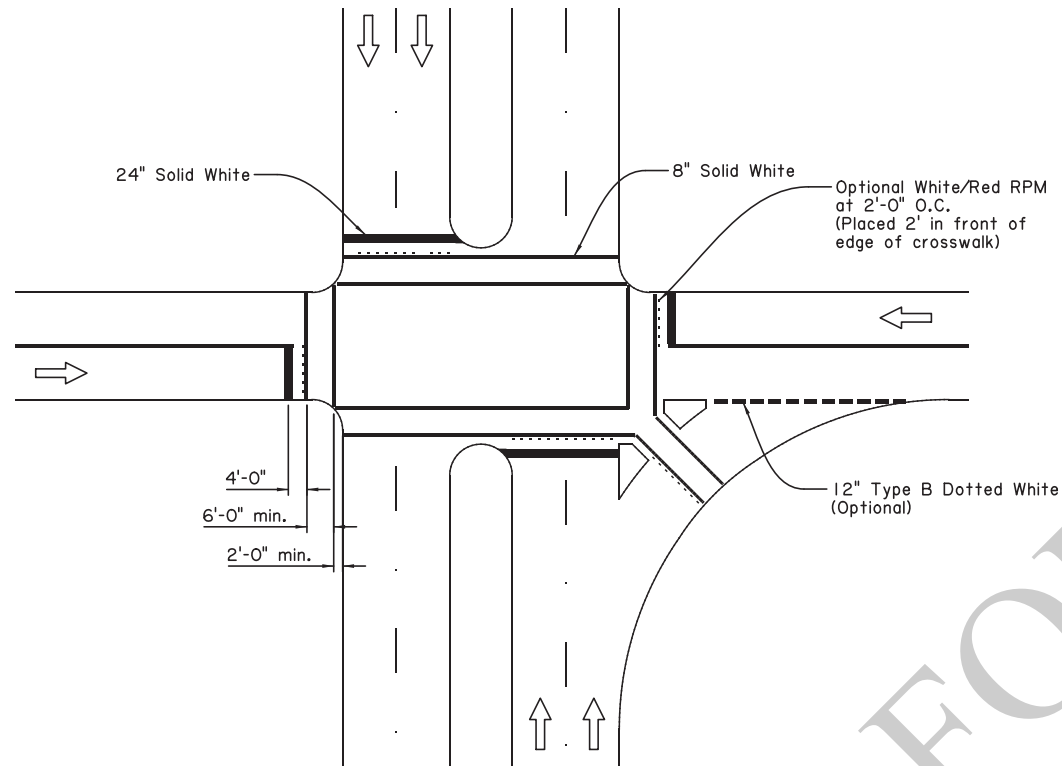


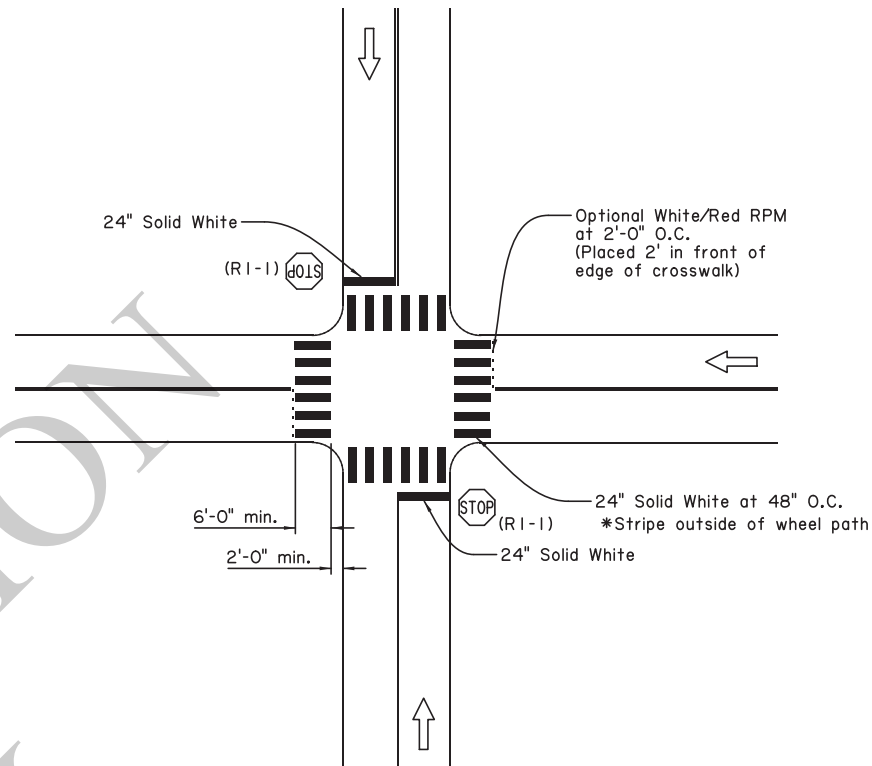
Ⓐ

CROSSWALK MARKINGS AT SIGNALIZED INTERSECTION



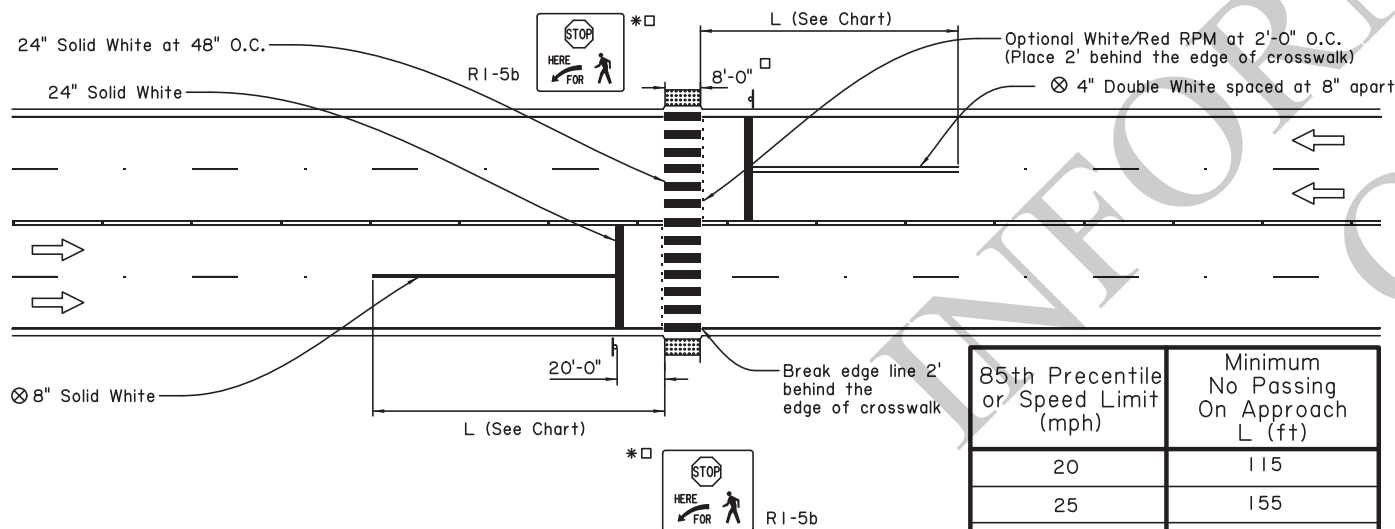
Ⓑ

CROSSWALK MARKINGS AT UNSIGNALIZED INTERSECTION



Ⓒ

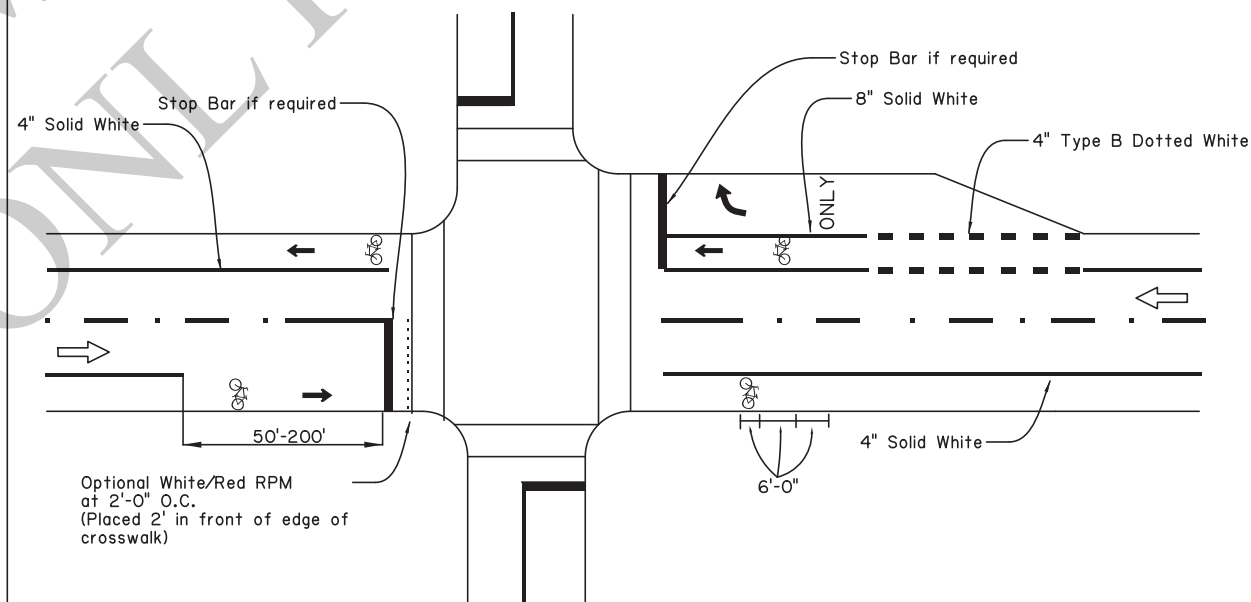
UNCONTROLLED MID-BLOCK CROSSWALK MARKINGS



- ⊗ At approaches to mid-block crossings, roadways with design speeds $x > 35$ mph shall use double 4" solid white lines as lane lines. Design speeds $x \leq 35$ mph shall use a single 8" solid white line.
- * If within a school zone, sign may be changed to the In Street Schoolchildren Crossing (R1-6c) sign.
- For shared use paths, R1-5e signs should be used on multi-lane approaches and crosswalks should at least match facility widths.

Ⓓ

BIKE LANE AT INTERSECTION WITH RIGHT TURN LANE



GENERAL NOTES:

1. If the width of the existing sidewalk is greater than 6 feet, the width of the crosswalk should match the width of the sidewalk, except at midblock crossings where the crosswalk width shall be 8' minimum.
2. ⇨ indicates direction of travel (not a pavement marker).
3. Bicycle pavement markings should be placed at the beginning of bike lanes, before and after each intersection with a minimum spacing of 100 ft in urban areas to a maximum of 1000 feet in suburban areas.

SHEET NUMBER		PARISH		CONTROL SECTION		STATE PROJECT	
DESIGN	B. MASON	CHECK	B. BOUCHER	DETAIL	B. MASON	CHECK	B. BOUCHER
REVIEW	REVIEW	REVIEW	REVIEW	REVIEW	REVIEW	REVIEW	REVIEW
APPROVED BY CHIEF ENGINEER:		DATE:		BY:		NO.	
3/6/2024							
REVISION OR CHANGE ORDER DESCRIPTION							
PAYEMENT MARKING DETAILS Pedestrian/Bike Striping Layout PM-08							
STANDARD PLAN							