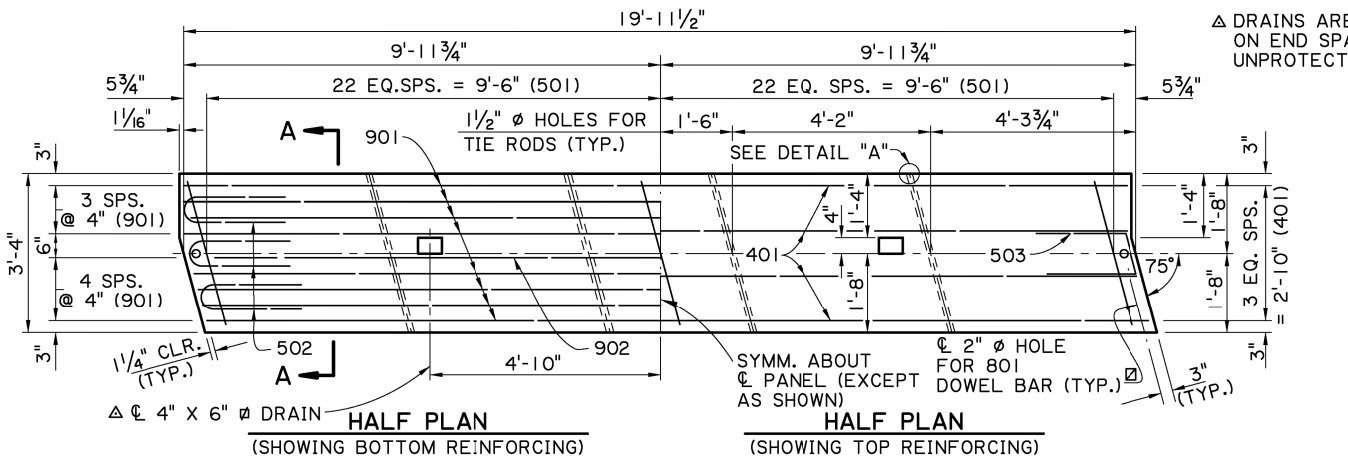


05/17/17

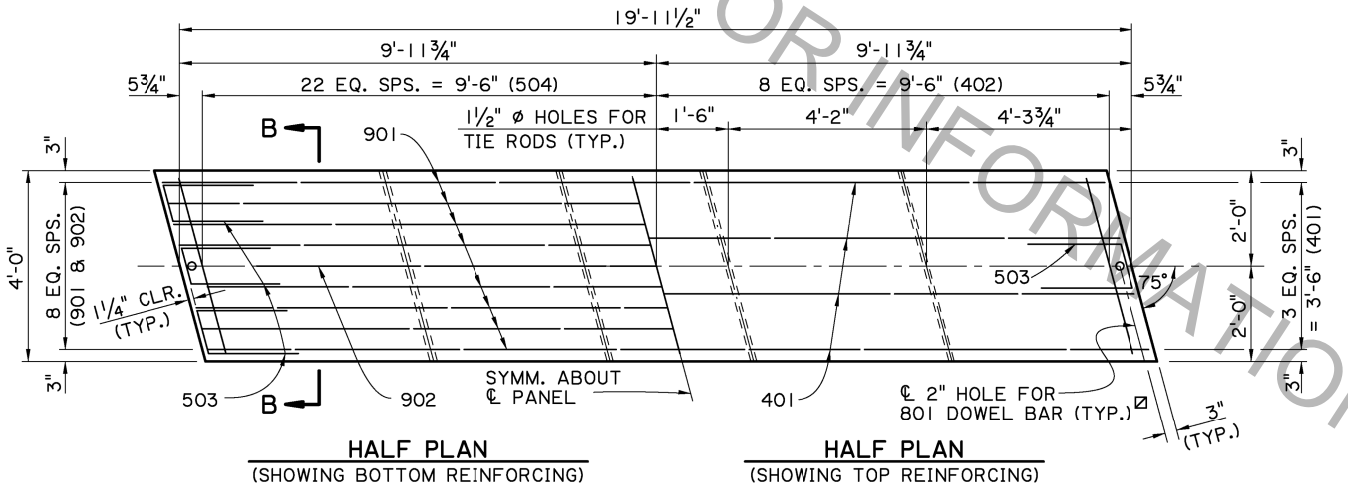
**NOTES:**

- 1) ALL BARRIER RAIL SURFACES ARE TO RECEIVE A CLASS 3 SPECIAL FINISH.
- 2) ALL SURFACES OF THE BLOCKOUTS EXCEPT THE BOTTOM MAY BE TAPERED AND ALL CORNERS MAY BE ROUNDED TO A RADIUS TO ALLOW FOR EASY REMOVAL OF PLUGS OR FORMS. AFTER PLACING AND TIGHTENING THE ANCHOR BOLTS, THE BLOCKOUTS SHALL BE FILLED WITH AN APPROVED NON-SHRINK GROUT FROM AML AND TROWELED TO THE REQUIRED FINISH AND TO THE SATISFACTION OF THE ENGINEER.
- 3) AFTER BARRIER IS PLACED AND ALIGNED, ALL GAPS UNDER BARRIER AND TOP OF SLAB SHALL BE FILLED WITH NON-SHRINK GROUT FROM AML AND ALLOWED TO SET PRIOR TO TIGHTENING OF BOLTS. IT IS IMPORTANT TO FILL ALL VOIDS AND GAPS UNDER THE BARRIER TO ENSURE EVEN BEARING ON DECK WHEN THE ANCHOR BOLTS ARE LOADED.
- 4) ALL 1"  $\varnothing$  BOLTS SHALL BE HIGH STRENGTH A325 OR APPROVED EQUAL. BOLT, NUT & WASHER TO BE GALVANIZED AS PER ASTM A-153. BOLTS SHALL BE TENSIONED TO 36 KIPS, OR APPROXIMATELY 540 FOOT-LB. OF TORQUE (LUBRICATED CONNECTION).

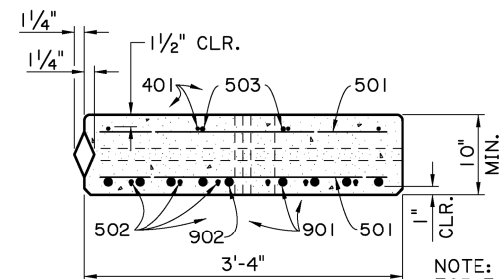




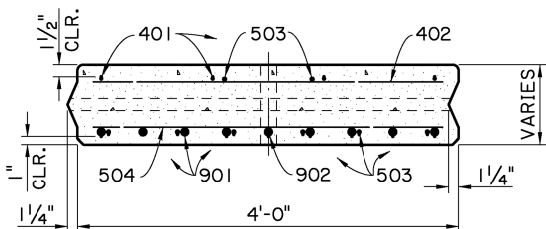
EXTERIOR UNIT  
SCALE: 1/2" = 1'-0"



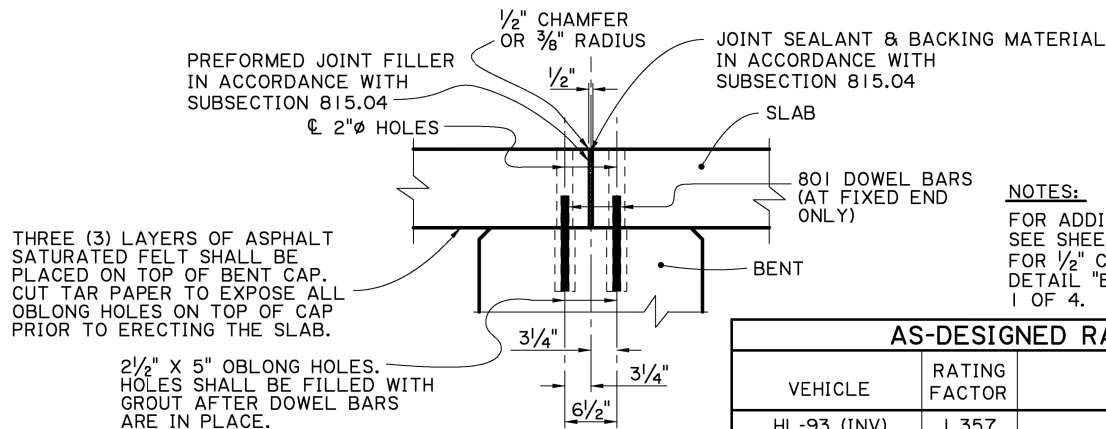
INTERIOR UNIT  
SCALE: 1/2" = 1'-0"



SECTION A-A  
EXTERIOR UNIT  
SCALE: 1" = 1'-0"



SECTION B-B  
INTERIOR UNIT  
SCALE: 1" = 1'-0"



TYPICAL JOINT DETAIL  
SCALE: 1" = 1'-0"

AS-DESIGNED RATING		
VEHICLE	RATING FACTOR	NOTES
HL-93 (INV)	1.357	
HL-93 (OPR)	1.759	
LADV-11 (INV)	1.044	MAGNIFICATION FACTOR = 1.3

△ DRAINS ARE NOT REQUIRED ON END SPANS OVER UNPROTECTED SLOPES.

ALTERNATE SPAN NOTES:

CONSTRUCTION SPECIFICATIONS : LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS : AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION, WITH 2008 & 2009 INTERIMS.

DESIGN LOAD : THE BRIDGE DECK IS DESIGNED FOR A FUTURE WEARING COURSE OF 19 PSF. THE LIVE LOAD IS HL-93, AND LADV-11 (LOUISIANA DESIGN VEHICLE LIVE LOAD 2011).

STRUCTURAL CONCRETE : ALL CONCRETE SHALL BE CLASS P1. THE BRIDGE RAIL CONCRETE SHALL BE CLASS A1 IF RAIL IS CAST IN PLACE. STEEL SIDE FORMS AND STEEL OR CONCRETE BOTTOM FORMS SHALL BE USED FOR PRECAST COMPONENTS. EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER, UNLESS OTHERWISE NOTED. ALL SURFACES SHALL RECEIVE A CLASS 1 ORDINARY SURFACE FINISH UPON REMOVAL OF THE FORMS. THE FINAL FINISH SHALL BE A FINE FINISH IN ACCORDANCE WITH SUB-SECTION 805.08.5.3 OF THE LOUISIANA STANDARD SPECIFICATIONS.

REINFORCING STEEL : ALL REINFORCING STEEL SHALL BE GRADE 60. DIMENSIONS RELATING TO FABRICATION ARE OUT TO OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE NOTED. ALL REINFORCING BARS SHALL BE PLACED TO PROVIDE A MINIMUM COVER OF 1" FROM THE DRAIN HOLES. REINFORCING STEEL MAY BE TACK WELDED FOR A DISTANCE OF NOT MORE THAN 4'-0" FROM EACH END OF UNIT. NO OTHER WELDING SHALL BE PERMITTED.

MISCELLANEOUS STEEL : HIGH STRENGTH BOLTS SHALL CONFORM TO ASTM DESIGNATION A-325. PLATES, TIE RODS, AND DRIFT BOLTS SHALL CONFORM TO ASTM DESIGNATION A709, GRADE 36 (AASHTO M270). STEEL SPECIFIED TO BE ZINC COATED SHALL BE IN CONFORMANCE WITH ASTM DESIGNATION A-123.

GROUT : THE GROUT SHALL BE AN APPROVED FLOWABLE NON-SHRINK GROUT LISTED ON AML. THE GROUT SHALL BE TESTED FOR ACCEPTANCE PRIOR TO USAGE. THE GROUT SHALL ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 3500 PSI PRIOR TO LOADING SLABS. SURFACES SHALL BE THOROUGHLY SATURATED WITH WATER BY FLOODING THE HOLES FOR APPROXIMATELY FIVE (5) MINUTES IMMEDIATELY BEFORE THE GROUT IS PLACED. ONLY POTABLE WATER SHALL BE USED FOR SATURATION AND MIXING PURPOSES.

PATCHING MATERIAL : THE PATCHING MATERIAL SHALL BE AN APPROVED PATCHING MATERIAL FOR PRECAST OR PRESTRESSED CONCRETE PRODUCTS LISTED ON AML(QPL 49). SURFACE PREPARATION, MIXING AND PLACEMENT SHALL BE IN ACCORDANCE WITH THE MANUFACTURERS' RECOMMENDATIONS. ONLY POTABLE WATER SHALL BE USED FOR SATURATION AND MIXING PURPOSES.

PRECAST UNITS : THE PLANS FOR AN ONGOING OPERATION OF FABRICATION FACILITIES SHALL BE APPROVED BY THE DEPARTMENT. EACH UNIT SHALL HAVE "LIVE LOAD HL-93 & LADV-11", THE FABRICATOR'S MARK, AND UNIQUE NUMBER, MEETING THE APPROVAL OF THE ENGINEER STAMPED OR INSCRIBED IN THE PLASTIC CONCRETE. PRECAST UNITS MAY BE CAST WITH OR WITHOUT CAMBER. IF CAMBER IS PROVIDED IT SHALL NOT EXCEED 1/4" AT THE CENTERLINE OF SPAN. ALL UNITS SHALL BE HELD AT THE PLANT FOR A MINIMUM OF TEN(10) DAYS AFTER CASTING. THE CONCRETE SHALL REACH A MINIMUM STRENGTH OF 3,000 PSI BEFORE HANDLING IS PERMITTED. THE LIFTING INSERTS SHALL BE 1", TYPE S INSERTS AS MANUFACTURED BY DAYTON-SUPERIOR CORPORATION OR AN APPROVED EQUAL. EACH INSERT SHALL HAVE A MINIMUM LOAD CAPACITY OF 10,000 POUNDS. FOUR(4) INSERTS WITH 1" Ø X 5" LONG COIL BOLTS SHALL BE PLACED IN THE TOP OF THE UNIT AND LOCATED 1'-3" FROM ITS ENDS AND 1'-0" FROM ITS EDGES. INSERT HOLES SHALL BE GROUT FILLED AFTER PLACEMENT OF UNIT. AT THE CONTRACTOR'S OPTION A SLING OF SUFFICIENT CAPACITY MAY BE USED FOR LIFTING, PROVIDED THE SAME PICKUP LOCATION FROM THE ENDS ARE USED. FABRICATION TOLERANCES SHALL BE AS FOLLOWS:

UNIT DEPTH ± 3/16"  
UNIT LENGTH + 1/8" AND -1/2"  
OVERALL SPAN WIDTH ± 2"

ALL PRECAST UNITS IN EACH BRIDGE SPAN SHALL BE MATCH CAST IN THE SAME CASTING BED TO ENSURE A PROPER FIT DURING INSTALLATION.

GUARDRAIL : REFER TO GENERAL PLAN FOR GUARDRAIL REQUIREMENTS. PROVIDE HOLES FOR GUARDRAIL CONNECTIONS ACCORDING TO STANDARD PLAN BD.1.1.1.0.01(GR-200) ON ALL FOUR(4) BRIDGE ENDS.

BASIS OF PAYMENT : ALL MATERIALS SHALL BE PAID FOR UNDER "BRIDGE SUPERSTRUCTURE AND SUBSTRUCTURE" ACCORDING TO THE SPECIFICATIONS.

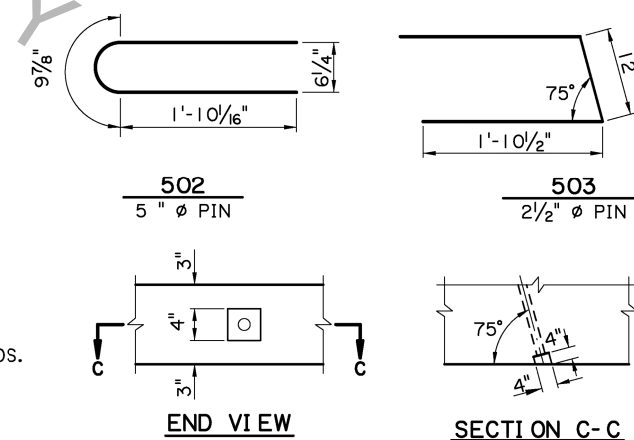
ESTIMATED QUANTITIES (ONE EXTERIOR UNIT)

BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
901	8	19'-8"	157'-4"	LONGIT. BOT. OF SLAB
902	1	18'-11"	18'-11"	LONGIT. BOT. OF SLAB
TOTAL NO. 9 BARS = 176'-3" = 599 LBS.				
801	1	1'-0"	1'-0"	DOWELS
TOTAL NO. 8 BARS = 1'-0" = 3 LBS.				
501	90	3'-1"	277'-6"	TRANS. TOP & BOT. OF SLAB
502	6	4'-6"	27'-0"	BOT. END OF SLAB
503	2	4'-9"	9'-6"	TOP END OF SLAB
TOTAL NO. 5 BARS = 313'-11" = 327 LBS.				
401	4	19'-8"	78'-8"	LONGIT. TOP OF SLAB
TOTAL NO. 4 BARS = 78'-8" = 53 LBS.				
DEFORMED REINFORCING STEEL = 982 LBS.				
CLASS P1 CONCRETE = 2.05 CU. YDS.				
CONCRETE RAILING (PER SPAN) = 40.00 LIN. FT.				

ESTIMATED QUANTITIES (ONE INTERIOR UNIT)

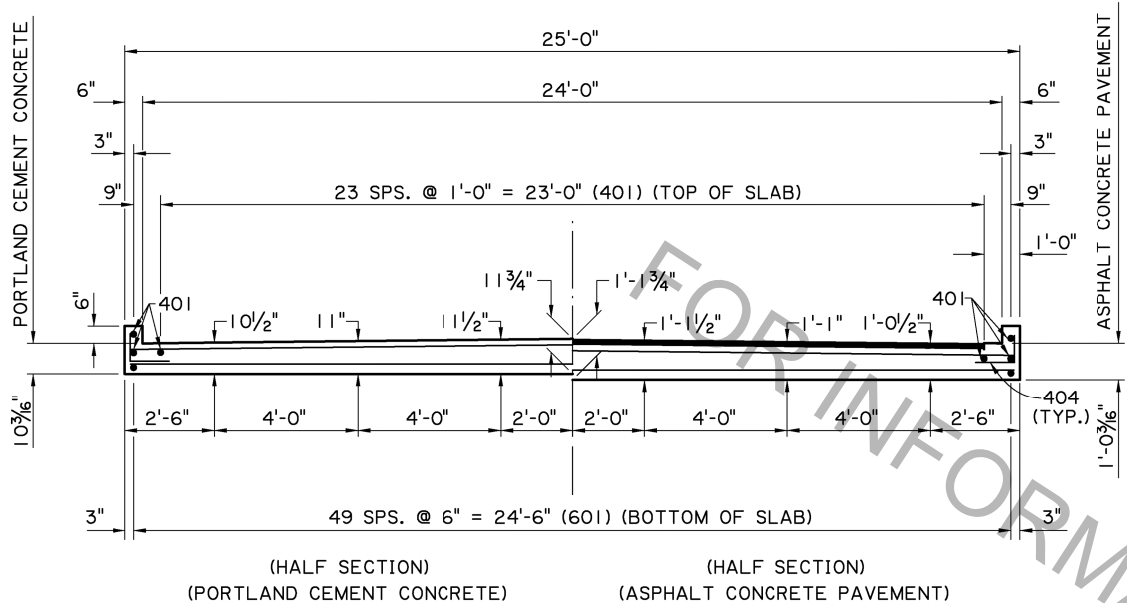
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
901	8	19'-8"	157'-4"	LONGIT. BOT. OF SLAB
902	1	18'-11"	18'-11"	LONGIT. BOT. OF SLAB
TOTAL NO. 9 BARS = 176'-3" = 599 LBS.				
801	1	1'-0"	1'-0"	DOWELS
TOTAL NO. 8 BARS = 1'-0" = 3 LBS.				
503	8	4'-9"	38'-0"	TOP & BOT. END OF SLAB
504	44	3'-9"	165'-3"	TRANS. BOT. OF SLAB
TOTAL NO. 5 BARS = 203'-0" = 212 LBS.				
401	4	19'-8"	78'-8"	LONGIT. TOP OF SLAB
402	17	3'-9"	63'-9"	TRANS. TOP OF SLAB
TOTAL NO. 4 BARS = 142'-5" = 95 LBS.				
DEFORMED REINFORCING STEEL = 909 LBS.				
CLASS P1 CONCRETE = 2.46 CU. YDS.				

○ BASED ON A 10" SLAB THICKNESS

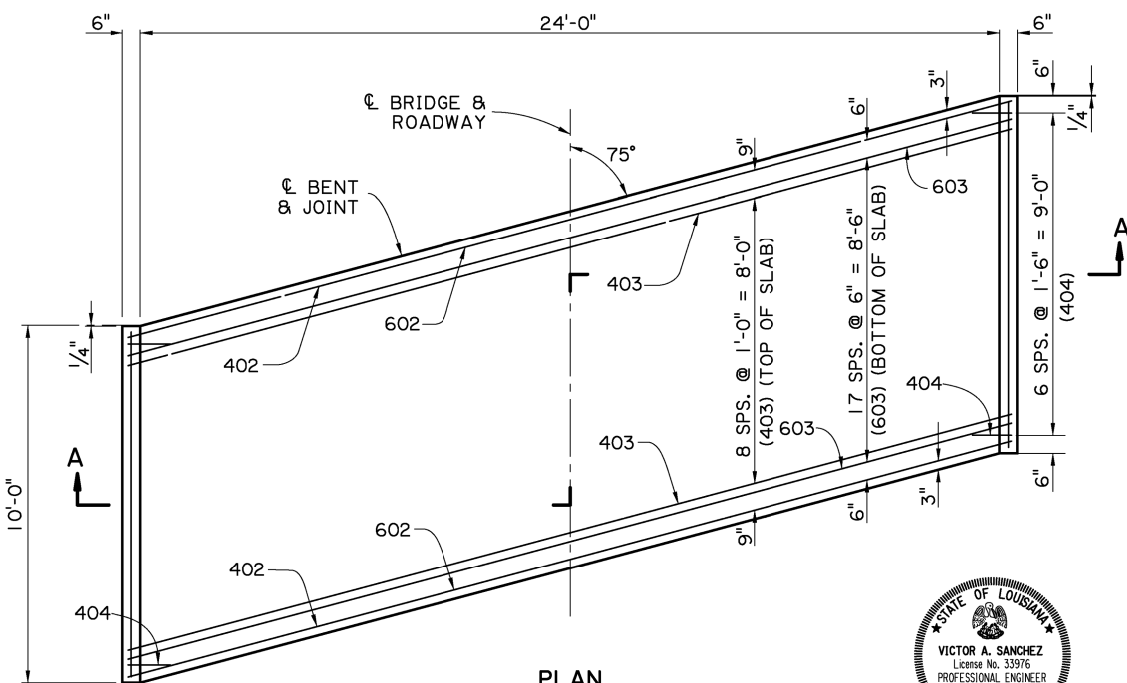


DETAIL "A"  
TYP. EXTERIOR EDGE ONLY  
SCALE: 1" = 1'-0"

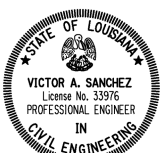




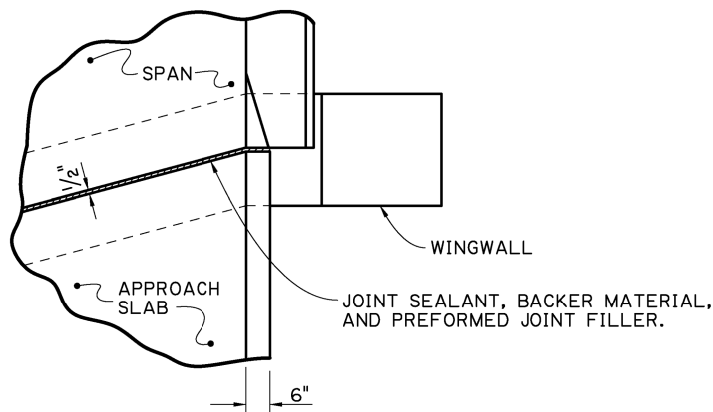
SECTION A-A  
SCALE: 3/8" = 1'-0"



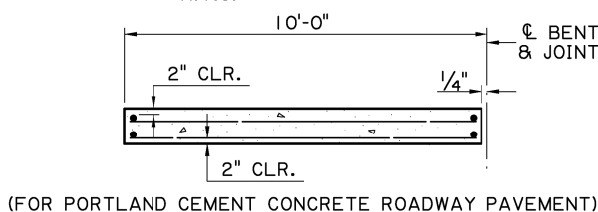
PLAN  
SCALE: 3/8" = 1'-0"



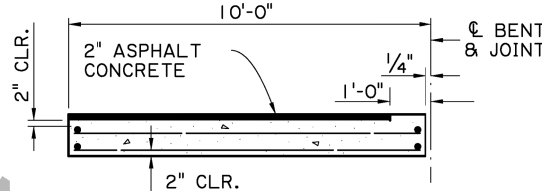
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JOINT DETAIL  
N.T.S.

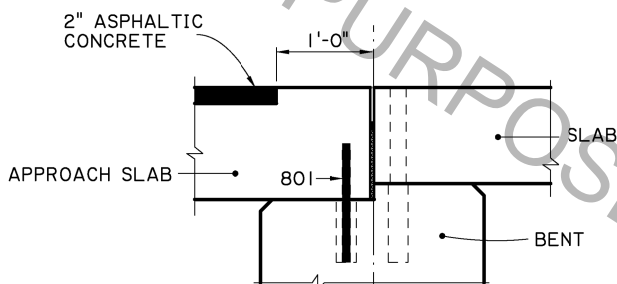


(FOR PORTLAND CEMENT CONCRETE ROADWAY PAVEMENT)



(FOR ASPHALT CONCRETE ROADWAY PAVEMENT)

SECTION ALONG C ROADWAY  
SCALE: 1/4" = 1'-0"

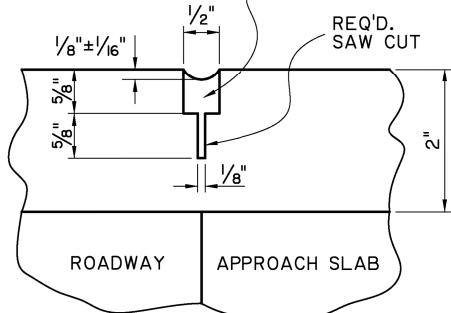


DETAIL A

SCALE: 1" = 1'-0"

(ASPHALT CONCRETE PAVEMENT OPTION)

HOT POURED SEALANT  
IN ACCORDANCE WITH  
SUBSECTION 1005.02



SAWING & SEALING  
JOINT DETAIL  
N.T.S.

ESTIMATED QUANTITIES (ONE SLAB)				
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
801	6	1'-0"	6'-0"	DOWELS
TOTAL NO. 8 BARS = 6'-0" = 16 LBS.				
601	50	9'-7"	479'-2"	LONGIT. BOT. OF SLAB
602	2	25'-5"	50'-10"	TRANSV. BOT. OF SLAB
603	18	25'-6"	459'-0"	TRANSV. BOT. OF SLAB
TOTAL NO. 6 BARS = 989'-0" = 1,485 LBS.				
401	28	9'-7"	268'-4"	LONGIT. TOP OF SLAB & CURB
402	2	25'-5"	50'-10"	TRANSV. TOP OF SLAB
403	9	25'-6"	229'-6"	TRANSV. TOP OF SLAB
404	14	1'-10"	25'-8"	DOWELS IN CURB
TOTAL NO. 4 BARS = 574'-4" = 384 LBS.				
TOTAL DEFORMED REINFORCING STEEL = 1,885 LBS.				
CONCRETE APPROACH SLAB = 27.78 SQ. YDS.				
ASPHALT CONCRETE = 2.5 TONS				
SAW CUT & SEAL = 24 LIN. FT.				

- TO BE PAID FOR UNDER ITEM CONCRETE APPROACH SLABS.  
REQUIRED WHEN APPROACH SLAB IS ADJACENT TO ASPHALT CONCRETE PAVEMENT.

APPROACH SLAB NOTES:

CONSTRUCTION SPECIFICATIONS: LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION, WITH 2008 & 2009 INTERIMS.

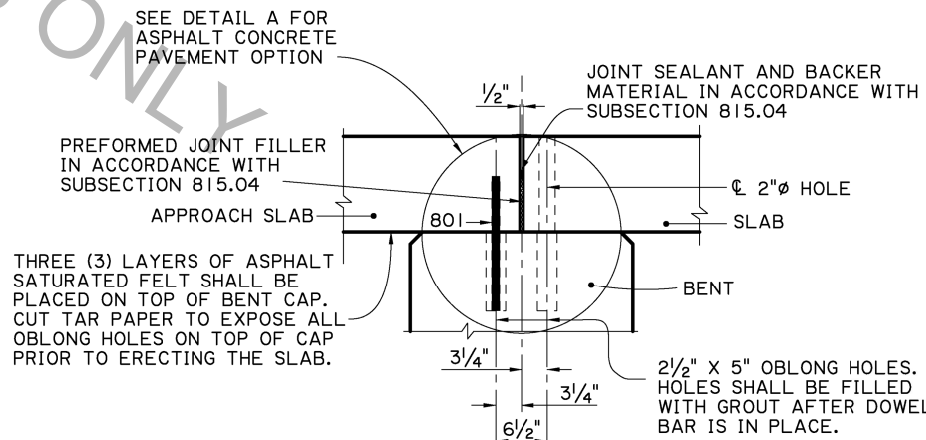
STRUCTURAL CONCRETE: ALL CONCRETE SHALL BE CLASS A1. EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER, UNLESS OTHERWISE NOTED.

REINFORCING STEEL: ALL REINFORCING STEEL SHALL BE GRADE 60. DIMENSIONS RELATING TO THE FABRICATION ARE OUT-TO-OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE NOTED.

BEDDING MATERIAL: FOR DETAILS OF BEDDING MATERIAL AND UNDERDRAINS, SEE STANDARD DETAIL BD.2.10.1.0.07.

SAWING & SEALING: THE ASPHALT CONCRETE SHALL BE SAW CUT AT THE END OF THE CONCRETE APPROACH SLAB THE ENTIRE ROADWAY WIDTH AND SEALED. COST TO BE INCLUDED WITH CONCRETE APPROACH SLABS.

BASIS OF PAYMENT: ALL MATERIAL SHALL BE PAID FOR UNDER 'CONCRETE APPROACH SLABS' ACCORDING TO THE SPECIFICATIONS.



TYPICAL JOINT DETAIL  
SCALE: 1" = 1'-0"

NOTE: FOR ADDITIONAL JOINT DETAILS SEE SHEET 2 OF 11.