



# US 11 Norfolk Southern Railroad Environmental Assessment Route US 11

State Project No. H.000688.2  
Federal Aid Project No. H000688  
St. Tammany Parish, Louisiana

August 2022



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FEDERAL HIGHWAY ADMINISTRATION  
**FINDING OF NO SIGNIFICANT IMPACT**

For

State Project No. H.000688.2  
Federal Aid Project No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish

The Federal Highway Administration (FHWA) has determined Alternative 1R Modified, Bridge Alternative 1 (the Selected Alternative) will not have any significant impact on the human environment. This Finding of No Significant Impact (FONSI) is based on the Environmental Assessment (EA), which has been independently evaluated by the FHWA and determined to adequately and accurately discuss the environmental issues and impacts of the proposed project. It provides sufficient evidence and analysis for determining that an environmental impact statement is not required. The FHWA takes full responsibility for the accuracy, scope, and content of the attached EA.



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Date

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Daniel Suarez  
Project Delivery Team Leader  
Federal Highway Administration  
Louisiana Division

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# EXECUTIVE SUMMARY

## Description of the Proposed Project

The Louisiana Department of Transportation and Development (LADOTD) and the Federal Highway Administration (FHWA), in cooperation with the New Orleans Regional Planning Commission (RPC), propose to replace the US Highway 11 (US 11) bridge over the Norfolk Southern (NS) railroad and widen US 11 from two lanes to four lanes from north of the intersection of Lafayette Street and US 11 to Interstate 12 (I-12) in Slidell, St. Tammany Parish, Louisiana. The RPC is the designated Metropolitan Planning Organization for the New Orleans urbanized area including St. Tammany Parish.

The proposed project will be programmed through construction within LADOTD's On-System Bridge Program.

The study of the alternatives developed in this environmental assessment and the associated environmental consequences were evaluated according to the National Environmental Policy Act (NEPA); LADOTD's Stage 1 Planning/Environmental Manual of Standard Practice; and FHWA Guidance for Preparing and Processing Environmental and Section 4(f) Documents.

## Study Area

The Study Area is located in the southeastern portion of St. Tammany Parish and extends south along US 11 from Powell Drive to the intersection of US 11 at Florida Avenue, a distance of approximately 1.7 miles, and includes the logical termini and the area that may be impacted by the indirect and cumulative impacts from the proposed project (Figure ES-1). The proposed construction limits extend from Powell Drive south a distance of approximately 1.4 miles, terminating approximately 225 feet north of Lafayette Street.

## Project Purpose

The purpose of the project is to address safety features of the bridge crossing the NS railroad and system reliability of the roadway segment from US 190 north to Powell Drive.

## Project Need

The project is needed in order to upgrade the bridge and roadway segment to current design standards for travel lane and shoulder widths, bridge width, stopping sight distances, and design speed, as well as system reliability. Roadway segment and bridge improvements will also provide better accommodations for bicycle users and will serve the surrounding community and the larger metropolitan area. More specifically, needs for the proposed project include:

- Address the safety features of the US 11 bridge crossing over the NS railroad;
- Improve capacity;
- Improve travel time;
- Relieve congestion on US 11 by removing the bottleneck between US 190 and I-12;
- Relieve future congestion on area roadways; and
- Improve area-wide mobility and system reliability.

## Alternatives Development

Early coordination with federal, state, and local agencies as well as NS solicited comments and responses that were combined with available environmental data. This information was used to help determine if the preliminary alternatives impact certain human, natural, or cultural

## ENVIRONMENTAL ASSESSMENT

resources that would result in the decision to dismiss an alternative from further evaluation. This coordination resulted in the dismissal of an at-grade crossing as a viable alternative.

To minimize impacts and reduce the amount of right-of-way to be acquired, alignments were located as close to the existing US 11 roadway as LADOTD design standards and project construction limits would allow. Policies such as roadway and bridge design, intersection configuration, traffic, noise, and minimization of social and environmental impacts were also considered in the alternatives development. Requirements for horizontal and vertical clearances for the railroad crossing were evaluated, and cost and constructability factors were also considered. The roadway segments north and south of the bridge follow the existing roadway alignment providing widening and improvements to accommodate a four-lane divided highway. This resulted in one alternative for the roadway segments, identified as Alternative 1. Additional bridge and intersection configurations were also developed.

Potential alignment revisions were reviewed and incorporated into the preliminary alignments following the officials and public meetings held on August 22, 2013. Suggestions were considered to shift the roadway segment east on the north side of the bridge in order to reduce impacts to residential and commercial properties.

The public expressed a need for bicycle and/or pedestrian facilities within the Study Area. Currently, the City of Slidell does not have a master plan that includes bicycle and/or pedestrian improvements along US 11. Bicycle and pedestrian improvements were evaluated in accordance with LADOTD's Complete Streets Policy and in coordination with the City of Slidell and St. Tammany Parish.

A revision to Alternative 1, Bridge 1 resulted in Alternative 1 Revised (Alternative 1R) and included a roadway centerline shift to the east on the north side of the US 11 bridge, which continued-along this new alignment toward North Boulevard. A right-turn lane was added from the

northbound lanes at North Boulevard — with two through lanes continuing north that shift back to the west — in order to meet recent roadway widening improvements at US 11 and Powell Drive.

Following a December 15, 2015, officials meeting and public hearing, potential modifications to surface streets accessing the Addis Boulevard neighborhood were reviewed along with elimination or revision of proposed intersection configurations.

The City of Slidell expressed concern over the proposed improvements making through traffic

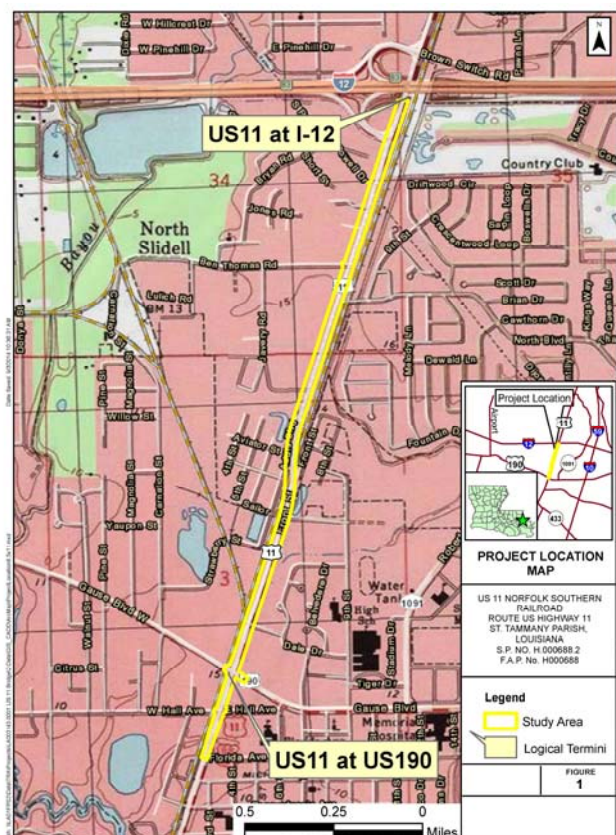


Figure ES-1: Study Area and Location Map

more efficient. This result is inconsistent with the City's objectives to slow traffic and re-connect neighborhoods with the Slidell business district. This was of particular concern from Gause Boulevard south to Hall Avenue.

The City also expressed concern over the Addis Boulevard neighborhood access including the proposed connection from Strawberry Street to

Carnation Street, which would allow access to Gause Boulevard to the south or Ben Thomas Road to the north. The addition of traffic at the Gause Boulevard-Carnation Street intersection and increase in congestion were a concern.

Additional comments that were received expressed concern regarding proposed intersection improvements and associated turning movements along the corridor.

Upon further review of comments received from the public hearing and subsequent coordination with the City of Slidell, a revision to Alternative 1R resulted in Alternative 1R Modified (Selected Alternative). This alternative includes reducing construction limits by beginning approximately 225 feet north of the intersection of US 11 with Lafayette Street and terminating at US 11 and Powell Drive. Improvements south of Lafayette Street are no longer included under the Selected Alternative. Additional improvements include a roundabout at Powell Drive, Addis Boulevard realignment, U-turns, and turn lanes.

### Bridge Rehabilitation

Following early agency coordination, the bridge over the NS railroad was identified as eligible for the National Register of Historic Places (NRHP). The LADOTD in cooperation with the FHWA and the State Historic Preservation Office completed a statewide historic bridge inventory for bridges constructed prior to 1971. A *National Register Eligibility Documentation Report* (September 2013) was prepared by Mead & Hunt. The FHWA made final NRHP eligibility determinations, which are presented in the Mead & Hunt report, and the SHPO has concurred with those determinations.

The NRHP employs four criteria for evaluation in determining eligibility: A, B, C, and D. This bridge was determined eligible under Criterion A Events, which applies to structures that have an important association with single events, a pattern of events, repeated activities, or historic trends that are significant within the context of Louisiana's transportation and bridge-building history.

The bridge is also potentially eligible for protection under Section 4(f) of the *Department of Transportation Act of 1966* (Section 4(f)) and Section 106 of the National Historic Preservation Act (Section 106).

Bridge rehabilitation was included in the list of preliminary alternatives as the only potential means of preserving the bridge. Rehabilitation to the original condition without changing the 1930s bridge design features such as lane widths and lack of shoulders was one form of rehabilitation considered. However, to remain in service, the bridge deficiencies related to its age and design must be addressed.

Rehabilitation that maintains the existing bridge would not sufficiently address structural and functional deficiencies of the bridge. Current LADOTD roadway design criteria cannot be met. Therefore, rehabilitation of the existing bridge was dismissed from further consideration.

## Alternatives Comparison

### Resource Impact Analysis

Alternatives were evaluated with respect to environmental resources and engineering factors. No impacts were identified for churches, public facilities, cemeteries, farmlands, archaeological resources, or oil and gas wells. Table ES-1 provides a summary of resources that will be impacted, and Table ES-2 provides preliminary cost analyses for the alternatives.

### Selected Alternative

As a result of the comprehensive resources evaluation, traffic studies, and coordination with public, local, state, and federal officials or agencies, sufficient information and public opinion exist to identify Alternative 1R Modified, Bridge 1 as the Selected Alternative (Figures ES-2 through ES-12). This alternative includes the widening of US 11 from Powell Drive south a distance of approximately 1.4 miles, terminating approximately 225 feet north of Lafayette Street, and replacement of the existing NS bridge with one situated along the existing bridge alignment. This alternative also includes the addition of a

## ENVIRONMENTAL ASSESSMENT

roundabout at Powell Drive to allow U-turn movements for large commercial trucks as well as adding a dual right-turn lane and signal from the I-12 eastbound off-ramp at US 11. The Selected Alternative also includes an optional complete streets configuration with 4-foot shoulders on both sides of US 11 and a 7-foot sidewalk on the east side of US 11. The Selected Alternative is shown on Figures ES-2 through ES-12. The design report is included on Figures ES-13 through ES-15.

The identification of the Selected Alternative addresses the stated purpose and need and satisfies, to the fullest extent possible, the objectives of NEPA. Impacts from the Selected Alternative were avoided where possible and minimized to the greatest extent practicable.

Project implementation will include permits, mitigation, and commitments that will be incorporated into the design and construction phases of the project:

- Louisiana Department of Natural Resources Coastal Use Permit
- U.S. Army Corps of Engineers Wetland Permit
- Louisiana Department of Environmental Quality National Pollutant Discharge Elimination System Permit for Construction-Related Activities
- Traffic Control
- Property Access
- Section 401 Water Quality
- Bicycle/Pedestrian Facilities (with optional complete streets configuration)
- Relocation

**Table ES-1: Alternatives Evaluation Matrix**

Evaluation Factors	Alternative 1, Bridge 1	Alternative 1R, Bridge 1	Selected Alternative (Alternative 1R Modified)	No Build
<b>Structure Impacts</b>				
Residence	0	0	0	0
Business	13	13	5	0
Church	0	0	0	0
Public Facility	0	0	0	0
Underground Storage Tanks/Pumps/	6	6	0	0
Signage	11	11	6	0
Parking Spaces	107	107	25	0
Potential Relocations	13	13	3	0
Noise	10	10	5	5
<b>Natural Resources Impacts</b>				
Cemetery	0	0	0	0
100-Year Floodplain (acres)	20	20	21	0
Surface Waters (acres)	<1	<1	<1	0
Wetland (acres)	<1	<1.1	<2	0



Evaluation Factors	Alternative 1, Bridge 1	Alternative 1R, Bridge 1	Selected Alternative (Alternative 1R) Modified)	No Build
<b>Natural Resources Impacts</b>				
Prime Farmland (acres)	0	0	0	0
Archaeological	0	0	0	0
Historic Resources > 50 Years Old	23	23	23	0
Eligible Historic Resources—Impacted	2	2	1	0
Known Underground Storage Tank Sites	3	3	3	0
Water Wells	3	3	3	0
Oil and Gas Wells	0	0	0	0
<b>Traffic Impacts</b>				
Permanent Road Closures	2	2	1	0
Temporary Detour	1	0	0	0

**Table ES-2: Alternatives Cost Analysis**

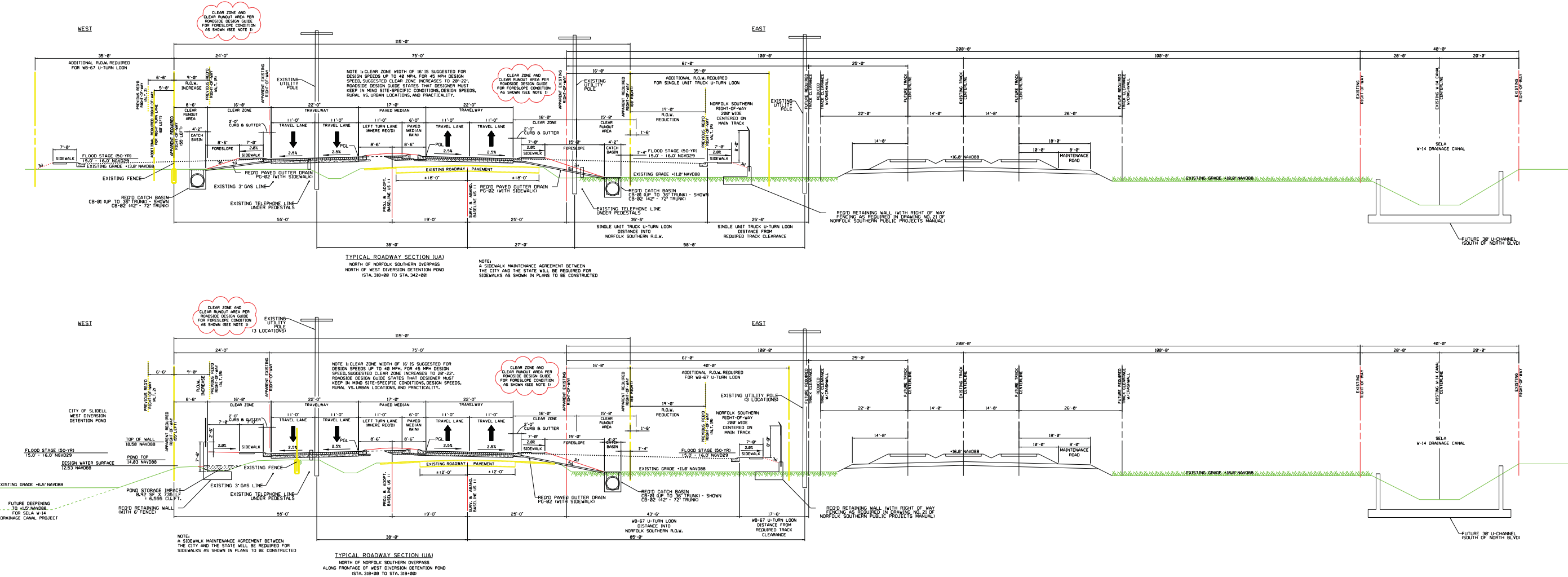
Evaluation Factors	Alternative 1, Bridge 1	Alternative 1R, Bridge 1	Selected Alternative (Alternative 1R ) Modified)	No Build
Total Length (miles)	1.7	1.7	1.4	0
<b>Cost (million dollars)</b>				
Roadway Construction	19	19	13.6	0
Roundabout Construction	-	-	1.9	0
Bridge Construction	48	48	48.2	0
Utility Relocation	0.505	0.505	0.405	0
Right-of-Way Acquisition	7.1	6.8	3.7	0
Wetland Mitigation	0.16	0.16	0.20	0
Surveying, Engineering, Construction Supervision, and Inspection	3.8	3.8	3.8	0
<b>TOTAL</b>	<b>78</b>	<b>78</b>	<b>71.8</b>	<b>0</b>

Includes 20% Roadway and 10% Bridge Contingency for Estimating Purposes.  
Costs Rounded.

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FOR REVIEW ONLY

ARCADIS

ENGINEER: Toby D. Picard  
LICENSE #: LA29911  
DATE: 6/5/2019

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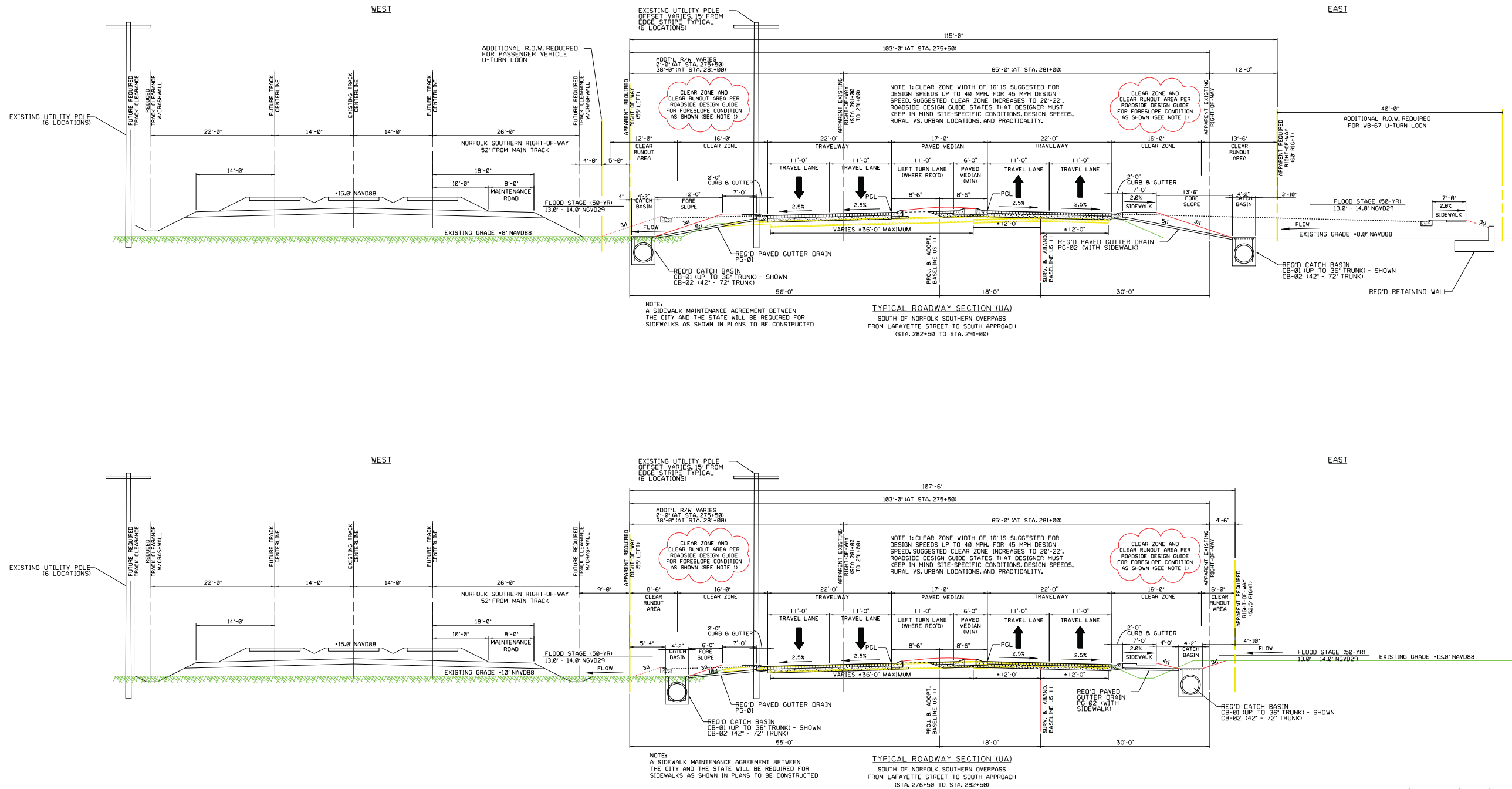
FIGURE ES-2


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**FIGURE ES-3**

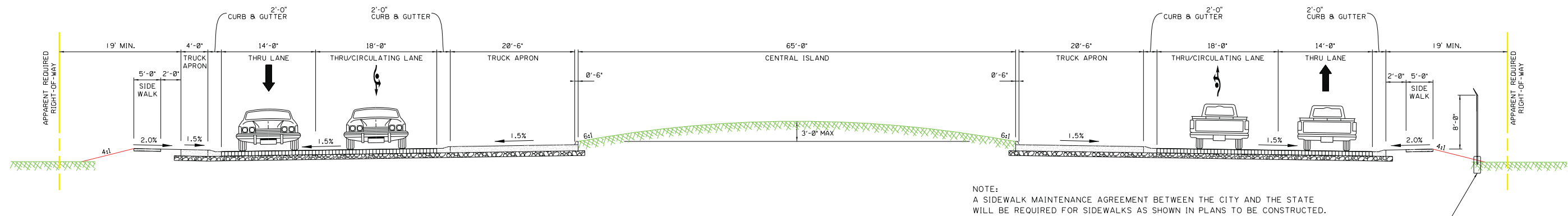
					
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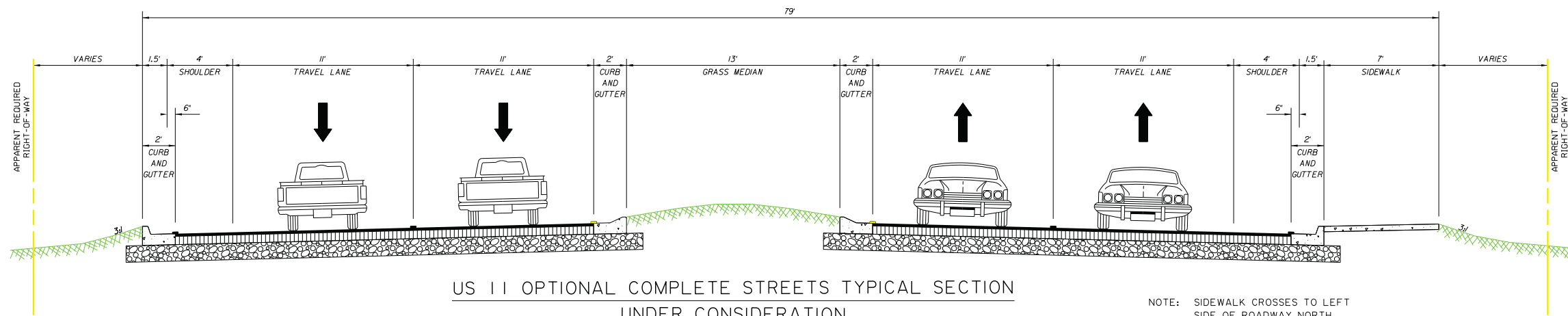


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TYPICAL ROUNDABOUT SECTION  
US 11 AT POWELL INTERSECTION



US 11 OPTIONAL COMPLETE STREETS TYPICAL SECTION  
UNDER CONSIDERATION  
(SUBJECT TO AGREEMENT & LOCAL SUPPORT)

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LICENSE #: LA29911  
DATE: 6/5/2019

FIGURE ES-4

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H.000688.2	STATE PROJECT
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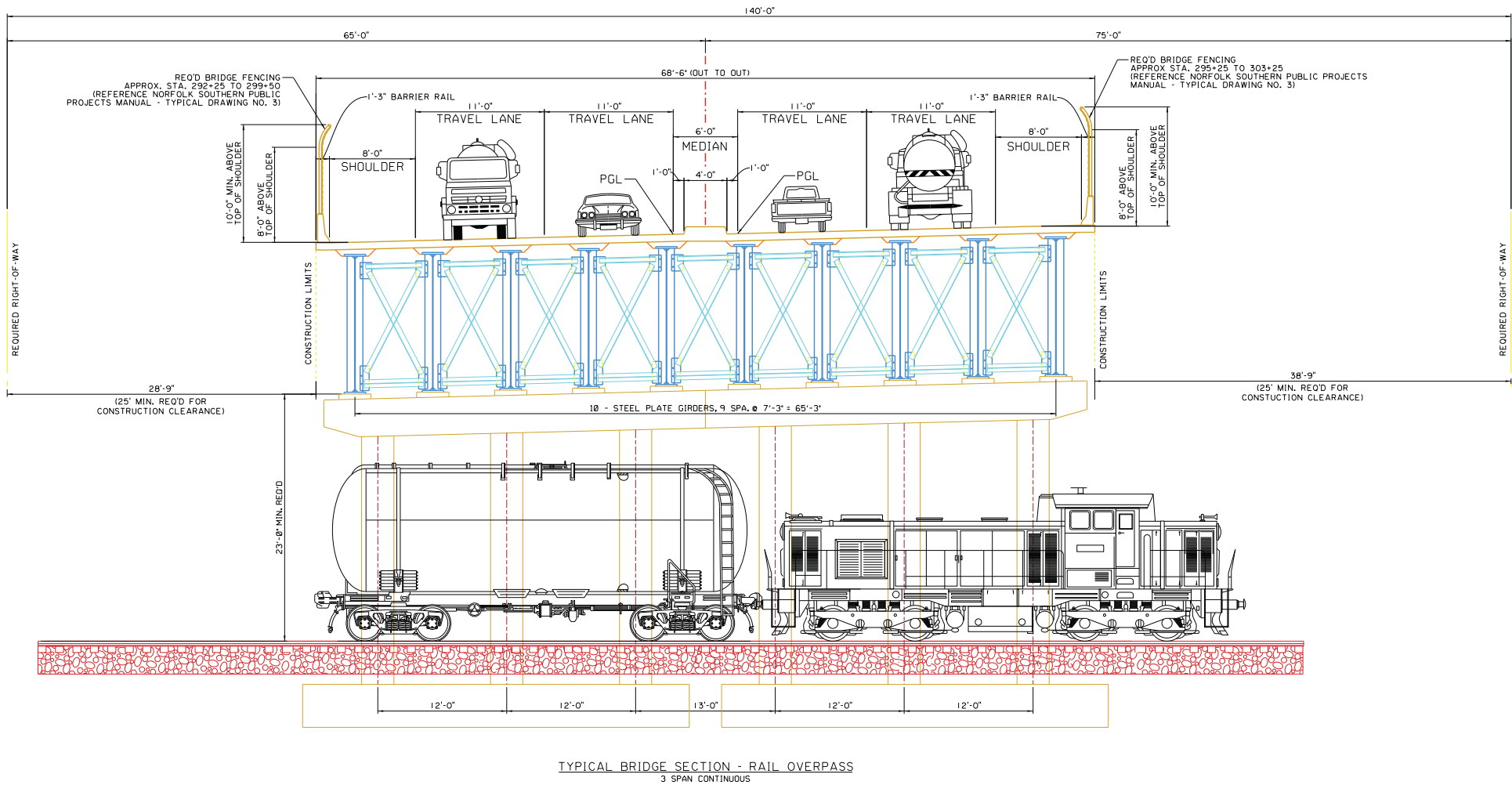
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FIGURE ES-5

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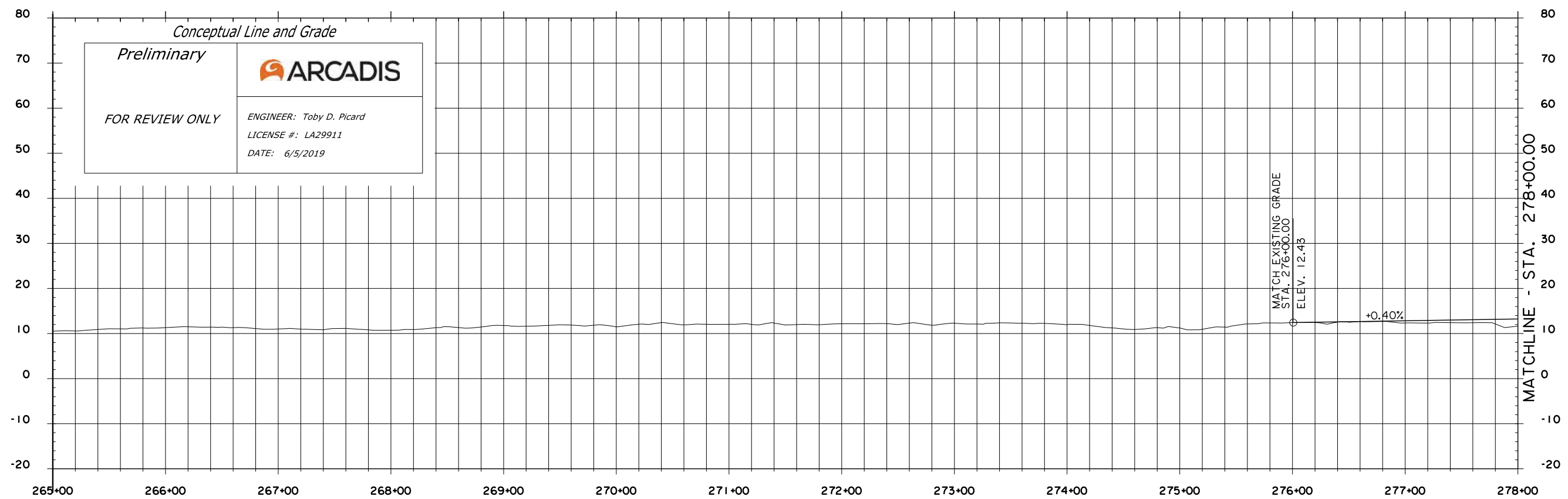
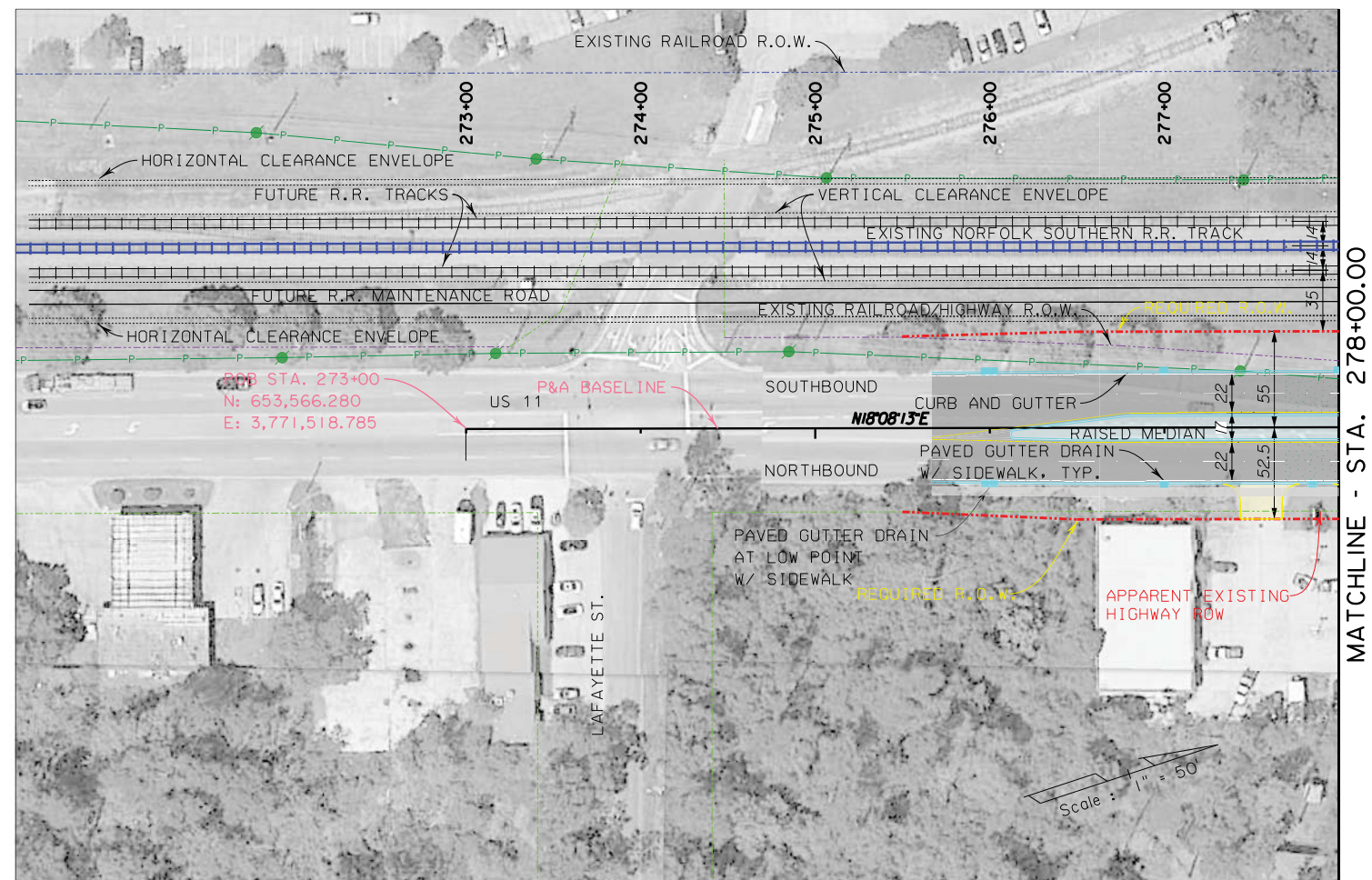


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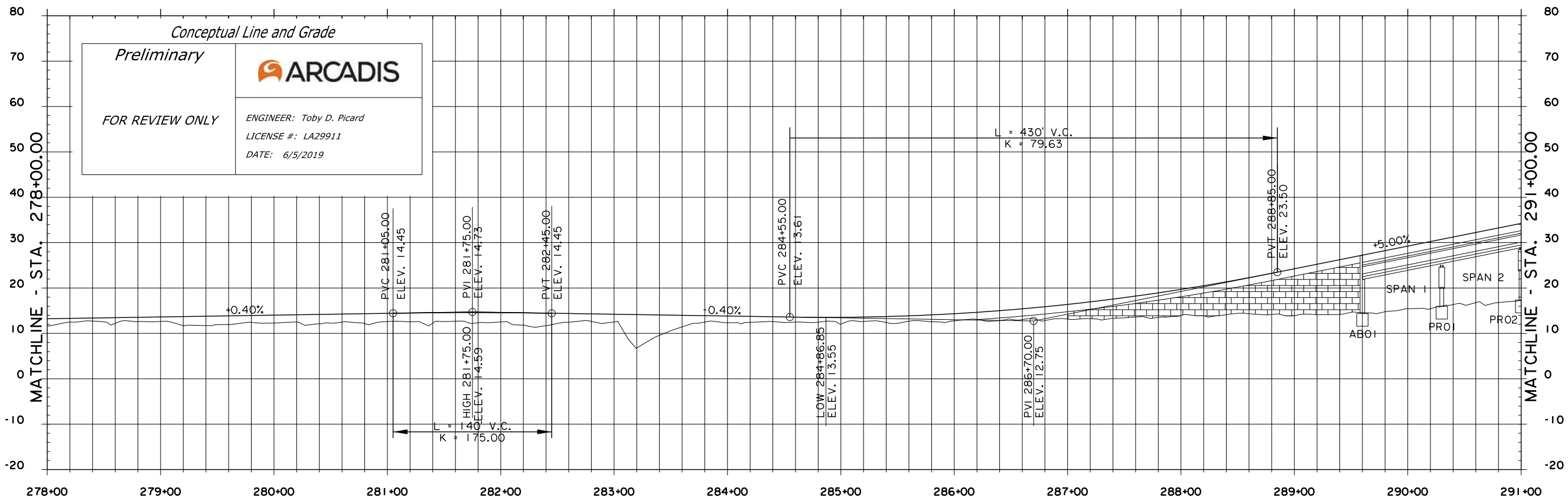
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US 11 NORFOLK SOUTHERN RAILROAD BRIDGE	NO.
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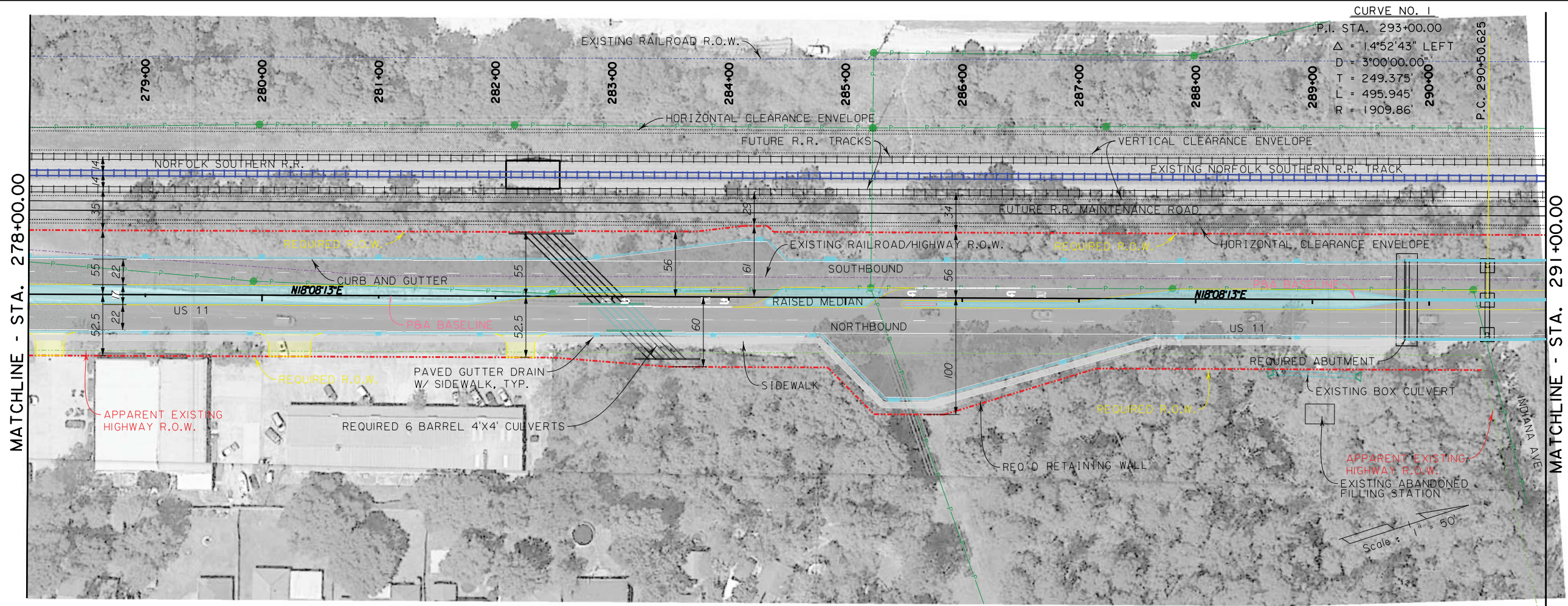
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FIGURE ES-7



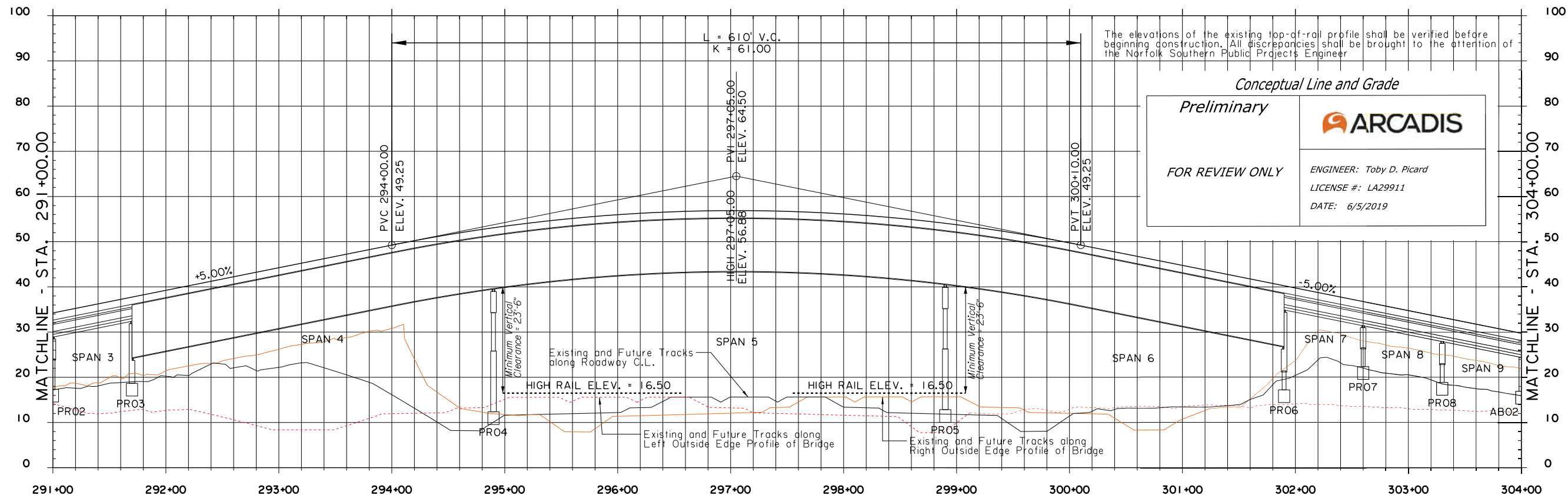
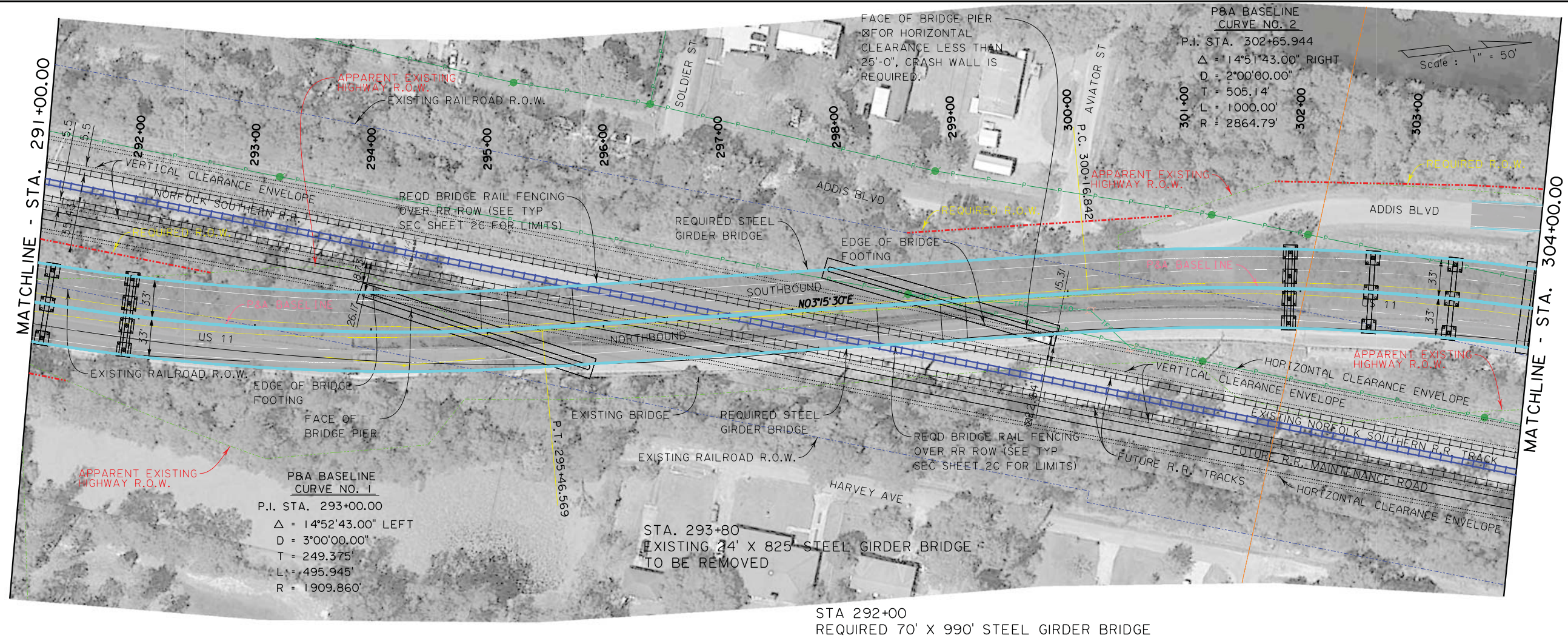
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PRELIMINARY AND SUBJECT TO CHANGE

FIGURE ES-8

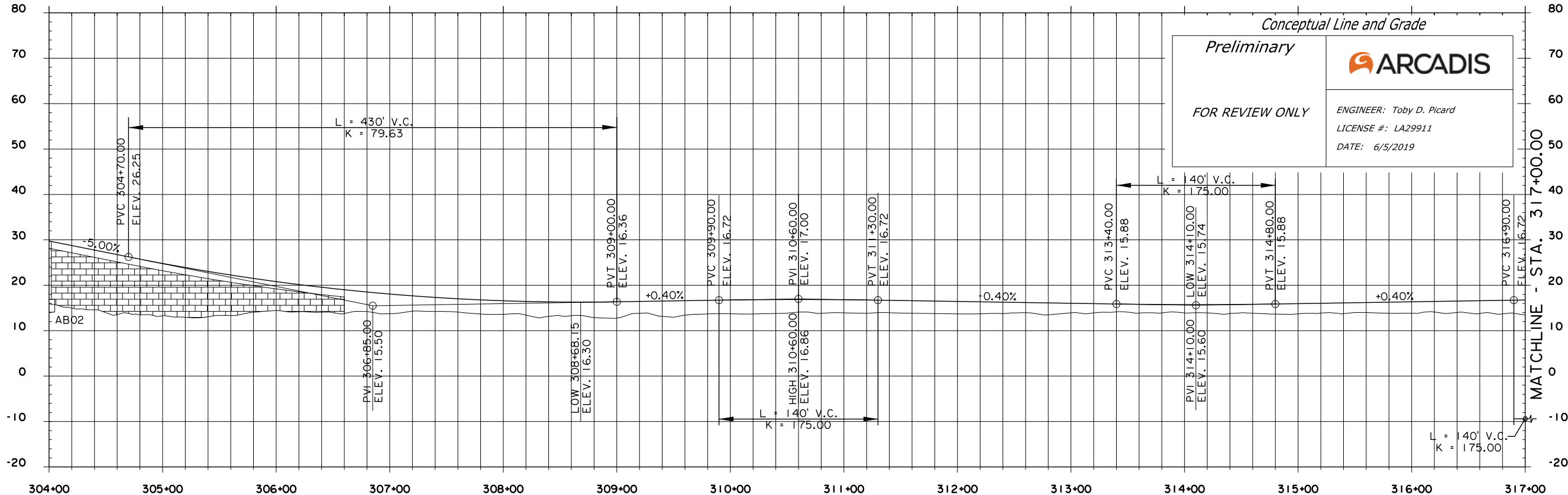
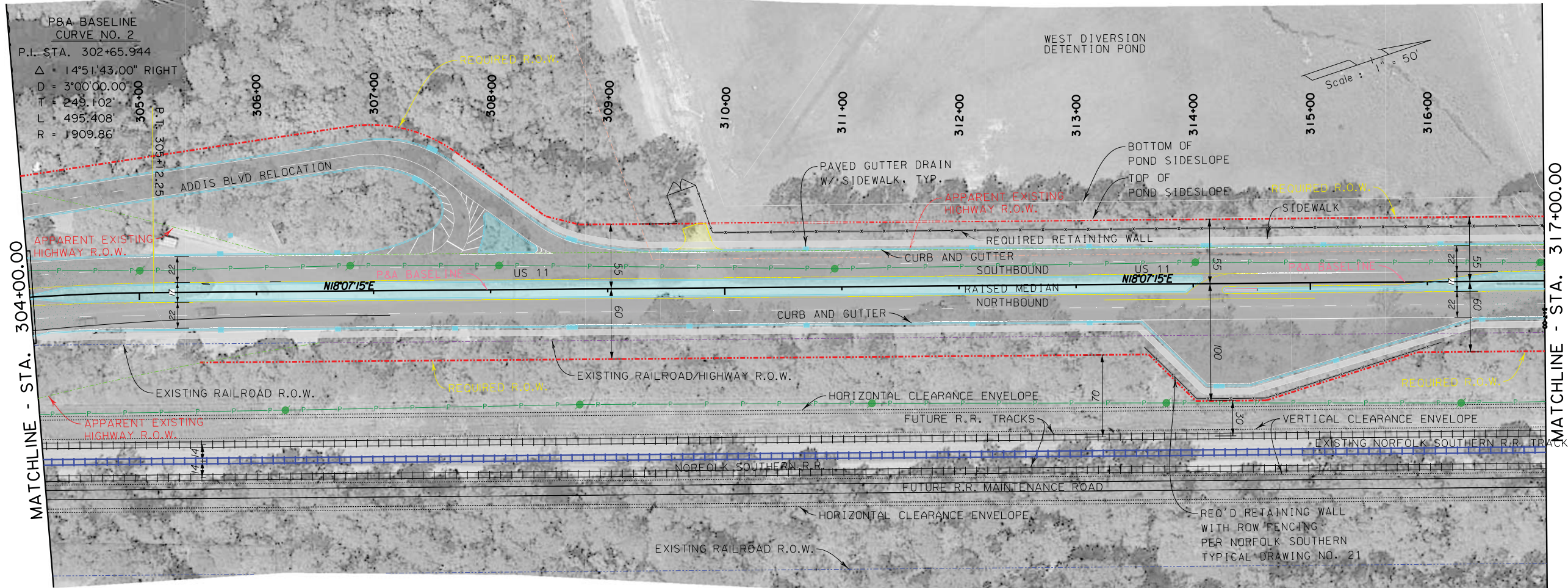
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CHECKED	L. PORTA	CONTROL SECTION	018-04
CHECKED	D. FULKS	STATE PROJECT	H.000688.2
REVISION OR CHANGE ORDER DESCRIPTION		BY	
NO.		DATE	
PLAN AND PROFILE ALTERNATIVE NO. 1R MODIFIED STA. 291+00 TO STA. 304+00 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE			

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PRELIMINARY AND SUBJECT TO CHANGE

FIGURE ES-9

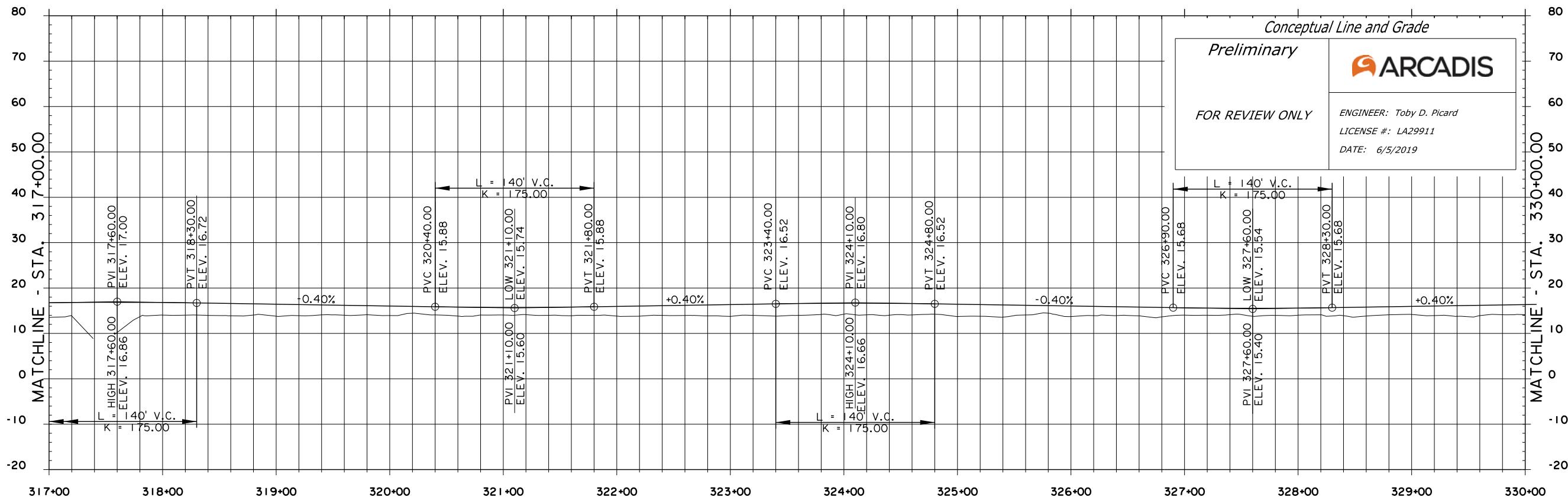
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PARISH	ST. TAMMANY
CONTROL SECTION	018-04
CHECKED	D. FULKS
STATE PROJECT	H.000688.2
SERIES NUMBER	
BY	
REVISION OR CHANGE ORDER DESCRIPTION	
NO.	DATE
PLAN AND PROFILE	
ALTERNATIVE NO. 1R MODIFIED	
STA. 304+00 TO STA. 317+00	
US 11 NORFOLK SOUTHERN RAILROAD BRIDGE	
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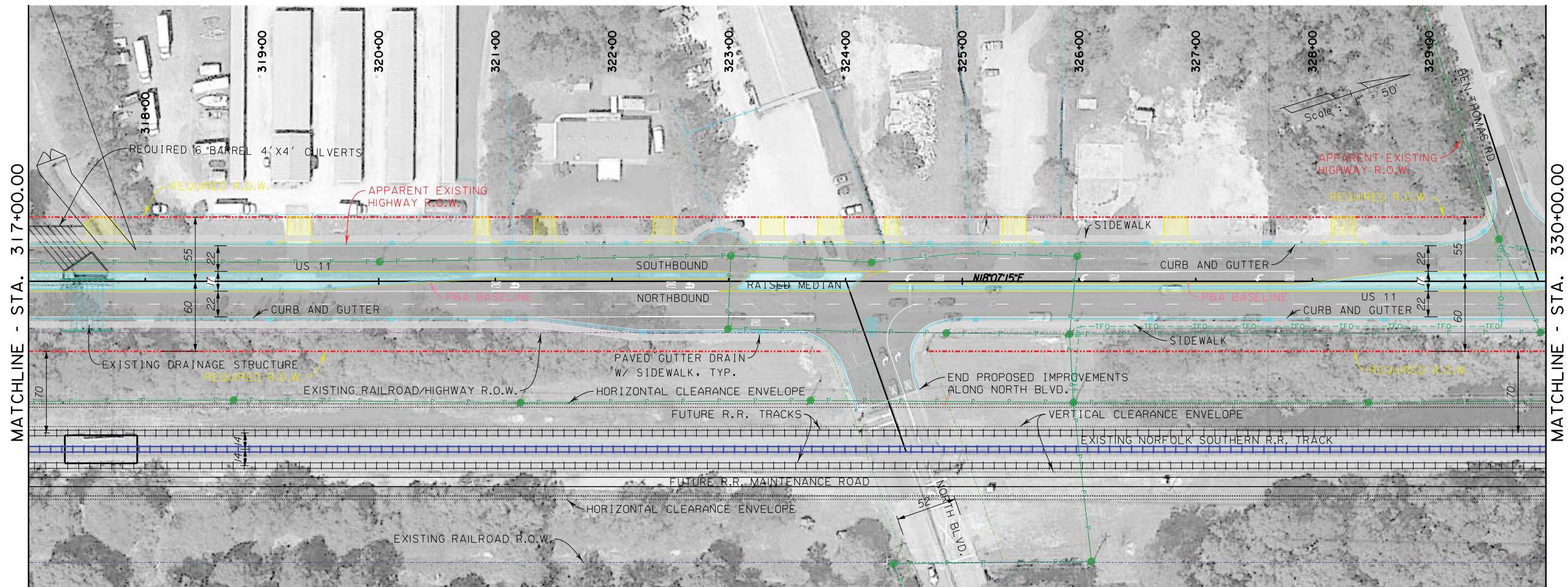
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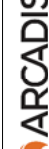

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PRELIMINARY AND SUBJECT TO CHANGE

FIGURE ES-10



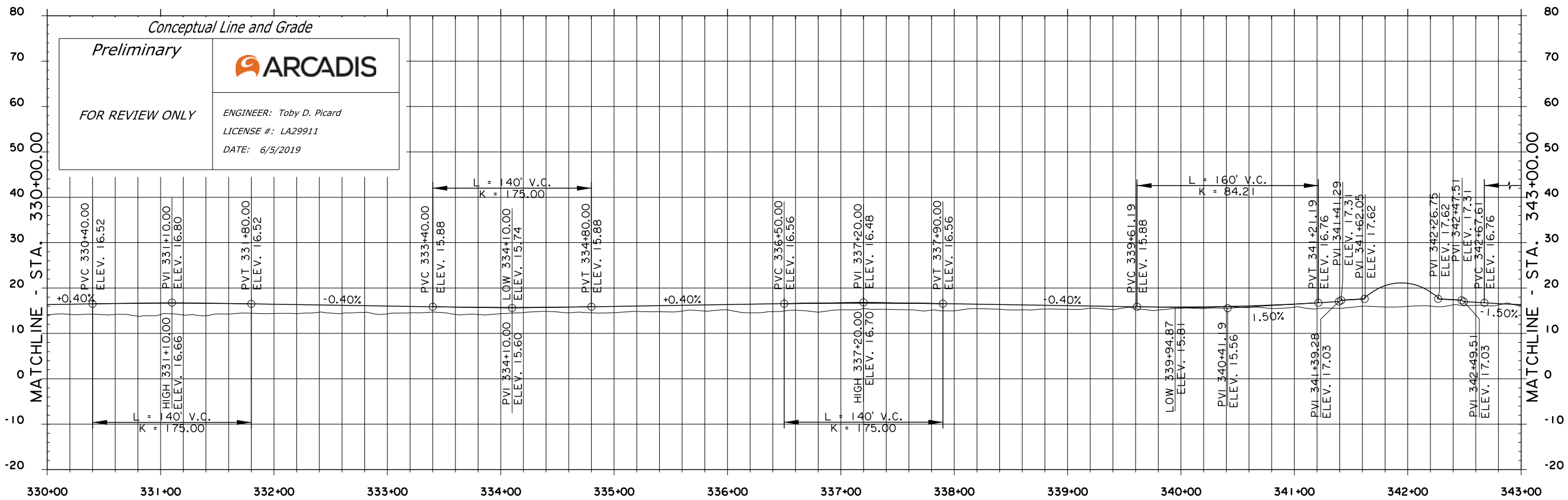
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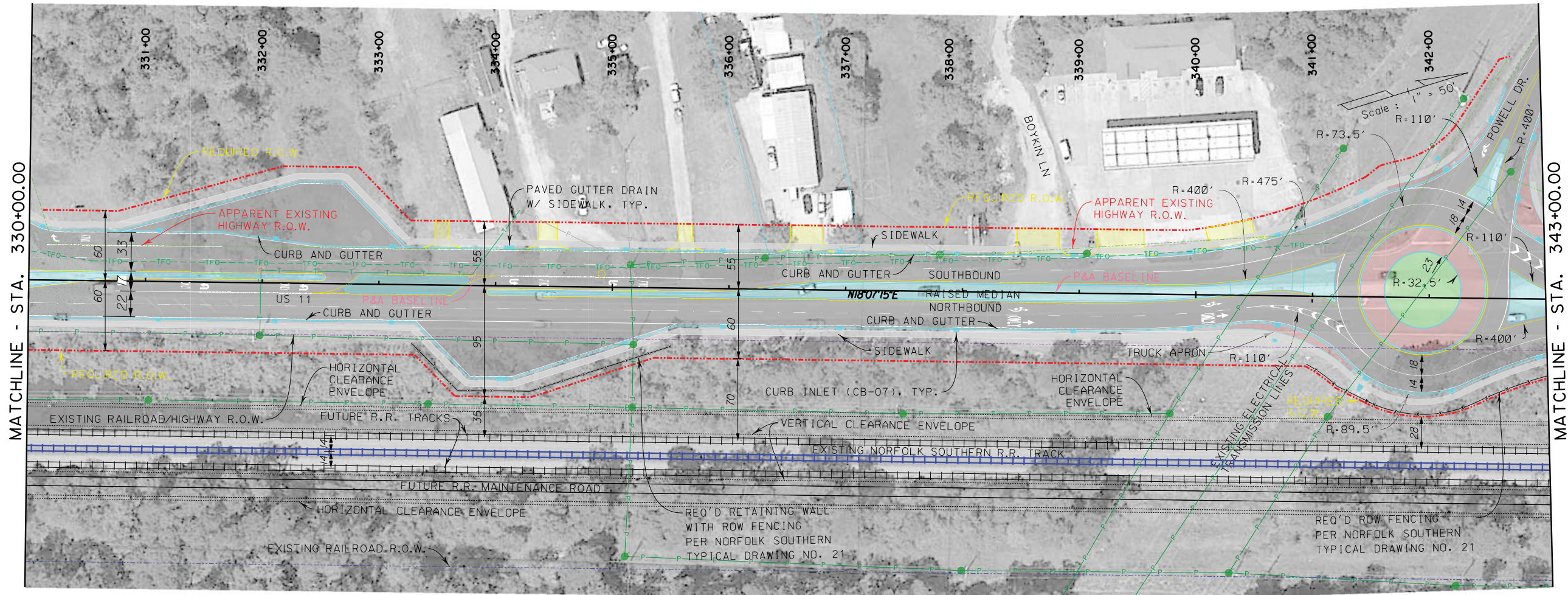
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PRELIMINARY AND SUBJECT TO CHANGE

FIGURE ES-11



SHEET NUMBER	ST. TAMMANY			PARISH	CONTROL SECTION	O 18-04	STATE PROJECT	H.000688.2
	DESIGNED: FULKS/ PICARD CHECKED: L. PORTA DETAILED: J. HOWARD CHECKED: D. FULKS							
REVISION OR CHANGE ORDER DESCRIPTION		NO.	DATE	BY				
PLAN AND PROFILE ALTERNATIVE NO. 1R MODIFIED STA. 330+00 TO STA. 343+00 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE								
 								

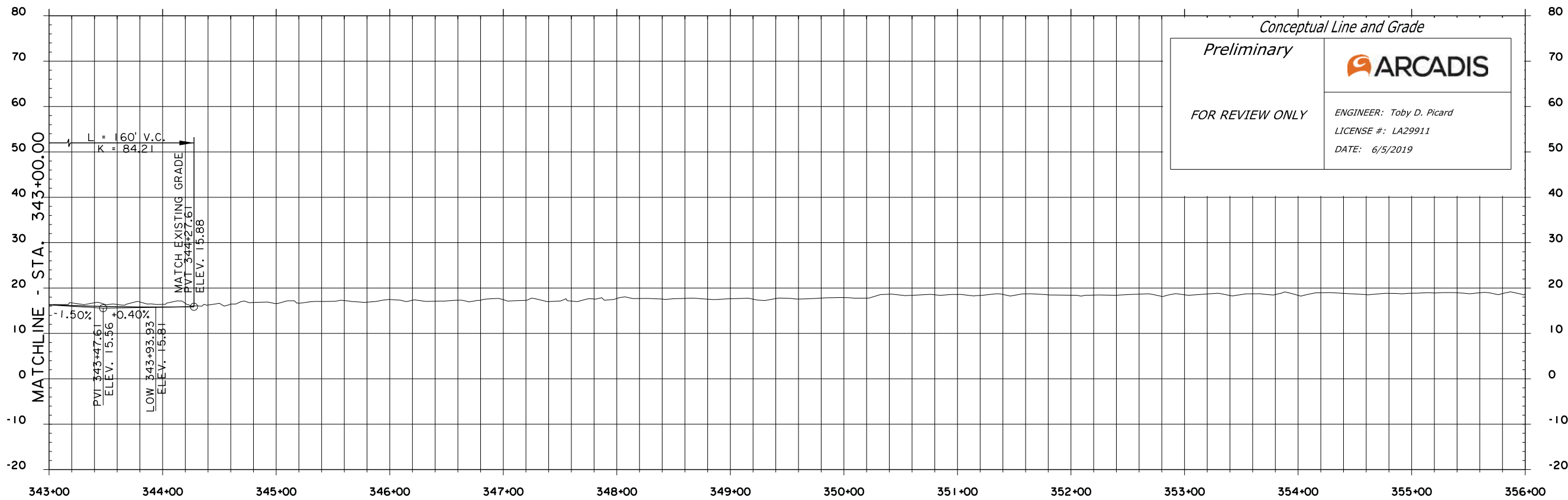
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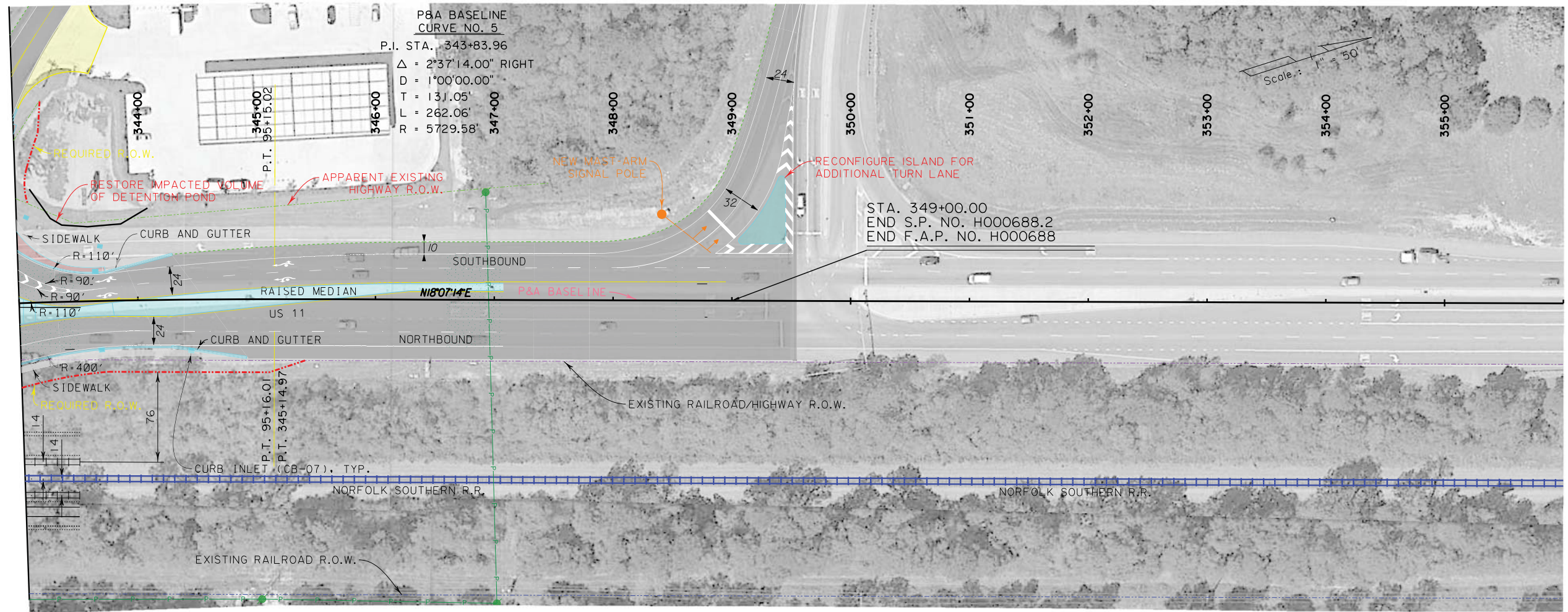
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PRELIMINARY AND SUBJECT TO CHANGE

FIGURE ES-12



						SHEET NUMBER		10					
PLAN AND PROFILE ALTERNATIVE NO. 1R MODIFIED STA. 343+00 TO STA. 356+00 US 111 NORFOLK SOUTHERN RAILROAD BRIDGE						DESIGNED		FULKS/ PICARD		PARISH		ST. TAMMANY	
						CHECKED		L. PORTA		CONTROL SECTION		018-04	
						DETAILED		J. HOWARD		STATE PROJECT		H.000688.2	
						CHECKED		D. FULKS		SERIES NUMBER			
						BY							
				NO.		DATE		REVISION OR CHANGE ORDER DESCRIPTION					
													

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Design Report  
for  
2017 Minimum Design Guidelines

Status:

Preliminary  
Final  
Revised

Project Information:

State Project No.	
Federal Aid Project No.	
Control Section(s)	
Project Name	
Route(s)	
Parish	

Description of Work (or Revision Description)

--

Traffic

Control Section		
Current ADT		
Design ADT		
D		
K		
T		
TDDHV		

Design Waivers

--

Design Exceptions

--

Route and Design Classification

Urban	Rural				
Freeway	Arterial	Collector	Local	Ramp	

Work Classification

Work Type	System	Oversight
New/Reconstruction	NHS	PoDI
Major Rehabilitation	Non NHS	Assumed
Structural Improvement		None
Spot Replacement		
Minor Rehabilitation		
Preventive Maintenance		

Recommended By:

Engineer of Record:	Title:	
Signature:		Date:
DOTD Technical Task Manager (Road):	Title:	
Signature:		Date:
DOTD Technical Task Manager (Bridge):	Title:	
Signature:		Date:
DOTD Project Manager:	Title:	
Signature:		Date:

Approved By:

Print Name:	Title:	
Signature:		Date:

Design Guideline Waivers are approved as noted in the attached instructions. All Design Exceptions must be approved by the Chief Engineer with the Design Exception form.

FIGURE ES-13

State Project No.

Route

Control Section

Roadway Features:

Design Feature	Preferred	Acceptable	Proposed Value	Design Waiver Required	Design Exception Required	Remarks or Explanation for Proposed Value
Design Speed (mph)						
Lane Width (ft)						
Shoulder Width (ft)						
Inside						
Outside						
Shoulder Type						
Inside						
Outside						
Lateral Offset (ft)						
Clear Zone (ft)						
Cross Slope (%)						
Longitudinal Grade						
Slopes (ft/ft)						
Fore Slope						
Back Slope						
Median Width (ft)						
Stopping Sight Distance: Vertical and horizontal distances must be met.						
Do plans meet Stopping Sight Distance requirements?						Design Exception Required
Yes						No
Complete Streets: Accommodations for bikes and pedestrians must be considered. See Design Guidelines for accommodation requirements.						
Do plans meet Complete Streets accommodations?						Design Exception Required
Yes						No
Horizontal Curves Radius/Superelevation:						
Max Super-elevation rate (%) e <sub>max</sub>	Design Speed (mph)	Required Minimum Radius (ft)			Minimum radius and appropriate superelevation are being used for all curves?	Design Exception Required
		Normal Crown	Reverse Crown	Full Super		
					Yes	No
					Yes	No

FIGURE ES-14



State Project No. Control Section Route

Bridge Features:						
Design Feature	Preferred	Acceptable	Proposed Value	Design Waiver Required	Design Exception Required	Remarks or Explanation for Proposed Value
Bridge Width (ft)						
Curb						
Shoulder						
Structural Capacity:						
Do all structures meet requirements for Structural Capacity?					Design Exception Required	Remarks or Explanation for Proposed Value
Yes				No		
Vertical Clearance:						
Are minimum required roadway clearances met for all structure types?					Design Exception Required	Remarks or Explanation for Proposed Value
Yes				No		
Additional Comments:						

FIGURE ES-15

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## ENVIRONMENTAL CHECKLIST

WBS No. H.000688.2

Name: US 11 Norfolk Southern Railroad

Route: US 11

Parish: St. Tammany

### 1. General Information

- |  |  |   |
|--|--|---|
| <input type="checkbox"/> Conceptual Layout | <input checked="" type="checkbox"/> Line and Grade | <input type="checkbox"/> Preliminary Plans    |
| <input type="checkbox"/> Survey            | <input type="checkbox"/> Plan-in-Hand              | <input type="checkbox"/> Advance Check Prints |

### 2. Class of Action

- |  |   |
|--|---|
| <input type="checkbox"/> Environmental Impact Statement (E.I.S.)                                   | <input type="checkbox"/> State Funded Only (EE/EF/ER) |
| <input checked="" type="checkbox"/> Environmental Assessment (E.A.)                                |   |
| <input type="checkbox"/> Categorical Exclusion (C.E.)  |   |
| <input type="checkbox"/> Programmatic C.E. (as defined in FHWA letter of agreement dated 03/15/95) |   |

### 3. Project Description

See Chapters 1, 2, and 3

### 4. Public Involvement

- ☒ Views were solicited.
- ☐ Views were not solicited.
- ☒ Public Involvement events held. (List events and dates in Section 11.)
- ☐ A public hearing/opportunity for requesting a public hearing required. (List dates in Section 11.)
- ☐ A public hearing/opportunity for requesting a public hearing not required.

### 5. Real Estate

- |  | NO                                  | YES                                 | N/A                      |
|--|-------------------------------------|-------------------------------------|--------------------------|
| a. Will additional <b>right-of-way</b> be required? .....                              | <input type="checkbox"/>            | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Is right of way required from a <b>burial/cemetery</b> site? .....                     | <input checked="" type="checkbox"/> | <input type="checkbox"/>            | <input type="checkbox"/> |
| Is right-of-way required from a Wetland Reserve Program ( <b>WRP</b> ) property? ..... | <input checked="" type="checkbox"/> | <input type="checkbox"/>            | <input type="checkbox"/> |
| Is required right-of-way prime <b>farmland</b> ? (Use form AD 1006, if needed) ...     | <input checked="" type="checkbox"/> | <input type="checkbox"/>            | <input type="checkbox"/> |
| b. Will any <b>relocation</b> of residences or businesses occur? .....                 | <input type="checkbox"/>            | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| c. Are construction or drainage <b>servitudes</b> required? .....                      | <input checked="" type="checkbox"/> | <input type="checkbox"/>            | <input type="checkbox"/> |

### 6. Section 4(f) and Section 6(f)

- |  | NO                                  | YES                                 | N/A                      |
|--|-------------------------------------|-------------------------------------|--------------------------|
| a. Will historic sites or publicly owned parks, recreation areas, wildlife or waterfowl refuges ( <b>Section 4f</b> ) be affected? ..... | <input type="checkbox"/>            | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b. Are properties acquired or improved with <b>L&amp;WC</b> funds affected? .....  | <input checked="" type="checkbox"/> | <input type="checkbox"/>            | <input type="checkbox"/> |

---

**7. Cultural Section 106**

---

	NO	YES	N/A
a. Are any <b>known historic properties</b> adjacent or impacted by the project? (If so, list below).....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Are any <b>known archaeological sites</b> adjacent or impacted by the project? (If so, list site # below) .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Would the project affect property owned by or held in trust for a federally recognized <b>tribal government</b> ? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

---

**8. Natural & Physical Environment**

---

	NO	YES	N/A
a. Are <b>wetlands</b> affected? .....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Are <b>other waters</b> of the U.S. affected? .....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Are <b>Endangered/Threatened Species/Habitat</b> affected? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d. Is project within 100 Year <b>Floodplain</b> ? .....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Is project in <b>Coastal Zone</b> Management Area? .....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Is project in a <b>Coastal Barrier Resources</b> area? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
g. Is project on a <b>Sole Source Aquifer</b> ? .....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
h. Is project impacting a <b>navigable waterway</b> ? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
i. Are any State or Federal <b>Scenic Rivers/Streams</b> impacted? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
j. Is a <b>noise</b> analysis warranted (Type I project) .....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
k. Is an <b>air</b> quality study warranted? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
l. Is project in a <b>non-attainment</b> area? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
m. Is project in an approved Transportation Plan, Transportation Improvement Program (TIP) and State Transportation Improvement Program ( <b>STIP</b> )? .....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
n. Are <b>construction</b> air, noise, & water impacts major? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
o. Will the project affect or be affected by a <b>hazardous waste site</b> , leaking underground storage tank, oil/gas well, or other potentially contaminated site? .....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

---

**9. Social Impacts**

---

	NO	YES	N/A
a. Will project change <b>land use</b> in the area? .....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Are any <b>churches and schools</b> impacted by or adjacent to the project? ..... (If so, list below)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Has <b>Title VI</b> been considered? .....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Will any <b>specific groups</b> be adversely affected? (i.e., <i>minorities, low-income, elderly, disabled, etc.</i> ) .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e. Are any <b>hospitals, medical facilities, fire police</b> facilities impacted by or adjacent to the project? (If so, list below).....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
f. Will <b>Transportation patterns</b> change? .....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g. Is <b>Community cohesion</b> affected by the project? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
h. Are <b>short-term social/economic</b> impacts due to construction considered major? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
i. Do conditions warrant <b>special construction times</b> ? (i.e., <i>school in session, congestion, tourist season, harvest</i> ) .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
j. Were <b>Context Sensitive Solutions</b> considered? (If so explain below).....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
k. Were <b>bike and pedestrian</b> accommodations considered? (explain below).....	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

	NO	YES	N/A
I. Will the <b>roadway/bridge be closed?</b> (If yes, answer questions below).....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Will a <b>detour bridge</b> be provided? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Will a <b>detour road</b> be provided? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Will a <b>detour route</b> be signed? .....	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

---

#### 10. Permits (Check all permits that may be required)

---

- |  |   |  |
|--|---|--|
| <input checked="" type="checkbox"/> Corps Nationwide | <input checked="" type="checkbox"/> CUP/Consistency Determination | <input type="checkbox"/> LA Scenic Stream            |
| <input type="checkbox"/> Corps Section 404/10        | <input type="checkbox"/> USCG Bridge                              | <input checked="" type="checkbox"/> DEQ WQC          |
| <input type="checkbox"/> Levee                       | <input type="checkbox"/> USCG Navigational Lights                 | <input checked="" type="checkbox"/> LPDES Stormwater |
| <input type="checkbox"/> Other (explain below)       |   |  |

---

#### 11. Other (Use this space to explain or expand answers to questions above.)

---

4. Views were solicited on May 3, 2013.

A Public Meeting was held on August 22, 2013, and the Public Meeting Summary dated September 17, 2013, is on file with LADOTD.

A Public Hearing was held on December 15, 2015, and the Public Hearing Summary dated January 26, 2016 (revised June 4, 2018) is on file with LADOTD.

A Public Information Meeting was held on April 10, 2018, and the Public Meeting Summary dated June 4, 2018, is on file with LADOTD.

- 5(a) Potential impacts to driveway entrances and parking area at Community Baptist Church located on the west side of US 11 north of the existing bridge.

Addis Boulevard will be re-configured as a right in / right out only; a raised median will prevent left-turn movements at this location. Due to the increased embankment for the US 11 bridge, the connection at US 11 and Indiana Avenue will be eliminated with all traffic utilizing 9th Avenue. Additional transportation pattern changes will be required during bridge construction for the Preferred Alternative. These changes will not require closure or detours.

- 5(b) One commercial structure located at Station 334+00 (shown on Figure ES-7 in the Executive Summary) on the west side of US 11 will require relocation. One commercial structure located on the east side of US 11 at Station 277+00 contains one business (shown on Figure ES-2 in the Executive Summary), which may require relocation. One structure located at Station 279+00 on the east side of US 11 (shown on Figure ES-3 of the Executive Summary) contains three businesses, which may also require relocation. During final design, a survey of the build alternative will confirm if these structures will require relocation.

- 6(a) The US 11 bridge over the Norfolk Southern Railroad was recommended as eligible for the National Register of Historic Places (NRHP) as part of the statewide historic bridge inventory for bridges constructed prior to 1971 and subject to 4(f) evaluation. This determination signified that the adverse effect from the potential demolition of the bridge would require compliance with the Statewide Programmatic Agreement Regarding Management of Historic Bridges in Louisiana (Sept 21, 2015) to demonstrate that the Federal Highway Administration and LADOTD have complied with Section 106 of the National Historic Preservation Act.

- 7(a) The US 11 bridge over the Norfolk Southern Railroad, built in 1937, is constructed with a concrete deck and bents, steel I-beams, and eight spans with two lanes and no shoulders crossing one railroad track.

It was determined eligible for the NRHP under Criterion A for its association with important trends or events that have made a significant contribution to the broad patterns of Louisiana history and for its association with federal New Deal-era funding through the U.S. Works Grade Crossing Program.

Steel beam and girder bridges often lack engineering distinction and typically exhibit little aesthetic treatment. This bridge type was built in substantial numbers, typically following standard plans, both nationally and in Louisiana. During the eligibility determination process, no evidence was found to indicate the bridge is an important example of bridge design, engineering, or construction and, therefore, the bridge does not demonstrate significance under Criterion C.

In addition to the US 11 bridge over the Norfolk Southern Railroad that was recommended as eligible for the NRHP, two structures, the Old Slidell Icehouse (52-02387) and a 1920s residence (52-02398) are located along US 11. The Old Ice House has a modern wooden handicap ramp on the west and north sides that will be avoided as a result of right-of-way (ROW) requirements. The Old Ice House structure will not be impacted. The 1920s residence will also be avoided.

- 8(a) Wetland impacts are estimated to be less than 1.5 acre.
- 8(b) Other surface waters are estimated to be 0.2 acre.
- 8(d) 100-Year floodplain is estimated to be 21 acres.
- 8(e) The Preferred Alternative will require a Coastal Use Permit (CUP) in compliance with the Louisiana Coastal Resources Program and for consistency with the federal Coastal Zone Management Act. The proposed improvements are a use of state concern. The permit process was initiated through the Solicitation of Views process and CUP Permit No. P20130672 has been assigned by the Louisiana Department of Natural Resources, Office of Coastal Management with a copy to the U.S. Army Corps of Engineers (USACE), New Orleans District.
- 8(j) The 2042 Alternative 1 build scenario shows growth in traffic volumes and the proposed changes in horizontal and vertical alignment will cause exterior sound levels at ten receiver locations to approach or exceed the Noise Abatement Criteria (NAC). None of these receiver locations will experience a substantial increase in noise level. The 2042 Alternative 1R and 1R Modified build scenario results showed no noticeable differences when compared to Alternative 1 results.

Any barrier constructed providing a reasonable noise reduction would prevent access to the properties they shield. Noise barriers were also not considered reasonable due to the generalized cost of the structures.

- 8(o) Required ROW for lane widening along US 11 and intersection improvements associated with the Preferred Alternative may occur at US 11 immediately south of Indiana Avenue near the former McMurray filling station. Further investigation may be required during final design and ROW acquisition. Potential, but unlikely, impacts may occur at the Shell station and former Jubilee Express located at the intersection of US 11 and Powell Drive.
- 9(a) To meet roadway widening design criteria, land use changes include some developed and undeveloped residential/commercial lands to transportation use.
- 9(b) Community Baptist Church is located adjacent to and on the west side of US 11 north of the bridge.
- 9(k) The roadway sections will allow for incorporation of a 7-foot sidewalk on the east and west sides of US 11. An optional Complete Streets configuration allows for 4-foot shoulders to accommodate cyclists on the east and west sides of US 11. With this configuration, a 7-foot sidewalk would only be constructed on the east side of US 11. The shoulders and sidewalks will provide an opportunity for future local bicycle and/or pedestrian linkages to Heritage Park and the Tammany Trace.

Eight-foot paved shoulders are provided in the bridge typical section to accommodate bicycles. Less experienced riders can utilize the local roadway network west of US 11 and south of the overpass to connect to the shoulders at the north end of Addis Boulevard if the optional Complete Streets configuration is constructed.

Bicycle accommodations and pedestrian improvements for the proposed project have been evaluated in accordance with the LADOTD Complete Streets Policy and in coordination with the City of Slidell and St. Tammany Parish.

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Preparer: Arcadis U.S., Inc.  
Title: Toby Picard, Project Manager  
Date: July 2019

### **Attachments**

- ☒ S.O.V. and Responses **Appendix C-1, C-2**
- ☒ Wetlands Finding **Section 3.2, Appendix CD-2**
- ☒ Project Description Sheet **Sections 1, 2, and 3**
- ☒ Conceptual Stage Relocation Plan
- ☒ Noise Analysis **Section 3.7, Appendix CD-3**
- ☐ Air Analysis
- ☒ Exhibits and/or Maps
- ☐ 4(f) Evaluation
- ☐ Form AD 1006 (Farmlands)
- ☒ 106 Documentation **Appendix C-3 – Agency Coordination – Section 106**
- ☒ Other

**The Permits, Mitigation and Commitments document follows the Environmental Checklist.**

### **Executive Summary**

**Appendix CD-1 – Traffic Analysis Files**

**Appendix CD-2 – Biological Resources and Wetland Findings Report**

**Appendix CD-3 – Noise Analysis Technical Report**

**Appendix CD-4 – Phase I Environmental Site Assessment Report**

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# SUMMARY

## Permits, Mitigation & Commitments

The Selected Alternative would involve the preparation and submittal of several federal and state permits as well as mitigation requirements.

◆LDNR Coastal Use Permit	◆USACE Wetland Permit	◆Section 401 Water Quality
◆Traffic Control	◆Bicycle Facilities	◆LDEQ NPDES for Construction-Related Activities
◆Relocation	◆Property Access	

### U.S. Army Corps of Engineers Permit

A Jurisdictional Determination by the U.S. Army Corps of Engineers (USACE), New Orleans District is required.

A programmatic general permit is required to satisfy Section 404 of the Clean Water Act. This permit is required for temporary and permanent construction-related impacts to wetlands and other waters of the U.S. determined to be jurisdictional. This permit will be jointly applied for in conjunction with a Coastal Use Permit (CUP). The joint permit process was initiated as part of the Solicitation of Views (SOV). Coordination with the Louisiana Department of Natural Resources (LDNR), Office of Coastal Management is required. All correspondence or inquiries shall reference CUP No. P20130672.

In order to comply with the federal policy of ensuring that there is no net loss of wetlands acres, unavoidable wetlands impacts along the project would be compensated according to an approved mitigation plan as part of the wetland permitting process.

### Section 401 Water Quality Certification

A Section 401 Water Quality Certification is required in conjunction with the Section 404 permit per Louisiana's Water Quality Regulations (Louisiana Administrative Code 33:IX Chapter 15).

This certification would be coordinated with the Louisiana Department of Environmental Quality (LDEQ).

### Louisiana Department of Natural Resources, Coastal Use Permit

The Selected Alternative will require a CUP in compliance with the Louisiana Coastal Resources Program and for consistency with the federal Coastal Zone Management Act. The proposed improvements are a use of state concern. The permit process was initiated through the SOV process and CUP No. P20130672 has been assigned by LDNR, Office of Coastal Management, with a copy to the USACE, New Orleans District. This permit will be jointly applied for in conjunction with a USACE Section 404 permit.

### Louisiana Pollutant Discharge Elimination System (LPDES) Permit and Storm Water Pollution Prevention Plan (SWPPP)

Short-term impacts associated with construction of the Selected Alternative are anticipated including erosion of areas cleared for construction, temporary increases in noise levels, and fugitive dust from use of heavy construction equipment. Temporary impacts to traffic flow and travel patterns are anticipated with construction of the Selected Alternative.

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Adverse construction impacts to water quality would be reduced by implementation of Best Management Practices as outlined in a project-specific SWPPP and Erosion and Sedimentation Control Plan. Measures to reduce erosion and nonpoint source pollution from runoff into surface waters (e.g., properly store materials and equipment, properly store and dispose of waste materials, maintain equipment, and avoid accidental discharges of fuels or other chemicals) will be outlined in the SWPPP. The Selected Alternative would require an LPDES Notice of Intent (NOI) for construction-related activities. The SWPPP will be required to be prepared and kept at the construction site in addition to the LPDES NOI application. LDEQ monitors these practices through its Water Quality Certification program, which is integrated into the Section 404 process.

### Commercial and Residential Relocations

Commercial and residential relocations associated with the Selected Alternative will be addressed through the Uniform Relocation Act of 1970. Measures to reduce relocation impacts will be incorporated during the design stage.

### Traffic Control

Construction-related traffic delays will be minimized through signing plans that inform the drivers of work zones, road closures, detours, and other temporary changes. All traffic maintenance plans will be prepared by qualified traffic engineers in accordance with Louisiana Department of Transportation and Development (LADOTD) standards and will be monitored for effectiveness throughout the construction process.

### Bicycle and Pedestrian Facilities

Currently, the City of Slidell and St. Tammany Parish Master Plans do not include bicycle and pedestrian improvements along the Study Area. However, understanding that future planning within the City of Slidell and St. Tammany Parish

may include bicycle and pedestrian improvements along or near the Study Area, The roadway sections will allow for incorporation of a seven-foot side walk on the east and west sides of US 11. An optional Complete Streets configuration allows for 4-foot shoulders to accommodate cyclists on the east and west sides of US 11. With this configuration, a 7-foot sidewalk would only be constructed on the east side of US 11. The shoulders and sidewalks will provide an opportunity for future local bicycle and/or pedestrian linkages to Heritage Park and the Tammany Trace.

Eight-foot paved shoulders are provided in the bridge typical section to accommodate bicycles. Less experienced riders can utilize the local roadway network west of US 11 and south of the overpass to connect to the shoulders at the north end of Addis Boulevard if the optional Complete Streets configuration is constructed.

Bicycle accommodations and pedestrian improvements for the proposed project have been evaluated in accordance with the LADOTD Complete Streets Policy and in coordination with the City of Slidell and St. Tammany Parish.

### Historic and 4(f) Resources

As part of the Section 106 process, documentation was submitted to the State Historic Preservation Office (SHPO) who concurred that the existing US 11 bridge over the Norfolk Southern railroad is eligible for the National Register of Historic Places (NRHP). This determination signified that the adverse effect from the potential demolition of the bridge would require compliance with the statewide *Programmatic Agreement Regarding Management of Historic Bridges in Louisiana* (Sept 21, 2015). Compliance with this programmatic agreement, along with marketing the bridge, will be completed in order to demonstrate that the Federal Highway Administration (FHWA) and LADOTD have complied with Section 106 of the National Historic Preservation Act (NHPA).

Requirements to assess whether all possible planning has been considered and whether there is a feasible and prudent avoidance alternative is specified in the Programmatic Section 4(f) Evaluation and Approval for the US 11 bridge. The proposed project meets all the applicability criteria specified in the programmatic agreement. Prior to demolition of the US 11 bridge, the LADOTD shall contact the SHPO to determine the appropriate form of documentation and the appropriate state or local depository for the documentation. Unless otherwise agreed to by the SHPO, the FHWA shall ensure that all documentation is completed and accepted by the SHPO prior to the relocation or demolition of the bridge.

### **Hazardous Materials Sites, Underground Storage Tanks**

No HRECs were identified but several RECs were identified within the Study Area. Site No. 1, Jubilee Express Store #4815, and Site No. 9, Circle K #2896, are located within the Study Area. Site No. 8, Western International Gas & Cylinder, is located adjacent to the Study Area.

Required ROW for lane widening along, and intersection improvements associated with, the Selected Alternative may impact Site Nos. 1 and 7. It is anticipated that UST facilities at Site No. 1 will not be impacted by required ROW although appurtenant piping may be impacted. Due to limited historical information regarding Site No. 7, direct impacts are not known. Caution should be taken during construction-related activities.

### **Property Access**

Access will be maintained to properties and all residences and businesses adjacent to the project during and following construction.

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CD-2	Biological Resources and Wetland Findings Report
CD-3	Noise Analysis Technical Report
CD-4	Phase I Environmental Site Assessment Report

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# LIST OF ACRONYMS

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AASHTO	American Association of State Highway and Transportation Officials
ACHP	Advisory Council on Historic Preservation
ADT	Average Daily Traffic
APE	Area of Potential Effect
CAAA	Clean Air Act Amendments
CERCLA	Comprehensive Environmental Response, Compensation, and Liability Act
CESQG	Conditionally Exempt Small Quantity Generator
CFR	Code of Federal Regulations
CUP	Coastal Use Permit
CZMA	Coastal Zone Management Area
dBA	A-Weighted Decibel
DHP	Division of Historic Preservation
DOA	Division of Archaeology
DOT	U.S. Department of Transportation
EA	Environmental Assessment
EDMS	Electronic Document Management System
EDR	Environmental Data Resources
ERNS	Emergency Response Notification System
ESI	Earth Search, Inc.
FEMA	Federal Emergency Management Agency
FHWA	Federal Highway Administration
FINDS	Facility Index Systems/Facility Registration System
FIRM	Flood Insurance Rate Maps
FONSI	Finding of No Significant Impact
FPFA	Farmland Protection Policy Act
GIS	Geographic Information System
HAER	Historic American Engineering Record
HREC	Historical Recognized Environmental Condition
I-10/12	Interstate 10/12
LADOTD	Louisiana Department of Transportation and Development
LCRP	Louisiana Coastal Resources Program
LDCRT	Louisiana Department of Culture, Recreation & Tourism
LDEQ	Louisiana Department of Environmental Quality
LDNR	Louisiana Department of Natural Resources
LEP	Limited English Proficiency
LHRI	Louisiana Historic Resource Inventory

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LNHP	Louisiana Natural Heritage Program
LOS	Level of Service
LPDES	Louisiana Pollutant Discharge Elimination System
LUST	Leaking Underground Storage Tank
MOA	Memorandum of Agreement
mph	Miles per hour
MPO	Metropolitan Planning Organization
MTP	Metropolitan Transportation Plan
NAAQS	National Ambient Air Quality Standards
NAC	Noise Abatement Criteria
NEPA	National Environmental Policy Act
NHPA	National Historic Preservation Act
NOI	Notice of Intent
NPDES	National Pollutant Discharge Elimination System
NRCS	Natural Resources Conservation Service
NRHP	National Register of Historic Places
NS	Norfolk Southern
RCRA	Resource Conservation and Recovery Act
REC	Recognized Environmental Condition
ROW	Right-of-Way
RPC	New Orleans Regional Planning Commission
SELA	Southeast Louisiana
SHPO	State Historic Preservation Office/Officer
SOV	Solicitation of Views
SPILLS	Emergency Response Section Incidents
SQG	Small Quantity Generator
SWPPP	Storm Water Pollution Prevention Plan
TDM	Travel Demand Model
TIP	Transportation Improvement Plan
TNM	Traffic Noise Model
UA	Urban Arterial Design Criteria
US 11/190	U.S. Highway 11/190
US Hist Auto	Historical Gas Station
USACE	U.S. Army Corps of Engineers
USDA	U.S. Department of Agriculture
USEPA	U.S. Environmental Protection Agency
USFWS	U.S. Fish and Wildlife Service
USGS	U.S. Geological Survey
UST	Underground Storage Tank
VOC	Volatile Organic Compound



## SECTION 1

# Proposed Project

*In This Section.....*The Louisiana Department of Transportation and Development and the Federal Highway Administration, in cooperation with the New Orleans Regional Planning Commission, propose to replace the US 11 bridge over the Norfolk Southern Railroad and widen US 11 from two lanes to four lanes from US 190 (Gause Boulevard) north to Interstate 12 in Slidell, St. Tammany Parish, Louisiana.

The Study Area is located in the southeastern portion of St. Tammany Parish and extends south along US 11 from Powell Drive to the intersection of US 11 at Florida Avenue, in Slidell, a distance of approximately 1.7 miles.

### 1.1 Description of the Proposed Project

The Louisiana Department of Transportation and Development (LADOTD) and the Federal Highway Administration (FHWA), in cooperation with the New Orleans Regional Planning Commission (RPC), propose to replace the US Highway 11 (US 11) bridge over the Norfolk Southern (NS) railroad and widen US 11 from two lanes to four lanes from US 190 (Gause Boulevard; US 190) north to Interstate 12 (I-12) in Slidell, St. Tammany Parish, Louisiana. The RPC is the designated Metropolitan Planning Organization (MPO) for the New Orleans urbanized area including St. Tammany Parish.

The proposed action is identified as a Tier II – On System Unfunded Project for fiscal year 2015 – 2024 in the *Metropolitan Transportation Plan*,

*St. Tammany Urbanized Areas, Fiscal Years 2011 – 2040* (MTP November 2010) and in the Transportation Improvement Plan (TIP) for the parish. The proposed project was included as a financially constrained priority project in the *Transportation Improvement Plan, St. Tammany Urbanized Areas, Fiscal Years 2012 – 2016* (March 2012).

The study of the alternatives developed in this environmental assessment (EA) and the associated environmental consequences were evaluated according to the National Environmental Policy Act (NEPA); LADOTD's Stage 1 Planning/Environmental Manual of Standard Practice; and FHWA Guidance for Preparing and Processing Environmental and Section 4(f) Documents.

### 1.2 Study Area

The Study Area is located in the southeastern portion of St. Tammany Parish and

extends south along US 11 from Powell Drive to the intersection of US 11 at Florida Avenue, a distance of approximately 1.7 miles. The proposed construction limits extend from Powell Drive south



**US 11 Bridge over the Norfolk Southern railroad Slidell, Louisiana.**

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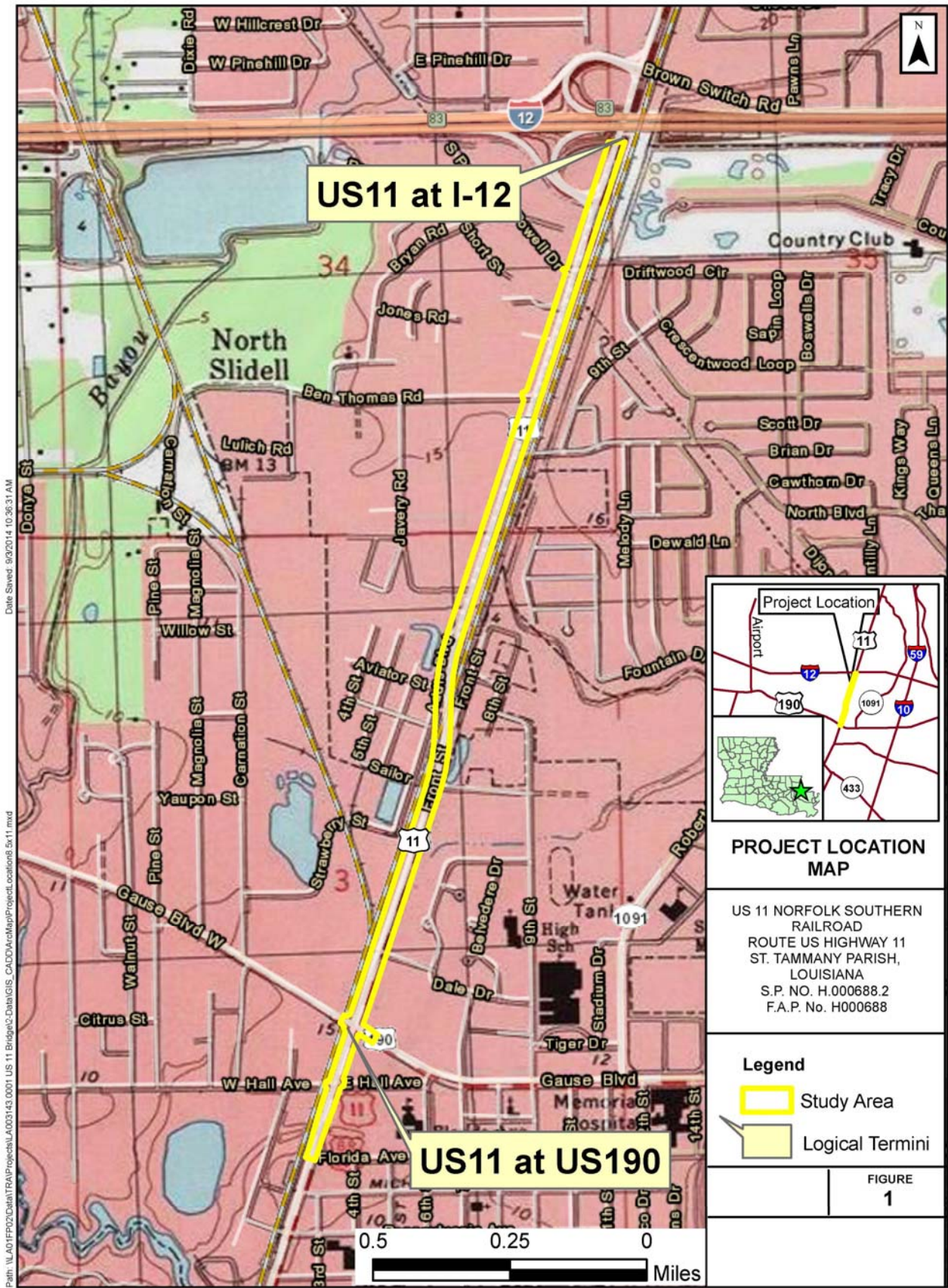
a distance of approximately 1.4 miles, terminating approximately 225 feet north of Lafayette Street. The Study Area is located to the east of Airport Road and west of Interstate 10. North Slidell is located to the north/northwest and Lake Pontchartrain to the south. The Study Area includes the logical termini and the area that may be impacted by the indirect and cumulative impacts from the proposed project (Figure 1).

### **1.3 Logical Termini**

The FHWA defines logical termini for project development as (1) rational end points for a transportation improvement, and (2) rational end points for a review of the environmental impacts. The environmental impact review frequently covers a broader geographic area than the strict limits of the transportation improvements. In the past, the most common termini have been points of major traffic generation, especially intersecting roadways. This is due to the fact that in most cases traffic generators determine the size and type of facility being proposed. Choosing a corridor of sufficient length to look at all impacts need not preclude staged construction.

The logical termini identified on Figure 1 are a result of previous studies completed for the project and surrounding areas, completed improvements, and identified future improvements. The logical termini for the proposed project are US 11 at I-12 and US 11 at US 190. Widening improvements were recently completed at US 11 and I-12 and continue south along US 11 to just north of Powell Drive.









## SECTION 2

# Purpose and Need

*In This Section.....* The purpose of the proposed project is to replace the existing structurally deficient and functionally obsolete bridge crossing the Norfolk Southern railroad and widen US 11 from two lanes to four lanes from US 190 north to Powell Drive. The project is needed in order to upgrade the bridge and roadway segment to current design standards for travel lane and shoulder widths, bridge width, stopping sight distances, design speed, and structural capacity. The proposed roadway segment and bridge improvements will also provide better accommodations for bicycle users and will serve the surrounding community and the larger metropolitan area.

## 2.1 Introduction

The City of Slidell and St. Tammany Parish are located in southeast Louisiana approximately 35 miles northeast of New Orleans and the Mississippi River, 6 miles north of Lake Pontchartrain, and 6 miles west of the state of Mississippi. The City of Slidell is located within the New Orleans urbanized area which includes the city of New Orleans and surrounding suburban areas located in Jefferson, Orleans, Plaquemines, St. Bernard, and St. Tammany Parishes. Due to the proximity of the Gulf of Mexico, Mississippi River, and the Gulf Intracoastal Waterway, this urbanized area has developed into a major port for the region and country. As a result, the New Orleans region supports an extensive multimodal transportation system and is the origination point for six Class I railroads. The region has also experienced an increase in the energy, tourism, and healthcare industries.

As of the 2010 U.S. Census, the greater New Orleans region had a total estimated population of 1,172,734. The City of Slidell had a population of 27,197 and St. Tammany Parish had 233,740. Between 2000 and 2010, the greater New Orleans region experienced a decline in population of 143,766 while St. Tammany Parish experienced an estimated increase of 42,482. The shift in population is due, in part, to the impacts and lingering effects sustained as a result of

Hurricane Katrina in 2005 and has led to more complex travel patterns and lengthier trips.

The LADOTD proposes to replace the existing bridge crossing the NS railroad and widen US 11 from two lanes to four lanes from US 190 north to Powell Drive in Slidell, St. Tammany Parish.

## 2.2 Project Purpose

The purpose of the project is to address safety features of the bridge crossing the NS railroad and system reliability of the roadway segment from US 190 north to Powell Drive.

## 2.3 Project Need

The project is needed in order to upgrade the bridge and roadway segment to current design



Aerial view of the US 11 bridge over the Norfolk Southern railroad, Slidell, Louisiana.

Source: ©2014 Google Maps

standards for travel lane and shoulder widths, bridge width, stopping sight distances, and design speed, as well as system reliability. Roadway segment and bridge improvements will also provide better accommodations for bicycle users and will serve the surrounding community and the larger metropolitan area. More specifically, needs for the proposed project include:

- Address the safety features of the US 11 bridge crossing over the NS railroad;
- Improve capacity;
- Improve travel time;
- Relieve congestion on US 11 by removing the bottleneck between US 190 and I-12;
- Relieve future congestion on area roadways; and
- Improve area-wide mobility and system reliability.

### 2.3.1 Bridge Replacement

The US 11 bridge that crosses the NS railroad was built in 1937 to bridge and roadway standards in place at that time. Even though the US 11 Bridge over the NS railroad is eligible for inclusion on the National Register of Historic Places (NHRP), it must perform as an integral part of the modern transportation system. When unable to maintain system continuity and integrity, the result is bridge rehabilitation or replacement. When a proposed action will “use” a bridge that is eligible for inclusion on the NHRP and the historic integrity of the bridge will be impaired by demolition a programmatic Section 4(f) evaluation is completed.

Constructed with a concrete deck and steel I-beams, the bridge spans 828 feet. The substructure is composed of concrete piers and bents, and the approaches are constructed on embankment. Because the bridge has two 12-foot travel lanes, has no shoulders, and is undivided, it is rated as functionally obsolete (Appendix A). The classification of functional obsolescence does not mean that the bridge is inherently unsafe. It is a

term that identifies a bridge as one that does not perform adequately for its current use, which indicates that measures should be taken to improve functionality. Functional obsolescence is also a term that assigns priority status for federal funding for bridge replacement and rehabilitation.

A further indication of the inadequacy of the US 11 bridge to remain in service is its bridge sufficiency rating. This rating takes many factors into account including structural adequacy and safety, serviceability, functional obsolescence, and essentiality for public use. A sufficiency rating of 80 or below qualifies the bridge for rehabilitation funding. A sufficiency rating of 50 or below qualifies it for replacement funding (FHWA 2006). The 2010 bridge sufficiency rating for the US 11 bridge is 16.4, a clear signal that the bridge is ready for replacement.

The LADOTD Bridge Inspection Report prepared in March 2010 provides an evaluation of the superstructure and substructure according to National Bridge Inspection condition ratings. The general condition ratings are an overall assessment of the physical condition of the deck, superstructure, substructure, and culverts and range from 0 (failed condition) to 9 (excellent condition). The superstructure includes the load-carrying members such as beams or girders that support the driving surface (deck). The substructure includes abutments, piers, and bents.

The 2010 bridge inspection shows that the bridge is structurally deficient. The inspection recorded an overall rating of 4, which indicates poor condition with advanced section loss, deterioration, or spalling (concrete pitting). A rating of 4 was recorded for the superstructure and substructure, and a rating of 5 was recorded for the deck because it was recently resurfaced. A bridge is classified as “structurally deficient” with a general condition rating of 4 or less for the deck, superstructure, and substructure.

The US 11 bridge was built to standards that no longer meet minimum American Association of State Highway and Transportation Officials (AASHTO) and LADOTD guidelines. In order to determine geometric adequacy, consideration

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must be given to the number of travel lanes, roadway width, approach roadway, vertical and horizontal clearances, sight distances, and functional classifications of the roadway. Because historic bridges are often geometrically inadequate, the functional classification of the existing bridge and average daily traffic (ADT) count are utilized to evaluate geometric adequacy. The existing bridge is classified as urban arterial. For 2013, the ADT for the project length was 19,381 with a projected ADT of 26,475 for design year 2042, based on a 1.0 percent annual growth rate. Truck traffic in 2042 is projected to be 6.5 percent of the ADT for 2013.

The roadway classification also determines which standards will be applied to the new design. Current (2017) standards and guidelines for this functional classification prefer 12-foot travel lanes (11-foot lanes are acceptable with a design waiver) and 8-foot shoulders, as well as adequate sight distances. The recommended design speed for the roadway classification is 45 miles per hour (mph). The existing bridge guardrails are concrete posts and rails. Additional guardrails were installed to the interior of the existing guardrail as part of the recent overlay project. The vertical clearance of the existing bridge cannot accommodate widening and meet NS's vertical and horizontal clearance requirements.

### 2.3.2 Roadway Widening

In 1930, the population of St. Tammany Parish was 23,624, growing to 233,540 in 2010. Likewise, the City of Slidell grew from a station on the New Orleans and Northeastern

Railroad to a city of 27,068 persons today. The New Orleans urbanized area, which includes Slidell and St. Tammany Parish, had a total population of approximately 1,316,500 in 2000. St. Tammany Parish experienced dramatic population growth between 1990 and 2000, growing by nearly 47,000. Growth between 2000 and 2010 increased by 42,482 in St. Tammany Parish while Jefferson, Plaquemines, and St. Bernard Parishes all experienced population declines. The City of Slidell also experienced a population increase of 1,373 during this time period. Population projections estimate growth within St. Tammany Parish to increase 51 percent by 2030 to a total population of 459,160.

The Study Area is comprised of five signalized intersections and four un-signalized intersections. Existing year traffic volumes were determined through data obtained from LADOTD and traffic count data collected in May 2013. The RPC is responsible for long- and short-range roadway and transportation plans for the New Orleans urbanized area including St. Tammany Parish and the City of Slidell. The RPC maintains a regional travel demand model (TDM) to forecast traffic conditions. The TDM was utilized to evaluate opening-year (2021) and design-year (2042) traffic volumes for the No Build alternatives.

Traffic volumes are projected to increase along US 11 as shown in Table 1. Travel demand projections for the opening-year and design-year projections applied an estimated annual growth rate of 1 percent and include impacts from proposed developments in the Study Area.



Aerial view looking north along US 11, north of Lafayette Street at transition from recent roadway widening back to two lanes.

Source: Google Maps ©2014 Google

**Table 1: Average Daily Traffic Volumes**

<b>Location US 11 Between:</b>	<b>2013 Existing</b>	<b>2021 No-Build</b>	<b>2042 No-Build</b>
I-12 Westbound and Eastbound Ramps	22,090	24,980	30,480
1-12 Eastbound Ramps and Powell Drive	21,000	24,340	29,650
Powell Drive and Ben Thomas Road	20,850	23,850	29,090
Ben Thomas Road and North Boulevard	20,470	23,120	28,290
North Boulevard and Addis Boulevard	18,000	20,270	24,780
Addis Boulevard and Indiana Avenue	17,890	20,140	24,600
Indiana Avenue and Lafayette Street	17,810	20,050	24,530

Safety is not included in the measures that establish service levels. LADOTD design standards specify an acceptable level of service (LOS) based on roadway classifications. Because of its urban location, LOS C is acceptable for proposed improvements along US 11 although LOS D is allowable in heavily developed urban areas. These measures were assessed through a capacity analysis utilizing methodology found in the Highway Capacity Manual (HCM).

Capacity analysis was conducted along the corridor for a.m. and p.m. peak periods during existing and design year conditions. The capacity analysis results provided in Table 2 indicate that traffic conditions will continue to deteriorate if the roadway is not widened under No Build conditions. Therefore, the widening of US 11 was proposed as part of the bridge replacement project. With the suggested capacity improvements, the corridor will have improved traffic conditions. More details are provided in Section 3.5.

# ENVIRONMENTAL ASSESSMENT

**Table 2: LOS Results for Existing, No Build, and Build Conditions**

Intersection	S/U	Existing Conditions (2013)			No-Build Conditions (2042)			Build Conditions (2042)		
		V/C (S) / Worst Approach (U)	Delay (sec/veh)	LOS	V/C (S) / Worst Approach (U)	Delay (sec/veh)	LOS	V/C (S) / Worst Approach (U)	Delay (sec/veh)	LOS
A.M. Peak Period										
I-12 WB Ramps	S	0.65	47	D	0.87	64.9	E	0.99	70.6	E
I-12 EB Ramps	S	0.46	18.4	B	0.64	22.6	C	0.81	15.6	B
U-Turn (1)	U							NB	17.7	C
Powell Drive	U	EB	17.7	C	EB	59	F	EB	23	C
U-Turns (2)	U							NB	27	D
Ben Thomas Rd	U	EB	19.6	C	EB	41.4	E	EB	16.5	C
North Boulevard	S	0.57	22.4	C	0.78	24.1	C	0.62	13.5	B
U-Turn (3)	S							0.47	0.8	A
Addis Boulevard	U	EB	31.2	C	EB	49.5	E	EB	15.2	A
Indiana Avenue	U	WB	15.8	C	WB	23	C			
U-Turns (4)	U							NB	9.6	A
Lafayette Street	S	0.31	7.2	A	0.43	8.1	A	0.5	9.5	A
US 190 (Gause Boulevard)	S	0.63	28.4	C	0.95	49.9	D	1.07	65.6	E
Hall Avenue	S	0.49	16.6	B	0.75	21.1	C	0.87	25.9	C
Noon Peak Period										
I-12 WB Ramps	S	0.53	35.7	D	0.72	42.6	D	0.8	44.5	D
I-12 EB Ramps	S	0.33	20	B	0.44	20.8	C	0.65	8.3	A
U-Turn (1)	U							NB	13.3	B
Powell Drive	U	EB	15.7	C	EB	35.8	E	EB	16.4	C
U-Turns (2)	U							NB	16	C
Ben Thomas Rd	U	EB	19.7	C	EB	57	F	EB	18.4	C
North Boulevard	S	0.45	8.9	A	0.62	11.6	B	0.49	7.6	A
U-Turn (3)	S							0.39	1.2	A
Addis Boulevard	U	EB	17	C	EB	27.8	D	EB	13.6	B
Indiana Avenue	U	WB	11.7	B	WB	13.8	B			
U-Turns (4)	U							SB	8.7	A
Lafayette Street	S	0.27	10.6	B	0.38	12.2	B	0.45	12.6	B
US 190 (Gause Boulevard)	S	0.65	31.7	C	0.96	47.9	D	1.06	65.6	E
Hall Avenue	S	0.47	18.9	B	0.68	23.2	C	0.81	26	C

 = U-Turn Delay

 = No Approach

 = N/A

LOS = Level of Service

S = signalized

U = unsignalized


V/C = Volume to Capacity Ratio

\* Intersections with V/C > 1.0 are considered LOS F per HCM 2010

Intersection	S/U	Existing Conditions (2013)			No-Build Conditions (2042)			Build Conditions (2042)		
		V/C (S) / Worst Approach (U)	Delay (sec/ veh)	LOS	V/C (S) / Worst Approach (U)	Delay (sec/ veh)	LOS	V/C (S) / Worst Approach (U)	Delay (sec/ veh)	LOS

**P.M. Peak Period**

I-12 WB Ramps	S	0.7	50.4	D	0.94	76.5	E	1.06	66.3	E
I-12 EB Ramps	S	0.49	18.4	B	0.67	43	D	0.93	13	B
U-Turn (1)	U							NB	15.7	C
Powell Drive	U	EB	21.6	C	EB	139.1	F	EB	24.2	C
U-Turns (2)	U							NB	20.6	C
Ben Thomas Rd	U	EB	33.2	D	EB	391.5	F	EB	37.3	E
North Boulevard	S	0.66	17.6	B	1.02	53.2	D	0.77	16.7	B
U-Turn (3)	S							0.54	3	A
Addis Boulevard	U	EB	31	D	EB	92	F	EB	13.9	B
Indiana Avenue	U	WB	15.1	C	WB	20.7	C			
U-Turns (4)	U							NB	14.2	B
Lafayette Street	S	0.34	12.6	B	0.49	16.4	B	0.64	14.2	B
US 190 (Gause Boulevard)	S	0.73	34.3	C	1	68.5	E	1.18	93.3	F
Hall Avenue	S	0.66	19.9	B	0.98	29	C	1.01	44.5	D

 = U-Turn Delay

 = No Approach

 = N/A

LOS = Level of Service

S = signalized

U = unsignalized

V/C = Volume to Capacity Ratio

\* Intersections with V/C &gt; 1.0 are considered LOS F per HCM 2010





## SECTION 3

# Alternatives

*In This Section.....* NEPA directs federal agencies to conduct environmental reviews to consider the potential impacts from proposed federal undertakings. The study of alternatives and the associated environmental consequences were evaluated according to NEPA and consists of three primary tasks:

♦ Scoping & Purpose and Need

♦ Alignment Studies & Development

♦ EA Documentation with FONSI

## 3.1 Introduction

NEPA directs federal agencies to conduct environmental reviews to consider the potential impacts from proposed federal undertakings. The NEPA process requires coordination with local, state, and federal agencies and stakeholders throughout planning and project development decision-making.

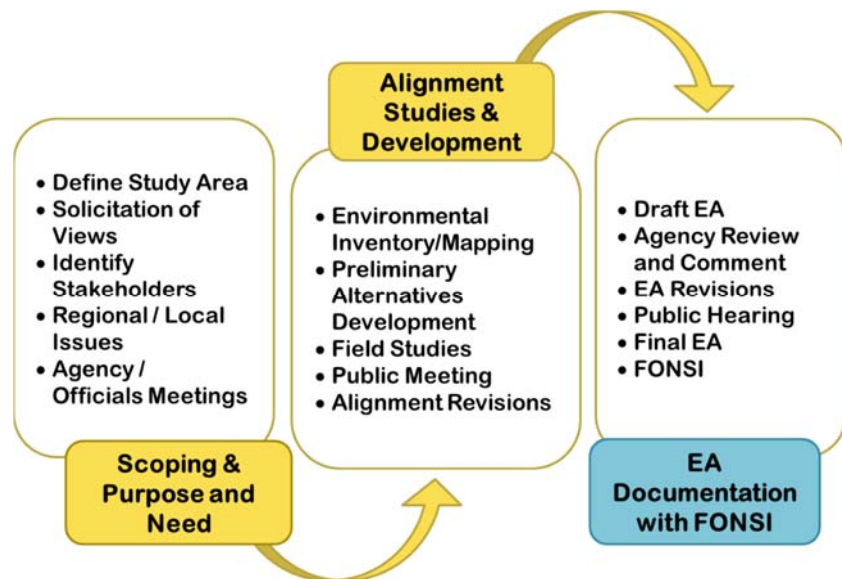
FHWA and LADOTD are committed to the practicable avoidance and minimization of potential impacts to the social and natural environment when considering approval of proposed transportation projects. NEPA project development must consider a range of alternatives that would serve the purpose of the project while balancing the impacts and benefits of the project.

The alternatives and the associated environmental consequences were evaluated according to NEPA, LADOTD's Stage 1 Planning/Environmental Manual of Standard Practice, and FHWA's Guidance for Preparing and Processing Environmental and Section 4(f) Documents.

This study consists of three primary tasks (Figure 2):

- Scoping & Purpose and Need;
- Alignment Studies and Development; and
- EA Documentation with FONSI.

This study process satisfies regulatory and coordination requirements as directed by NEPA allowing for coordination during the alternatives development process and thorough consideration of alternatives developed.



**Figure 2: The NEPA/LADOTD Stage 1 Study Process**



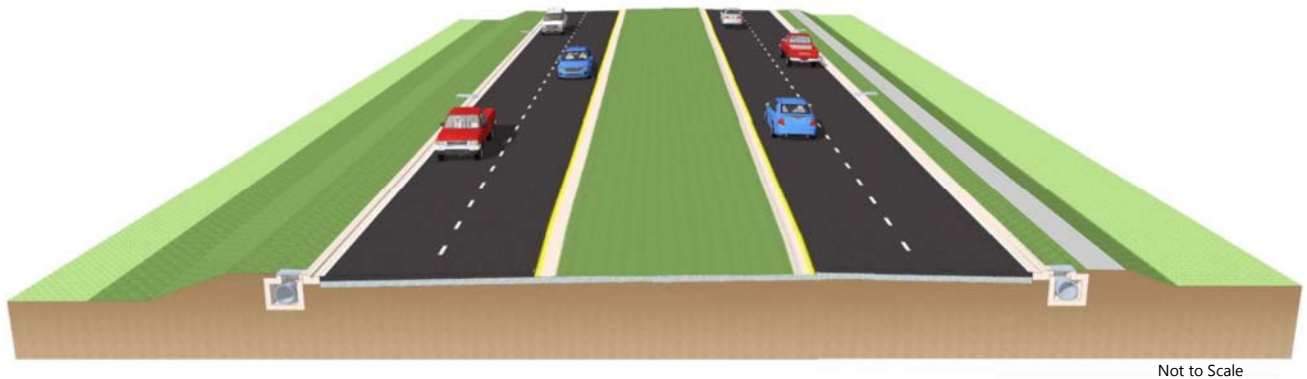
### 3.2 Design Criteria

The proposed project includes replacement of the bridge over the NS railroad designed to LADOTD Bridge Design Standards and widening of the roadway from a two-lane undivided roadway to a four-lane divided roadway designed

to LADOTD urban arterial design criteria (UA). The UA design criteria, bridge design criteria, and typical roadway sections for the bridge replacement and roadway sections are in Appendix B. A graphic representation of the roadway and bridge sections is shown on Figures 3 and 4.

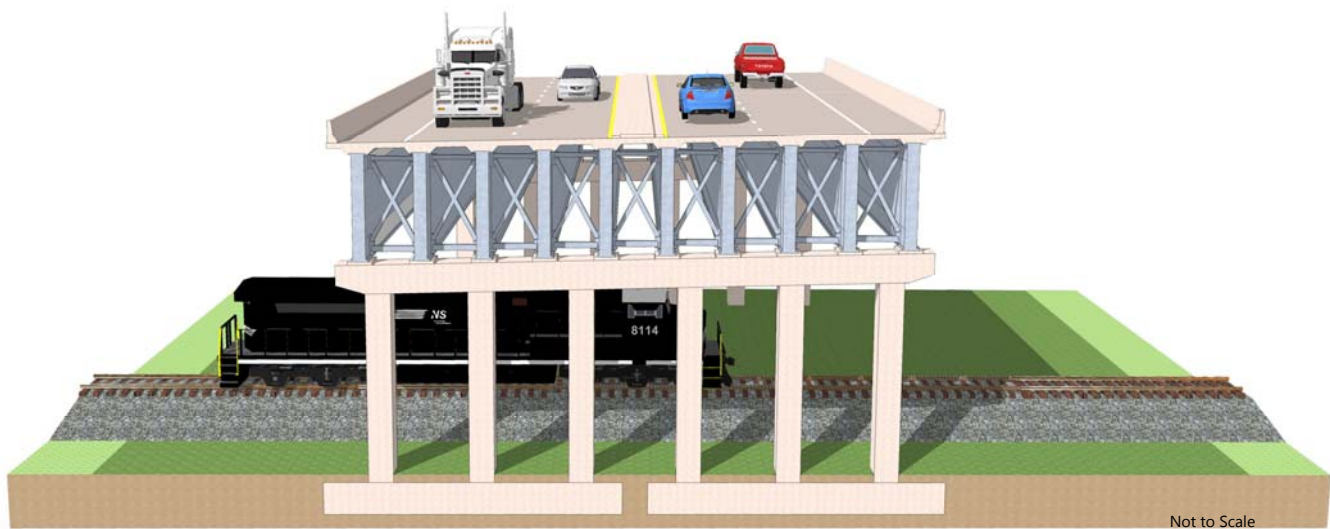


**Typical Roadway Section, Urban Arterial (UA):** Representation of roadway section for the roadway segment north of the of Norfolk Southern Overpass to Powell Drive (left-turn lanes where required). Proposed underground storm drainage on the east and west sides of US 11 with connecting yard drains on the west side.



**Typical Roadway Section, Urban Arterial (UA):** Representation of roadway section for the roadway segment south of the of Norfolk Southern Overpass to just north of Lafayette Street. Proposed underground storm drainage on the east and west sides of US 11.

**FIGURE 3**



**Typical Bridge Section**, Norfolk Southern Railroad Overpass.

**FIGURE 4**

## 3.3 GIS Environmental Inventory

An environmental inventory of existing social, natural, and cultural resource (secondary-source) data was collected within the Study Area. This information was supplemented with field-collected (primary-source) data for the Study Area and proposed alternatives. A Geographical Information System (GIS) was developed for the project and utilized to map and analyze the human, natural, and cultural resources and the proposed preliminary alternatives.

## 3.4 Alternatives Development

### 3.4.1 Stage 0 Alternatives

The Study Area was initially evaluated in a Stage 0 Feasibility Study completed for the RPC and is available at the RPC and LADOTD. The *US 11 Corridor Improvements Stage 0 Feasibility Study* (Krebs et al. 2008) developed a preliminary purpose and need statement, initial project

concepts to address the needs, and potential alternatives to the initial concepts. Two alternatives were identified in the Stage 0 study:

- Widening of US 11 from I-12 south to US 190 and replacement of the bridge crossing the NS railroad; and
- Widening of US 11 from I-12 south to US 190, removal of the bridge crossing the NS railroad, and replacement with an at-grade crossing of the NS railroad.

Early coordination with federal, state, and local agencies solicited comments and responses that were combined with available environmental data. This information was used to help determine if the preliminary alternatives impact certain human, natural, or cultural resources that would result in the decision to dismiss an alternative from further evaluation. This coordination resulted in the dismissal of an at-grade crossing as a viable alternative.

### 3.4.2 Preliminary Alternatives

To minimize impacts and reduce the amount of right-of-way (ROW) to be acquired, alignments were located as close to the existing US 11 roadway as design standards and construction limits would allow. LADOTD policies such as roadway and bridge design, intersection configuration, traffic, noise, and minimization of social and environmental impacts were also considered in the alternatives development. Requirements for horizontal and vertical clearances for the railroad crossing were evaluated and cost and constructability factors were also used. The roadway segments north and south of the bridge follow the existing roadway alignment providing widening and improvements to accommodate a four-lane divided highway. This resulted in one alternative for the roadway segments, identified as Alternative 1. Additional bridge and intersection configurations were also developed.

Two alternatives were considered for replacement of the existing two-lane bridge with a four-lane bridge. The existing bridge crossing angle and NS's overhead grade separation design requirements dictated vertical and horizontal clearance requirements for bridge replacement options. Two bridge alignment alternatives were developed: Bridge 1 follows the existing angle of the bridge; and Bridge 2 follows a slightly realigned bridge crossing angle.

Alternative 1, Bridge 1 crosses the railroad at a 74-degree angle requiring a 390-foot main span length with bridge approaches partially located on embankment. This bridge alternative will allow for maintenance of traffic during construction. With implementation of phased construction, traffic will be allowed to continue to use the existing bridge while part of the new bridge is built. Once completed, the existing bridge will be removed, allowing for bridge construction completion.

Alternative 1, Bridge 2 crosses the railroad at a 65-degree angle requiring a 250-foot main span length with bridge approaches located on

embankment. This bridge option will not allow for maintenance of traffic during construction due to the location of the new bridge situated over a portion of the existing bridge. Complete removal of the existing bridge would be required prior to construction of the new bridge, resulting in roadway closures and a prolonged detour. In



The US 11 bridge over the Norfolk Southern railroad, Slidell, Louisiana.

Source: Google Maps ©2014 Google

addition, US 11 is designated as a secondary hurricane evacuation route (U.S. Geological Survey [USGS] Multi-Hazards Demonstration Project).

For any bridge replacement alternative, the closing of Addis Boulevard and Indiana Avenue will result due to the increased horizontal extents of a new bridge. Alternative ingress/egress to the Addis Boulevard neighborhood includes local roadway improvements to connect 4th Street to Javery Road, which intersects with Ben Thomas Road, leading to US 11. A second option includes local roadway improvements to connect Addis Boulevard to Strawberry Street south of the Western Tampa facility. This option then connects to Carnation Street, which intersects with US 190.

Continued coordination with federal, state, and local agencies solicited comments and responses regarding Bridges 1 and 2. This information was used to help determine if bridge options impact certain human, natural, or cultural resources that would result in the decision to dismiss an alternative from further evaluation. This coordination resulted in the dismissal of Bridge 2

## ENVIRONMENTAL ASSESSMENT

as a viable component of Alternative 1 leaving Bridge 1 to be further evaluated (Appendix B).

### 3.4.3 Alignment Revisions

Potential alignment revisions were reviewed and incorporated into the preliminary alignments following the officials and public meetings held on August 22, 2013. Suggestions were considered to shift the roadway segment east on the north side of the bridge in order to reduce impacts to residential and commercial properties.

The public expressed a need for bicycle and/or pedestrian facilities within the Study Area. Currently, the City of Slidell does not have a master plan that includes bicycle and/or pedestrian improvements along US 11. Bicycle and pedestrian improvements were evaluated in accordance with LADOTD's Complete Streets Policy and in coordination with the City of Slidell and St. Tammany Parish. Anecdotal evidence also presented a need for bicycle and/or pedestrian facilities to allow safe crossing of the NS railroad.

A revision to Alternative 1, Bridge 1 resulted in Alternative 1 Revised (Alternative 1R) and includes a roadway centerline shift to the west on the north side of the US 11 bridge, which continues along this new alignment toward North Boulevard (Appendix B). A right-turn lane was added from the northbound lanes at North Boulevard — with two through lanes continuing north that shift back to the west — in order to meet recent roadway widening improvements at US 11 and Powell Drive.

Following the December 15, 2015, officials meeting and public hearing, potential modifications to surface streets accessing the Addis Boulevard neighborhood were reviewed along with elimination or revision of proposed intersection configurations.

The City of Slidell expressed concern over the proposed improvements making through traffic more efficient. This result is inconsistent with the City's objectives to slow traffic and re-connect neighborhoods with the Slidell business district.

This was of particular concern from Gause Boulevard south to Hall Avenue.

The City also expressed concern over the Addis Boulevard neighborhood access including the proposed connection from Strawberry Street to Carnation Street that would allow access to Gause Boulevard to the south or Ben Thomas Road to the north. The addition of traffic at the Gause Boulevard-Carnation Street intersection and increase in congestion were of concern.

Additional comments received expressed concern regarding proposed intersection improvements and associated turning movements along the corridor.

Upon further review of comments received from the public hearing and subsequent coordination with the City of Slidell, a revision to Alternative 1R resulted in Alternative 1R Modified (Selected Alternative). This alternative includes construction limits beginning approximately 225 feet north of the intersection of US 11 with Lafayette Street and terminating at US 11 and Powell Drive. Additional



improvements include dual right-turn lanes with a recommended signal at the I-12 eastbound exit ramp, a roundabout at Powell Drive, an Addis Boulevard realignment with right-in/right-out access, right-in/right-out access at North

Boulevard, U-turns, and turn lanes. The previously proposed connections from Strawberry Street to Carnation Street and from Addis Boulevard to 4th Street to Javery Road have been eliminated.

### 3.4.4 Complete Streets

Currently, the City of Slidell and St. Tammany Parish Master Plans do not include bicycle and pedestrian improvements along US 11 within the Study Area. LADOTD's Complete Streets Policy recommends that appropriate pedestrian facilities be determined by the context of the roadway. Currently there is no pedestrian access across the NS bridge. Alternative 1R Modified has an optional complete streets configuration with a



4-foot shoulder on the west side of US 11, which can accommodate bicycles. A 7-foot sidewalk would be provided on the east side of US 11 for pedestrians. Future planning within the City of Slidell and St. Tammany Parish may include bicycle and pedestrian improvements along or near the Study Area.

Bicyclists and pedestrians can then connect to areas south of the bridge utilizing the local neighborhood bicycle and/or pedestrian network providing an opportunity for future linkages to Heritage Park and the Tammany Trace. As a safety measure, pedestrian access across the bridge is not provided.

LADOTD Complete Streets Policy recommends new bridges accommodate bicycles where bicycling is not specifically prohibited. Shoulder improvements, such as paved shoulders, are recommended for roadways with greater than 10,000 vehicles per day and an average speed of more than 30 mph. Following the recommended improvements, 8-foot paved shoulders will be provided on the bridge to accommodate bicycles.



### 3.4.5 Bridge Rehabilitation

Following early agency coordination, the bridge over the NS railroad was identified as eligible for the NRHP. The LADOTD in cooperation with the FHWA and the State Historic Preservation Office (SHPO) completed a statewide historic bridge inventory for bridges constructed prior to 1971. A *National Register Eligibility Documentation Report* (September 2013) was prepared by Mead & Hunt. The FHWA made final NRHP eligibility determinations, which are presented in the Mead & Hunt report, and the SHPO has concurred with those determinations. The NRHP employs four criteria for evaluation in determining eligibility: A, B, C, and D. As a result of the Louisiana historic bridge inventory, the US 11 bridge crossing over the NS railroad was identified as eligible under

Criterion A for its association with important "events that have made a significant contribution to the broad patterns of our history".

Important New Deal agencies that funded road and bridge construction included the Civilian Conservation Corps, Civil Works Administration, Public Works Administration, and Works Progress Administration. During the New Deal era, increased attention was given to creating grade separation between railway lines and roadways. Specific legislation was passed which resulted in funding from the U.S. Works Grade Crossing Program through the National Industrial Recovery Act (1933), Hayden Cartwright Act (1934), and Emergency Relief Appropriation Act (1935). The US 11 bridge over the NS railroad was constructed utilizing funds from this program.

Because the US 11 Bridge crossing the NS railroad was found eligible for the NRHP, it is also potentially eligible for protection under Section 4(f) of the *Department of Transportation Act of 1966* (Section 4(f)) and Section 106 of the National Historic Preservation Act (NHPA; Section 106).

The bridge is identified as Recall Number 059090, Structure Number 62520180400531, and SHPO Number 52-08393. The bridge is constructed with a concrete deck and bents, steel I-beams, and eight spans with two lanes and no shoulders. Steel beam and girder bridges often lack engineering distinction and typically exhibit little aesthetic treatment. This bridge type was built in substantial numbers, typically following standard plans, both nationally and in Louisiana.

While eligible for listing in the NRHP, no evidence was found to indicate the bridge is an important example of bridge design, engineering, or construction and, therefore, the bridge does not demonstrate significance under Criterion C (distinctive design or construction characteristics).

Bridge rehabilitation was included in the list of preliminary alternatives as the only potential means of preserving the bridge. Rehabilitation to the original condition without changing the 1930s

bridge design features such as lane widths and lack of shoulders was one form of rehabilitation considered. Keeping the existing bridge in service as a part of the transportation network could avoid any adverse effects. However, to remain in service, the bridge deficiencies related to its age and design must be addressed. Bridge rehabilitation can be considered an avoidance alternative if modifications sufficient enough to address bridge deficiencies are implemented. The bridge rehabilitation can be considered an avoidance alternative that satisfies Section 4(f) requirements only if both of the following conditions can be met:

1. The elements that make the bridge historically significant are preserved; and
2. Structural and functional deficiencies are addressed.

Rehabilitation that maintains the existing bridge would not sufficiently address structural and functional deficiencies of the bridge. Correction of structural and functional deficiencies would entail removal or replacement of the existing bridge components in order to meet current LADOTD roadway design criteria, while meeting railroad clearance requirements. Therefore, rehabilitation of the existing bridge was dismissed from further consideration.

### **3.4.6 No Build Alternative**

NEPA requires that doing nothing to address the project need be considered during the environmental review process. This alternative was designated as the No Build Alternative. Although this alternative does not meet the purpose and need for the project because it would not address bridge deficiencies related to its age and design, it is considered in the EA as a baseline for comparison.

### **3.5 Traffic**

A capacity analysis was performed to address future capacity issues along US 11 from I-12 to

Hall Avenue (Appendix CD-1). Traffic forecasts were performed for the design year (2042) and capacity analyses were performed for a.m. and p.m. peak periods for existing conditions and future year (2042) No Build and Build conditions. In addition, alternative intersection configurations were developed that would improve future traffic conditions. The capacity analysis completed in January 2014 (Appendix CD-1) explored different configurations at each of the major intersections to further improve capacity along the corridor. The options included standard (signalized) median openings, restricted crossing intersections, and closed medians with U-turns. These configurations are consistent with LADOTD policy on highway capacity improvement ("widening") projects.

However, based on comments received following the December 15, 2015, public hearing and subsequent local and agency coordination, the construction limits for the project were revised and an updated capacity analysis was performed (Appendix CD-1).

The updated capacity analysis considers one configuration for each intersection and dismissed the restricted and closed median options from further consideration. Additional study was also performed in 2018 to introduce a roundabout at the intersection with Powell Drive. This addition was made to incorporate the desire for ease of heavy vehicle access and egress of the Textron facility and a service station that was built adjacent to the corridor after the original NEPA process was initiated. Table 3 presents a compilation of the proposed intersection changes and configurations. Figure 5 shows the locations of the proposed improvements.

Several new median U-turns were required in the build condition due to the closed median and restricted crossings planned for the corridor. Signalization of the U-turns provided north of the bridge is warranted under open-year volumes. The U-turn between Powell Drive and Ben

Thomas Road is only slightly above the threshold value and shows acceptable LOS values. Therefore, it is not deemed warranted.

### 3.5.1 Build Conditions for Intersections

Build improvements will provide safer and more efficient operating conditions along the study corridor as compared to the No Build scenario. (Table 2). The capacity analysis results for Build conditions are summarized in Table 4 and illustrated on the figures in Appendix B, respectively.

Build conditions show a reduction in delay at Powell Drive, Ben Thomas Road, North Boulevard, Addis Boulevard, Lafayette Street, and Hall Avenue as compared to No Build conditions in

the design year. These reductions are achieved through the widening of US 11 and the addition of turn lanes at several intersections. It was also noted that two intersections outside the limits of construction will be positively impacted: US 11 at the I-12 eastbound ramps and US 11 at Hall Avenue.

The intersection of the I-12 westbound ramps and US 11 shows high delay similar to the No Build option due to improvements needed at this intersection. Delay at the intersection of US 11 and US 190 will increase from LOS D to LOS E in the a.m. and noon peak periods. Mitigations at these intersections were not modeled as part of this analysis because the intersections are outside the established construction limits.

**Table 3: Intersection Configurations for the Build Condition**

<b>Intersection with US 11</b>	<b>Build Condition</b>
<b>I-12 EB Ramps</b>	Additional right-turn lane provided for eastbound approach
<b>Powell Drive</b>	Dual-lane roundabout; roundabout accommodates WB-67
<b>U-Turns (2)</b>	Southbound and northbound U-turns provided for driveways south of Powell Drive and Ben Thomas Road
<b>Ben Thomas Road</b>	Right-in, right-out only; closed median
<b>North Boulevard</b>	Signalized, partial-access opening. Left turns restricted from North Boulevard
<b>U-Turns (1)</b>	Southbound U-turn for Ben Thomas Road and North Boulevard
<b>Addis Boulevard</b>	Right-in, right-out only; closed median
<b>Indiana Avenue</b>	Closed due to new bridge footprint
<b>U-Turns (2)</b>	Southbound and northbound U-turns provided for Addis Boulevard and businesses on east side of US 11
<b>Lafayette Street</b>	Signalized, full-access opening



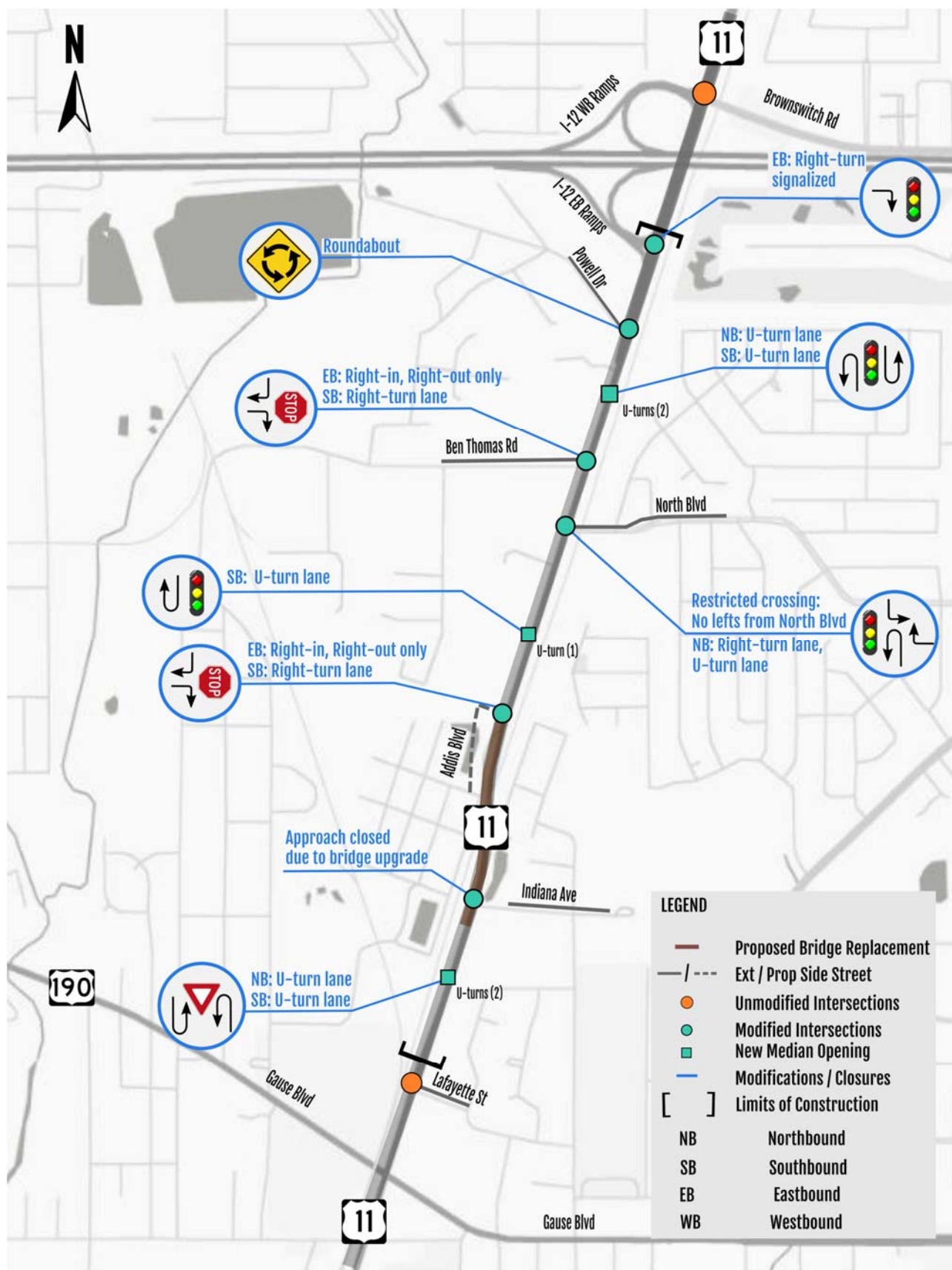


Figure 5: Intersection and Median Opening Locations for the Build Option

**Table 4: LOS Results for the Build Condition (2042)**

Intersection	S/U	Eastbound		Westbound		Northbound		Southbound		Intersection Average		
		Delay (sec/veh)	LOS	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS	V/C	Delay (sec/veh)	LOS


**A.M. Peak Period**

I-12 WB Ramps	S	82.4	F	79.6	E	65.5	E	63.6	E	0.99	70.6	E
I-12 EB Ramps	S	12.2	B	-	-	12.5	B	22	C	0.81	15.6	B
U-Turn (1)	U	-	-	-	-	17.7	C					
Powell Drive	U	23	C									
U-Turns (2)	U	-	-	-	-	27	D	9.6	A			
Ben Thomas Rd	U	16.5	C	-	-							
North Boulevard	S	-	-	18.4	B	10.9	B	13.3	B	0.62	13.5	B
U-Turns (3)	S	-	-	-	-	-	-	0.7	A	0.47	0.8	A
Addis Boulevard	U	15.2	C	-	-							
U-Turn (4)	U	-		-	-	9.6	A	8.8	A			
Lafayette Street	S	38.5	D	41.4	D	6.5	A	7.4	A	0.5	9.5	A
US 190 (Gause Boulevard)	S	62.8	E	53.1	D	80.9	F	65.4	E	1.07	65.6	E
Hall Avenue	S	23.4	C	33.2	C	25.4	C	25.8	C	0.87	25.9	C

**Noon Peak Period**

I-12 WB Ramps	S	55.7	E	49.1	D	42.6	D	39.3	D	0.8	44.5	D
I-12 EB Ramps	S	10.5	B	-	-	7.8	A	6.1	A	0.65	8.3	A
U-Turn (1)	U	-	-	-	-	13.3	B					
Powell Drive	U	16.4	C	-	-							
U-Turns (2)	U	-	-	-	-	16	C	9.5	A			
Ben Thomas Rd	U	18.4	C	-	-							
North Boulevard	S	-	-	19.9	B	8.5	A	4.8	A	0.49	7.6	A
U-Turns (3)	S	-	-	-	-	-	-	0.7	A	0.39	1.2	A
Addis Boulevard	U	13.6	B	-	-							
U-Turn (4)	U	-	-	-	-	0	A	8.7	A			
Lafayette Street	S	48.8	D	45.2	D	6.7	A	8.4	A	0.45	12.6	B
US 190 (Gause Boulevard)	S	43.4	D	62.6	E	71.5	E	84.6	F	1.06	65.6	E
Hall Avenue	S	25.1	C	38.7	D	25.8	C	21.6	C	0.81	26	C

 = U-Turn Delay

 = No Approach

 = N/A

LOS = Level of Service

S = signalized

U = unsignalized

V/C = Volume to Capacity Ratio

\* Intersections with V/C > 1.0 are considered LOS F per HCM 2010

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Intersection	S/U	Eastbound		Westbound		Northbound		Southbound		Intersection Average		
		Delay (sec/veh)	LOS	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS	V/C	Delay (sec/veh)	LOS

### P.M. Peak Period

I-12 WB Ramps	S	82.2	F	86.5	F	52.8	D	70.7	E	1.06	66.3	E
I-12 EB Ramps	S	15.4	B	-	-	13.7	B	8	A	0.93	13	B
U-Turn (1)	U	-	-	-	-	15.7	C					
Powell Drive	U	24.2	C	-	-							
U-Turns (2)	U	-	-	-	-	20.6	C	12.6	B			
Ben Thomas Rd	U	37.3	E	-	-							
North Boulevard	S	-	-	16.4	B	18.4	B	15.3	B	0.77	16.7	B
U-Turns (3)	S	-	-	-	-	-	-	14.9	B	0.54	3	A
Addis Boulevard	U	13.9	B	-	-							
U-Turn (4)	U	-	-	-	-	14.2	B	9.8	A			
Lafayette Street	S	30.7	C	27.7	C	10.7	B	12.2	B	0.64	14.2	B
US 190 (Gause Boulevard)	S	70	E	78.2	E	104.5	F	124.8	F	1.18	93.3	F
Hall Avenue	S	32.5	C	60.1	E	43.6	D	43.6	D	1.01	44.5	D

 = U-Turn Delay

 = No Approach

 = N/A

LOS = Level of Service

S = signalized

U = unsignalized

V/C = Volume to Capacity Ratio

\* Intersections with V/C > 1.0 are considered LOS F per HCM 2010

### 3.5.2 Build and No Build Comparison and Recommendation

The intersection capacity analysis for the No Build conditions shows that many intersections in the Study Area will operate near or over capacity by the design year. The analysis reveals that all signalized intersections will operate at LOS D or better in the design year under the Build condition.

Table 5 presents a summary and comparison of the No Build and Build option performance results from the analysis for both the network as a whole and for the US 11 corridor. Performance indicators included average delay per vehicle and average speed.

**Table 5: Comparison of No Build and Build Conditions**

Design Year (2042)				
Build / No Build Option	Network Level		US 11 Corridor	
	Delay/Veh (sec)	Average Speed (mph)	Delay/Veh (sec)	Average Speed (mph)
<b>A.M. Peak Hour</b>				
No-Build	26	17	15	23
Build	18	18	13	22
No-Build vs. Build (%)	-30.8%	+5.9%	-13.3%	-4.3%
<b>Noon Peak Hour</b>				
No-Build	21	19	13	25
Build	17	19	12	23
No-Build vs. Build (%)	-19.0%	0.0%	-7.7%	-8.0%
<b>P.M. Peak Hour</b>				
No-Build	100	6	24	18
Build	24	15	18	19
No-Build vs. Build (%)	-76.0%	+150.0%	-25.0%	+5.6%

### 3.6 Alternatives Comparison

#### 3.6.1 Resource Impact Analysis

A number of resources and issues were used to compare each alternative chosen for detailed evaluation. The resources used to compare the

alternatives are compiled in the GIS for the project or detailed in a series of technical documents that are incorporated by reference into the EA. Alternatives were evaluated with respect to the environmental and engineering factors. Table 6 summarizes these effects.

**Table 6: Alternatives Evaluation Matrix**

Evaluation Factors	Alternative 1, Bridge 1	Alternative 1R, Bridge 1	Alternative 1R Modified, Bridge 1	No Build
<b>Structure Impacts</b>				
Residence	0	0	0	0
Business	13	13	5	0
Church	0	0	0	0
Public Facility	0	0	0	0
Underground Storage Tanks/Pumps/	6	6	0	0
Signage	11	11	6	0
Parking Spaces	107	107	25	0
Potential Relocations	13	13	3	0
Noise	10	10	5	5
<b>Natural Resources Impacts</b>				
Cemetery	0	0	0	0
100-Year Floodplain (acres)	20	20	21	0
Surface Waters (acres)	<1	<1	<1	0
Wetland (acres)	<1	1.1	<2	0
Prime Farmland (acres)	0	0	0	0
Archaeological	0	0	0	0
Potential Historic Resources > 50 Years Old	23	23	23	0
Eligible Historic Resources—Impacted	2	2	1	0
Known Underground Storage Tank Sites	3	3	3	0
Water Wells	3	3	3	0
Oil and Gas Wells	0	0	0	0
<b>Traffic Impacts</b>				
Permanent Road Closures	2	2	1	0
Temporary Detour	1	0	0	0

### 3.6.2 Preliminary Costs Analysis

Preliminary costs analysis for the alternatives includes roadway construction, bridge construction, utility relocation, ROW, wetland

mitigation and surveying, engineering, and construction supervision/inspection. These costs are presented in Table 7.

**Table 7: Alternatives Cost Analysis**

Evaluation Factors	Alternative 1, Bridge 1	Alternative 1R, Bridge 1	Alternative 1R Modified, Bridge 1	No Build
Total Length (miles)	1.7	1.7	1.4	0
<b>Cost (million dollars)</b>				
Roadway Construction	19	19	13.6	0
Roundabout Construction	-	-	1.9	0
Bridge Construction	48	48	48.2	0
Utility Relocation	0.505	0.505	0.405	0
Right-of-Way Acquisition	7.1	6.8	3.7	0
Wetland Mitigation	0.16	0.16	0.20	0
Surveying, Engineering, Construction Supervision & Inspection	3.8	3.8	3.8	0
<b>TOTAL</b>	<b>78</b>	<b>78</b>	<b>71.8</b>	<b>0</b>

Includes 20% Roadway and 10% Bridge Contingency for Estimating Purposes.  
Costs Rounded.

### 3.6.3 Selected Alternative

As a result of the comprehensive resources evaluation, traffic studies, and public, local officials, and federal and state resource agency coordination, sufficient information and public opinion exist to identify Alternative 1R Modified with Bridge 1 as the Selected Alternative. Alternative 1R Modified includes the widening of US 11 from Powell Drive south to just north of Lafayette Street. Bridge 1 is situated along the existing bridge alignment. The Selected Alternative includes an optional complete streets configuration with 4-foot shoulders on the west side of US 11 connecting to a local roadway network.

The identification of the Selected Alternative addresses the stated purpose and need for the project and satisfies, to the fullest extent possible, the objectives of NEPA. Impacts from the Selected Alternative were avoided where possible and minimized to the greatest extent practicable.





## SECTION 4

# Existing Conditions & Impacts

*In This Section.....*Key resources are evaluated to determine the potential beneficial or adverse impacts of the project's Selected Alternative and No Build Alternative.

Evaluation of social, economic, and environmental resources indicates minor or no impacts to populations including minority and low-income, farmlands, biological resources, and air quality. Impacts are noted for noise, historic resources, underground storage tank sites, wetlands, and temporary construction related impacts.

### 4.1 Introduction

This section provides an analysis of the potential beneficial or adverse impacts of the project's Selected Alternative and No Build Alternative. The project is evaluated with respect to transportation, social, economic, cultural, physical, natural, and biological resources. This section discusses direct impacts (loss of a resources), indirect impacts (changes in function or quality of a resource), and cumulative impacts (historical, project related, and foreseeable impacts).

### 4.2 Land Use and Community Resources

#### 4.2.1 Land Use

Land use classifications within the Study Area are shown on Figure 6. The Study Area comprises approximately 35 acres. Land use within the Study Area is predominantly transportation including roadway and rail line uses. Commercial land uses are located along state routes while single-family residential and undeveloped lands are located along existing local roadways. North of the bridge that crosses the NS railroad, US 11 is bounded to the east by the railroad and to the west by a large flood control basin and commercial and light industrial uses. Interstate 12 is adjacent to the Study Area to the north. Single family residential is the primary use adjacent to the bridge to the east and west. Some light industrial is located to

the southwest of the bridge. South of the bridge, US 11 is bounded on the east by commercial land uses including retail, office, and a gasoline station and on the west by the railroad. A shopping center is located to the west beyond the railroad. Approximately 52 percent of the Study Area is transportation land use with 26 percent undeveloped lands including woodlands, wetlands, and surface waters. Commercial and residential land uses comprise approximately 10 and 2 percent of the Study Area, respectively.

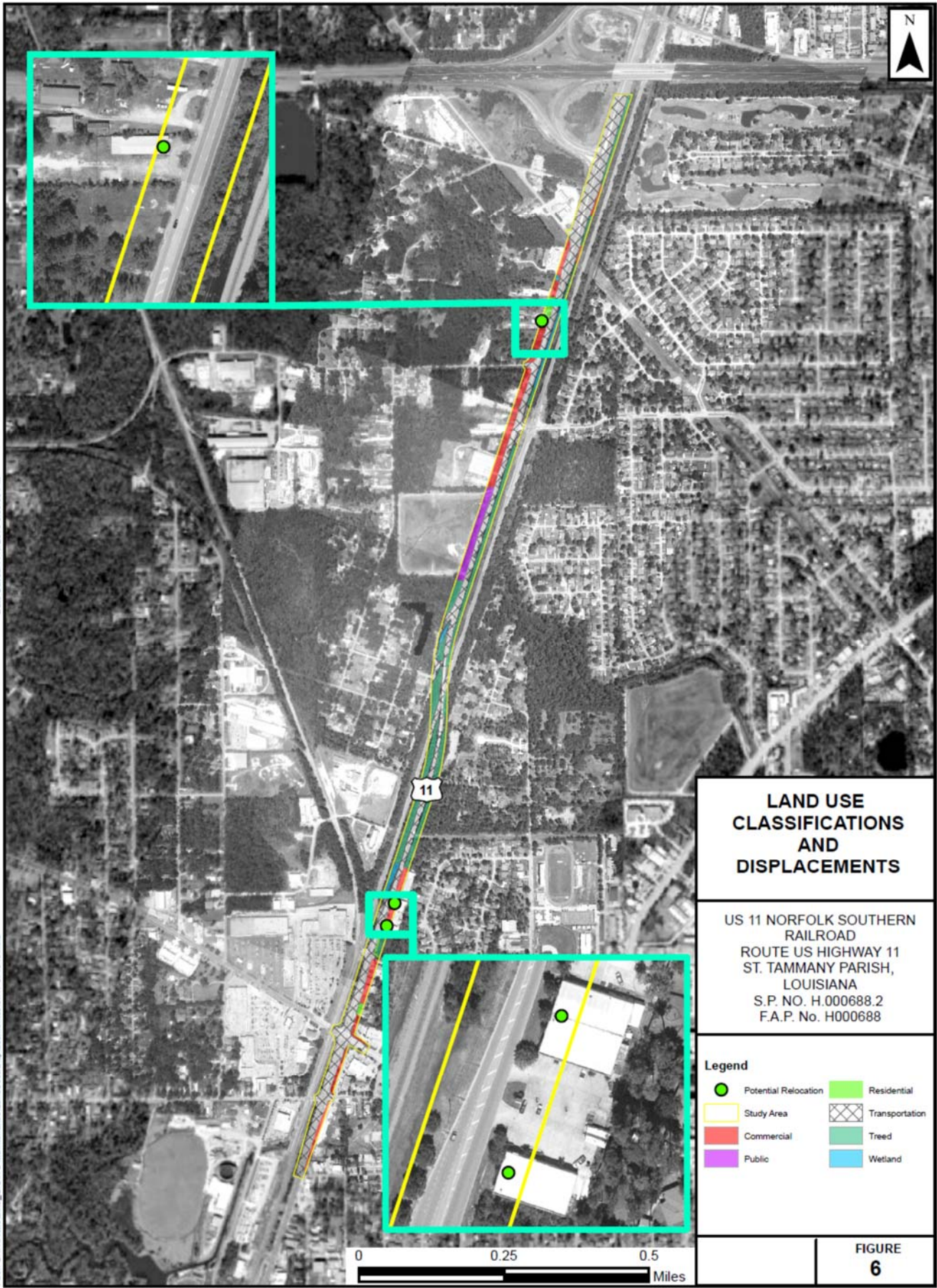
For all alternatives, existing roadway ROW or previously disturbed lands will be converted from its present use to transportation use.

The No Build Alternative would not impact land use.

#### 4.2.2 Residential and Business Relocations

Potential structure impacts within the Study Area include three commercial structures. Figure 6 shows and Table 8 lists the number of displacements the proposed project may have on commercial structures within the Study Area. During final design, a survey of the build alternative will confirm if these structures will require relocation.

The No Build Alternative does not require any relocations.



**Table 8: Number of Residential and Commercial Displacements - Selected Alternative**

Type of Displacement	Number of Displacements Alternative 1R Modified, Bridge 1
	Option 1
Residential	0
Owner-Occupied Business	0
Tenant Business	3
Personal Property Only Moves	0

Source: Quest Acquisitions, LLC 2014

### 4.3 Economic Environment

Early Slidell and St. Tammany Parish residents enjoyed a robust tourist industry while many depended on the land for their economic welfare. Along with farming and trapping, an aggressive timber industry supported lumber mills, such as the Salmen Brick and Lumber Company, into the 20th century. Brickmaking also remained an important industry in the Parish until their decline following World War I. Today, industry sectors that contribute the highest employment opportunities include a combination of educational services, health care, and social assistance followed by retail trade, government, light manufacturing, and professional, services (St. Tammany Economic Development November 2013).

Economic impacts associated with construction of the proposed project will include a temporary increase in construction-related employment. Benefits from the proposed project such as reduced congestion, increased traffic flow, and increased accessibility may improve the economic environment within and adjacent to the Study Area.

The No Build Alternative would lead to continued and worsened congestion within the Study Area

and surrounding area and may have a negative economic impact on employment.

## 4.4 Socioeconomic Resources

Executive Order 12898, *Federal Actions to Address Environmental Justice in Minority and Low-Income Populations* (59 Federal Register 7629 1994), requires federal agencies to determine whether a proposed action would have an adverse and disproportionately high impact on minority and/or low-income populations.

### 4.4.1 Population

The 2010 U.S. Census identified three Census Tracts, 410.02, 410.04, and 411.03, comprised of 46 Census Blocks that intersect the Study Area (Figure 7). The population within the Census Blocks was examined to determine total population and minority and/or low-income populations associated with improvements related to all alternatives. Census Block data were compared with Census Tract level data in order to identify potential disproportionate impacts.

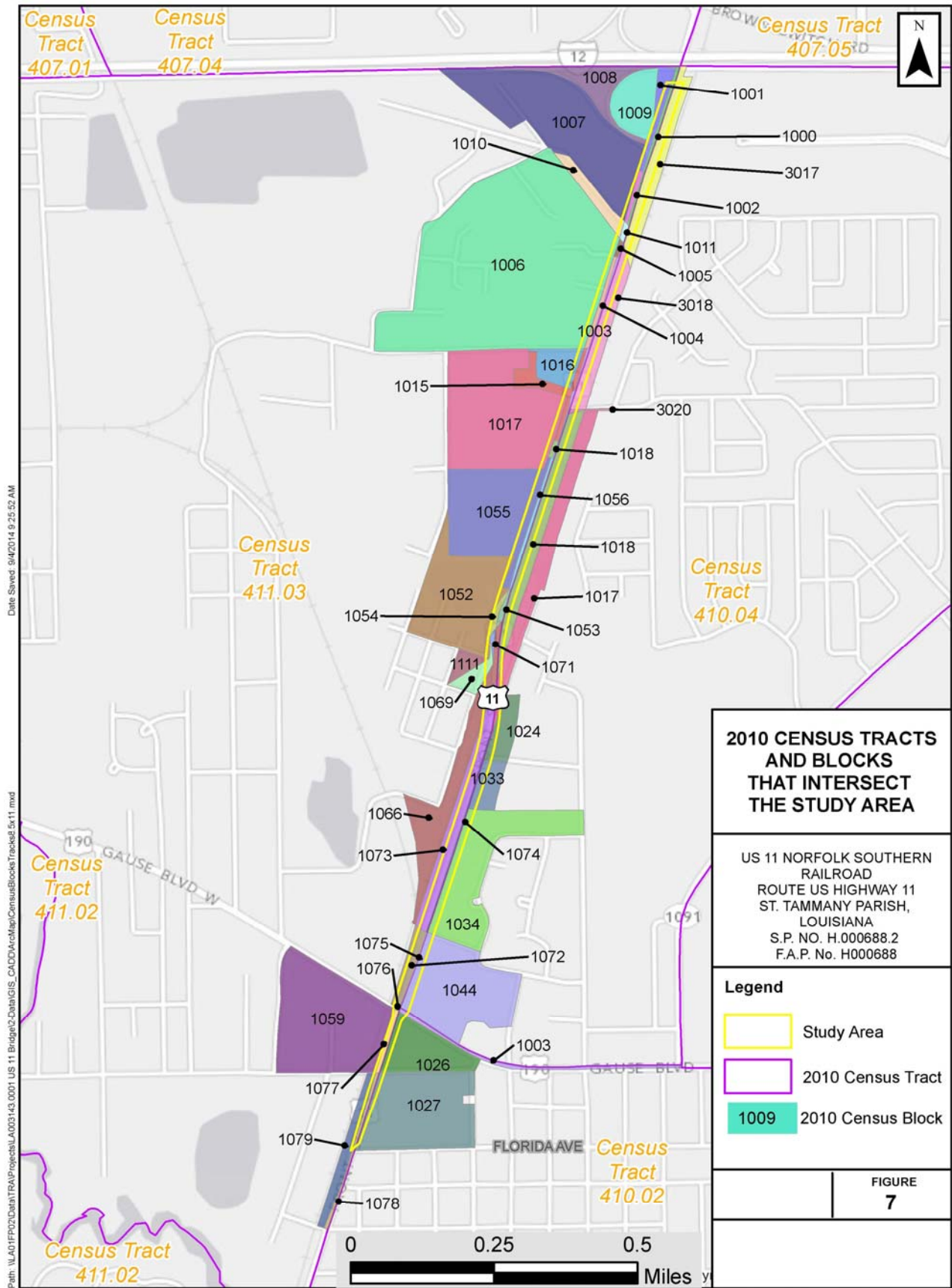
Only 9 of the 46 Census Blocks that intersect the Study Area represent the population within the Study Area. The estimated population within the Study Area is 241. The Census Block population is 2 percent of the Census Tract population, 0.8 percent of the Slidell population, and 0.01 percent of the St. Tammany Parish population (Table 9).

**Table 9: Total Population Data**

Geographic Area	Population
Louisiana	4,533,372
St. Tammany Parish	233,740
City of Slidell	27,068
2010 Census Tracts Within Study Area	12,709
2010 Blocks Within Study Area	241

Source: U.S. Census Bureau, Census 2010 ([www.census.gov](http://www.census.gov))  
Note: Geographic Area was determined to be the Census Blocks within Census Tracts 410.02, 410.04, and 411.03 that intersect the Study Area.





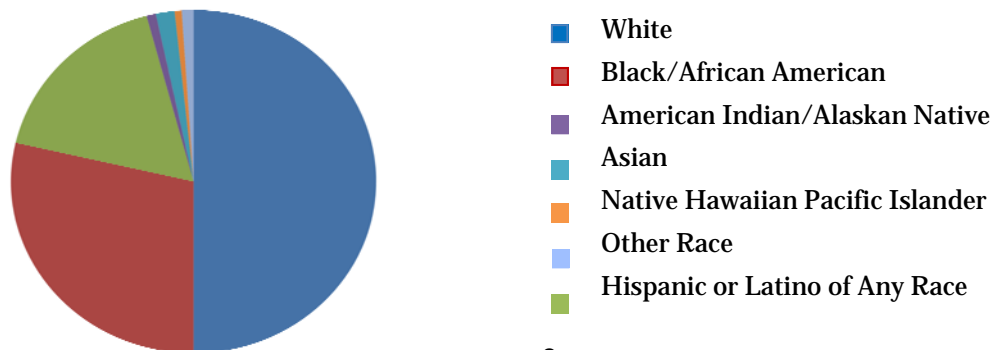
#### 4.4.2 Minority Populations

The racial and ethnic composition of the population within the Study Area was examined in order to identify the presence or absence of minority populations. Within the Census Blocks that intersect the Study Area, 57 percent of the population is identified as white alone and

43 percent as minority. Total and minority population data are depicted on Figures 8 and 9 and presented in Table 10.

The Selected Alternative and No Build Alternative would have no effect on the minority population within the Study Area.

**Figure 8: Total and Minority Populations**



Source: US Census Bureau, 2010 Summary File 1; Tables QT-P5 and P5., B03002 2008-2012 America Community Survey 5-Year Estimates

**Table 10: Total and Minority Populations**

Geographic Area	Total Pop.	Not Hispanic or Latino						Hispanic or Latino of Any Race	Total Minority Pop (%)
		White	Black / African American	AIAN *	Asian	NHPI*	Other Race		
<b>All Blocks TOTAL</b>	<b>256</b>	<b>136</b>	<b>83</b>	<b>4</b>	<b>8</b>	<b>3</b>	<b>5</b>	<b>17</b>	<b>40.2</b>
<b>TOTAL Percent</b>	<b>100</b>	<b>53.1</b>	<b>32.4</b>	<b>1.6</b>	<b>3.1</b>	<b>1.2</b>	<b>1.9</b>	<b>6.7</b>	<b>40.2</b>
Census Tract 410.02	2,988	2,764	72	14	0	0	76	62	5.4
Census Tract 410.04	4,267	3,751	179	0	53	0	59	225	6.8
Census Tract 411.03	3,454	1,571	1,403	0	35	0	82	363	44.0
<b>Census Tracts TOTAL</b>	<b>10,709</b>	<b>8,086</b>	<b>1,654</b>	<b>14</b>	<b>88</b>	<b>0</b>	<b>217</b>	<b>650</b>	<b>18.4</b>
<b>TOTAL Percent</b>	<b>100</b>	<b>75.5</b>	<b>15.4</b>	<b>0.1</b>	<b>0.8</b>	<b>0</b>	<b>2.1</b>	<b>6.1</b>	<b>18.4</b>

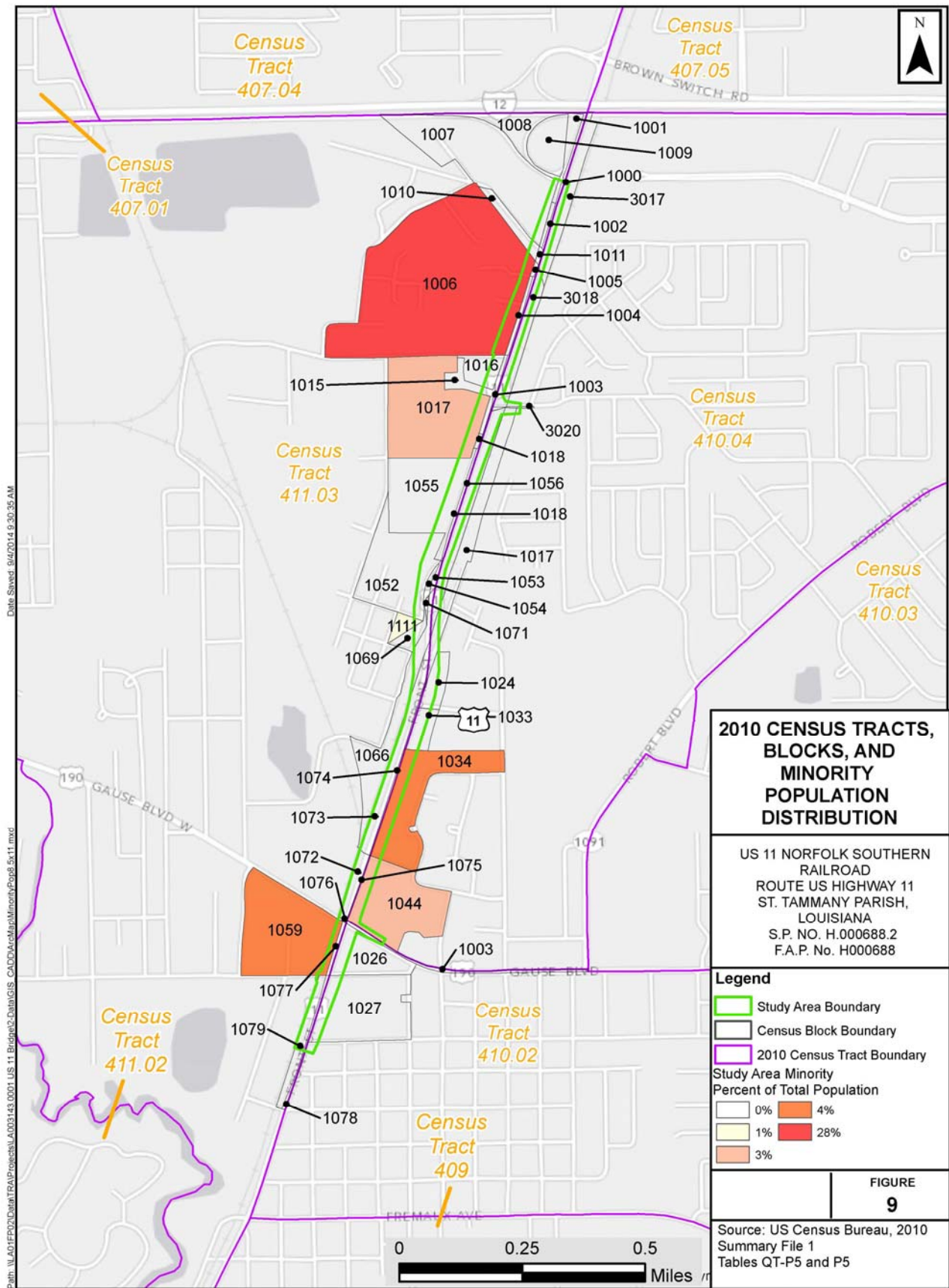
Source: US Census Bureau, 2010 Summary File 1; Tables QT-P5 and P5., B03002 2008-2012 America Community Survey 5-Year Estimates

\*AIAN - American Indian and Alaskan Native, NHPI - Native Hawaiian and Other Pacific Islander.

Excludes Hispanic or Latino of Any Race to avoid double counting. See Note Below.

Notes: Geographic Area was determined to be blocks within Census Tracts 410.02, 410.04, and 411.03 that intersect the Study Area.

The race concept "alone or in combination" includes people who reported a single race alone (e.g., Asian) and people who reported that race in combination with one or more of the other race groups. The sum of the six individual race "alone or in combination" categories may add to more than the total population because people who reported more than one race are tallied in each race category. Likewise the "alone" categories may add to less than the total population.





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### 4.4.3 Low-Income Populations

The Census Tracts that intersect the Study Area represent the demographic area evaluated for low-income populations. The median household income and persons of poverty status were examined in order to identify the presence or absence of low-income populations within the Study Area and determine if the proposed project would impact low-income persons. The poverty level was determined based on the 2013 U.S. Department of Health and Human Services poverty threshold of \$23,550 for a family of four.

Table 11 presents the estimated number of households, median household income, and households below the poverty level within Census Tracts 410.02, 410.04, and 411.03. Slightly more than 11 percent of households within the Census Tracts that intersect the Study Area are below the poverty level.

Neither the Selected Alternative nor the No Build Alternative would have a disproportionate impact on low-income populations.

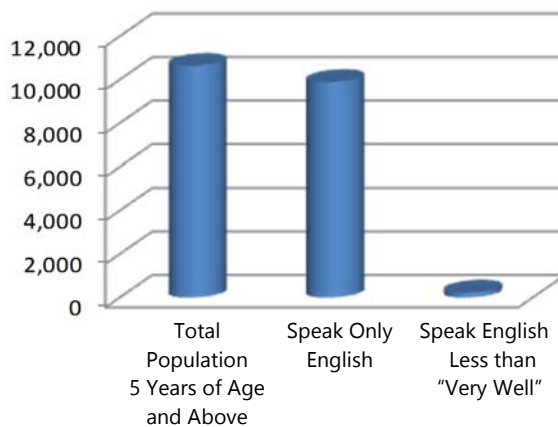
### 4.4.4 Limited English-Speaking Proficiency

Executive Order 13166, *Improving Access to Services for Persons with Limited English Proficiency* (LEP) (2001), requires federal agencies to work to provide meaningful access to LEP applicants and beneficiaries. 2010 Census data were reviewed for Language Spoken at Home by Ability to Speak English for the Population 5 Years

and Over in the Study Area. Less than 3 percent of the Study Area population speaks English "less than very well". Other languages spoken by a small percentage of the Study Area population are Spanish or Spanish Creole and French. Figure 10 shows LEP for the population within the Study Area.

The No Build Alternative would not have an impact on LEP populations within or adjacent to the Study Area.

**Figure 10: Limited English Proficiency Populations**



Source: U.S. Census Bureau, 2006-2010 American Community Survey 5-Year Estimates. Table B16001.

### 4.4.5 Environmental Justice

Per Executive Order 12989, the Study Area was examined to determine if the proposed project would disproportionately affect minority and

**Table 11: Median Household Income and Poverty Status**

Geographic Area	2012 Households <sup>(1)</sup>	Median Household Income	Households Below Poverty Level <sup>(2)</sup>	
			Number	Percent of Census Tract
Census Tract 410.02	1,104	\$49,423	210	19.0
Census Tract 410.04	1,590	\$61,333	85	18.7
Census Tract 411.03	738	\$49,636	10	1.4
<b>Total</b>	<b>3,432</b>		<b>305</b>	<b>11.3</b>

Source: U.S. Census Bureau, 2008-2012 American Community Survey 5-Year Estimates S1903 ([www.census.gov](http://www.census.gov)).

Note: Geographic Area was determined to be the Census Tracts that intersect the Study Area.

<sup>(1)</sup> Total Households within Census Tracts 410.02, 410.04, and 410.03.

<sup>(2)</sup> Households below the poverty level were determined based on 2008-2012 American Community Survey 5-Year Estimates, B25121 and 2013 U.S. Department of Health and Human Services poverty threshold of \$23,550 for a family of four.

low-income populations. Concentrations of minority populations were identified within the Study Area by mapping the census block populations of individuals self-identified as Black/African American, American Indian, Alaska Native, Native Hawaiian, Other Pacific Islander, Other Race and/or Two or More Races for the 2010 U.S. Census. Two census blocks have minority populations of 48 and 21 percent, respectively. The remaining census blocks within the Study Area have minority populations of 12 percent or less. Because improvements associated with the Selected Alternative are located along the existing US 11 roadway, the proposed project will not have disproportionately high adverse effects on minority populations.

Slightly more than 11 percent of households within the Census Tracts that intersect the Study Area are below the poverty level.

It is expected that the proposed project will not raise environmental justice issues. The No Build Alternative would not raise environmental justice issues.

## 4.5 Natural and Physical Environment

### 4.5.1 Geology and Soils

The Study Area is located within the Gulf Coastal Plain of Louisiana and lies on the Mississippi Embayment, a sedimentary sequence thousands of meters thick, which includes mostly unconsolidated clays, silts, sands (Earth Search, Inc. [ESI] 2014). Surface exposures consist of Quaternary sediment occupied by Pleistocene terraces. According to the USGS, Slidell, Louisiana, 7.5-minute quadrangle map (2012), topography in the Study Area is associated with the broad flats and low ridges characteristic of the southeastern part of the parish. Elevations across the Study Area rise from 10 feet at the US 11/US 190 intersection to 20 feet at the US 11/I-12 intersection.

Soils within the Study Area are primarily composed of the Myatt-Stough-Prentiss map unit classified by the U.S. Department of Agriculture, *Soil Survey of St. Tammany Parish,*

*Louisiana* (March 1990). These soils are a mix of level and very gently sloping, poorly to moderately well drained soils that are loamy throughout. This association is located on broad terraces in the southern part of the parish. Myatt soils are level, poorly drained sandy loam soil located on flats and in depressional areas and drainageways. Stough soils are level and somewhat poorly drained sandy loam soil and occur in higher positions relative to Myatt soils. Prentiss soils also are level, very gently sloping, and moderately well drained sandy loam soils located on low ridges.

Figure 11 shows the soil series, or groups, mapped within the Study Area as well as the estimated percent coverage of each soil within the Study Area based on information provided by the Natural Resources Conservation Service (NRCS).

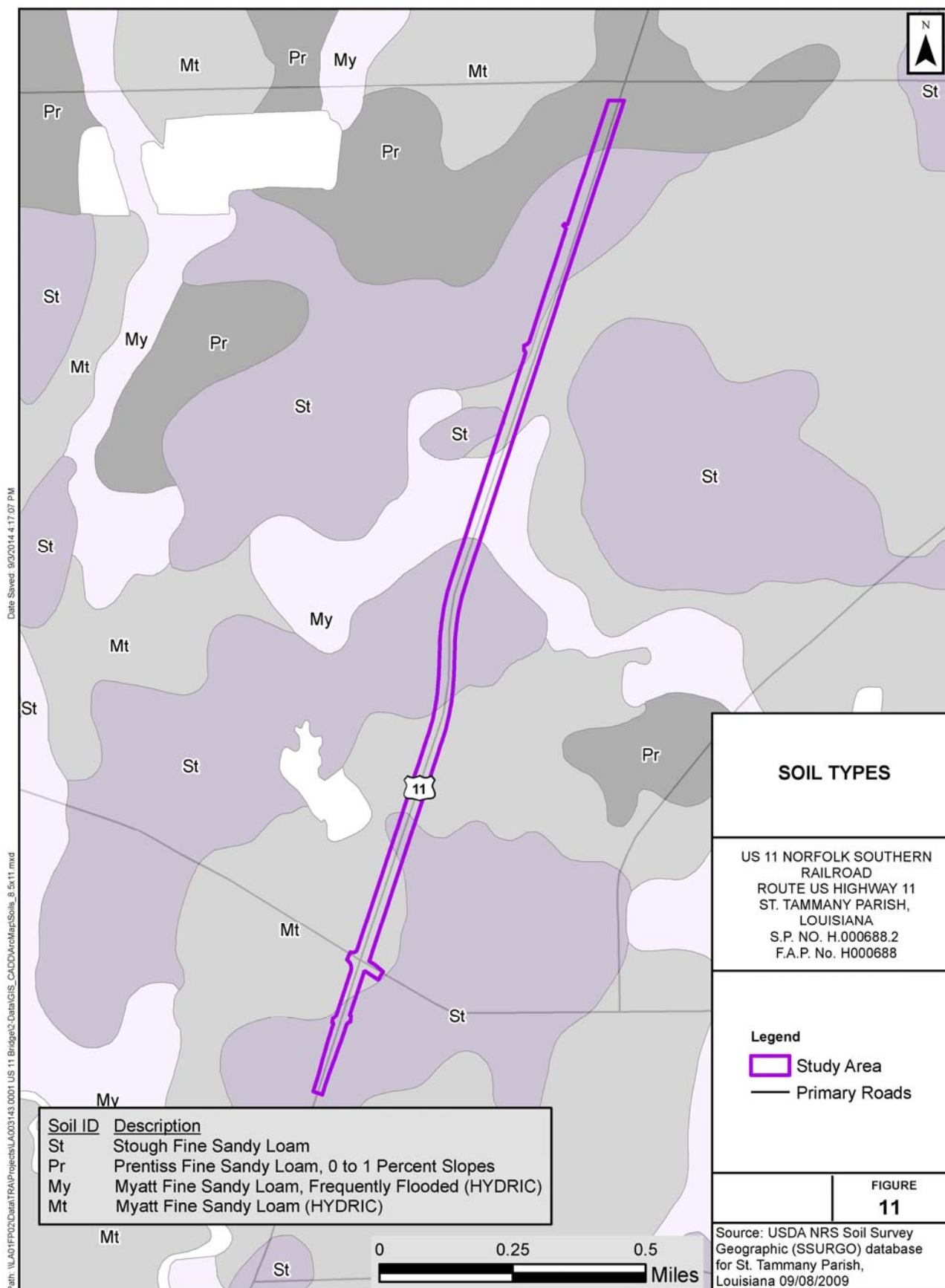
The Myatt soils are categorized within Hydrologic Group D which has very slow infiltration rates, a permanent high water table, and a clay layer near the surface and are considered hydric. The Stough and Prentiss soils are categorized within Hydrologic Group C, which have somewhat restricted infiltration rates, and a water table within 2 feet of the surface.

### 4.5.2 Farmland Protection Policy Act

The U. S. Department of Agriculture (USDA), through the NRCS, administers the *Farmland Protection Policy Act 1983 Subtitle I of Title XV, Section 1539 – 1549* (FFPA). The purpose of the FFPA is to “minimize the extent to which federal programs contribute to the unnecessary and irreversible conversion of farmland to nonagricultural uses.” The NRCS defines prime farmland and soils as those that have the best combination of physical and chemical characteristics to economically produce high yields of agricultural crops when treated and managed according to acceptable farming practices.

To ensure compliance with the FFPA, agency coordination with the NRCS, Alexandria, Louisiana, was initiated May 3, 2013 (Appendix C).

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In a letter dated May 6, 2013, the NRCS determined that the proposed project will not impact soils that are classified as prime, unique, or of statewide or local importance. Therefore, the proposed project is exempt from FPPA rules and regulations due to its location within an urban area.

The Selected Alternative would result in minimal disturbance to soils and geologic resources and is primarily located within existing roadway ROW. As such, these areas have been previously disturbed and no impacts are anticipated.

The No Build Alternative would not impact the geology, soils, or farmlands.

#### 4.5.3 Water Resources

The Study Area is located within the Pontchartrain Basin of Louisiana, which is bounded by the state of Mississippi to the north, the Gulf of Mexico to the south, the Pearl River to the east, and the Mississippi River to the west. Surface waters identified within the Study Area include two southeast Louisiana (SELA) Urban Flood Control drainage canals, which flow into Bayou Vincent and Bayou Bonfouca and discharge into Lake Pontchartrain south of the Study Area (Figure 11).

The existing roadway spans two drainage canals with concrete box culverts. The culvert for the SELA stream south of the US 11 bridge will be extended to accommodate the additional lanes. The box culvert north of the US 11 bridge will be replaced to accommodate the widened roadway and proposed bridge.

The No Build Alternative would not impact surface waters located within the Study Area.

The Louisiana Scenic Rivers Act of 1988 established the Louisiana Natural and Scenic Rivers System which is intended to protect, conserve, and replenish the natural resources of the state including certain free-flowing streams or segments.

Neither the Selected Alternative nor the No Build Alternative would impact natural and scenic rivers.

A floodplain evaluation was conducted in accordance with Executive Order 11988,

*Floodplain Management (1977)*, 23 Code of Federal Regulations (CFR) 650, Subpart A "Location and Hydraulic Design of Encroachments on Floodplains" and U.S. Department of Transportation (DOT) 5650.2 "Floodplain Management and Protection".

The location of the 100-year floodplain for the Study Area was identified from Federal Emergency Management Agency (FEMA) Digital Flood Insurance Rate Maps (FIRM) and is shown on Figure 12. Special Flood Hazard Area designations are indicated as Zone A and Zone X within the Study Area. Zone A designates the flood expected to occur on average once every 100 years. Zone X designates the flood expected to occur once every 500 years.

The Selected Alternative impacts approximately 21 acres of floodplain area (Table 12).

There is no practicable alternative to the proposed location of the Selected Alternative that

**Table 12: Floodplain Impact by Alternative**

Alternative	Flood Zone (Acres)		
	A	X	X500
Alternative 1	20	11	2
Alternative 1R	20	11	2
Alternative 1R Modified	21	11	2
No Build	0	0	0

Sources: *Flood Insurance Rate Map, City of Slidell, Louisiana, St. Tammany Parish*. Community Panel Number 220204 0010 C, Revised April 21, 1999 and *St. Tammany Parish, Louisiana* Community Panel No. 225205 0410D, Revised April 21, 1999.

does not cross floodplains. The Selected Alternative includes all practicable measures to minimize floodplain impacts.

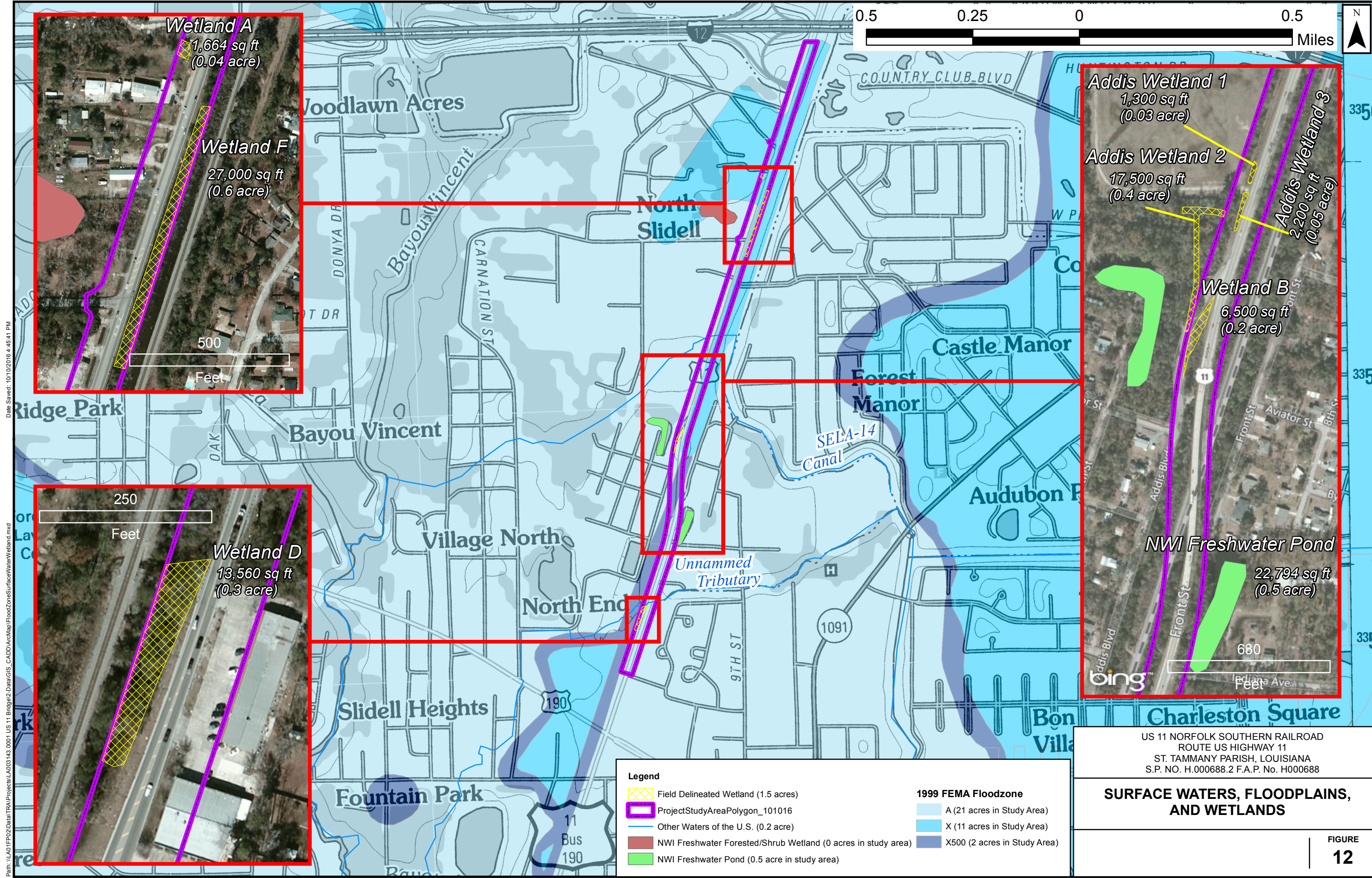
The No Build Alternative would not further impact floodplains within the Study Area.

#### 4.5.4 Wetlands

All wetlands identified within the Study Area were evaluated in accordance with Executive Order 11990, *Protection of Wetlands (1977)*, and the technical guidelines and methods for wetland delineations as set forth in the U.S. Army Corps of

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US 11 NORFOLK SOUTHERN RAILROAD  
ROUTE US HIGHWAY 11  
ST. TAMMANY PARISH, LOUISIANA  
S.P. NO. H.000688.2 F.A.P. No. H000688

**SURFACE WATERS, FLOODPLAINS,  
AND WETLANDS**

**FIGURE  
12**



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Engineers (USACE) *Wetland Delineation Manual* (1987) and the *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Atlantic and Gulf Coastal Plain Region Wetland Delineation Manual* (2010).

An initial site visit was conducted on May 9, 2013, to visually assess the Study Area and note the location of probable wetlands. A formal delineation followed on June 18, 2013. Additional field work was conducted on October 4, 2016, in the area of the proposed Addis Boulevard realignment.

Each wetland site was documented with photographs and field notes, and boundaries were delineated and mapped using a sub-meter global positioning system unit. Observations of vegetation, hydrology, soils, and other visible wetland indicators were recorded on *Wetland Determination Forms – Atlantic and Gulf Coastal Plain Region*.

Table 13 lists wetland impacts by alternative. The Selected Alternative impacts approximately 1.5 acres of wetland area. A detailed analysis and description of wetlands and other waters identified within the Study Area can be found in the Biological Resources and Wetland Findings Report (Appendix CD-2).

Wetlands lost from construction of the proposed project would be replaced through mitigation. Mitigation comprises measures which avoid, minimize, and/or compensate for unavoidable losses to resources that cannot be further minimized. The assessment of mitigation measures (avoidance, minimization, and compensation) is an integral part of the NEPA/Section 404 process. For those impacts that cannot be avoided, other mitigation efforts must be considered. These efforts include minimization of potentially adverse impacts and compensation for those remaining adverse impacts that cannot be reduced any further.

Construction activities associated with the build alternative would impact wetlands and surface waters to varying degrees. Land clearing during

construction would remove vegetative cover with the potential to increase surface runoff during storm events leading to erosion and increased sediment deposited in surface waters.

To aid in minimizing such impacts, placement and monitoring of erosion control measures for soil stabilization along with temporary and permanent vegetation measures at the start of, during, and after construction would be incorporated into project construction plans according to LADOTD's standard specifications.

Measures to minimize impacts to wetlands may

**Table 13: Wetland Impact by Alternative**

Alternative	Wetland Area (acres)
Alternative 1	0.9
Alternative 1R	1.1
Alternative 1R Modified	1.5
No Build	0

Sources: *Flood Insurance Rate Map, City of Slidell, Louisiana, St. Tammany Parish*, Community Panel Number 220204 0010 C, Revised April 21, 1999.

*Flood Insurance Rate Map, St. Tammany Parish, Louisiana* Community Panel Number 225205 0410D, Revised April 21, 1999.

include minimizing clearing of wetland vegetation to the limits of construction and minimizing use of wetland areas outside the construction limits for construction support activities (borrow sites, waste sites, storage, parking, access, etc.).

Final compensatory mitigation ratios and requirements for impacted areas classified as jurisdictional will be determined by the USACE New Orleans District through the Section 404 permit process.

It has been determined that there is no practicable alternative to the proposed construction involving impacts to wetlands and the proposed action includes all practicable measures to minimize harm to wetlands which may result from this project.

The No Build Alternative would not impact area wetlands and other waters of the U.S.

### 4.5.5 Coastal Zone

The LDNR Office of Coastal Management administers the Louisiana Coastal Resources Program (LCRP) in accordance with the federal Coastal Zone Management Act (CZMA). The proposed improvements are located within the coastal zone and are a use of state concern. A Coastal Use Permit (CUP) in compliance with the LCRP will be required for the project in conjunction with the USACE, New Orleans District.

### 4.5.6 Biological Resources

Section 7 of the Endangered Species Act of 1973 (as amended) requires that federal agencies ensure any action authorized, funded, or carried out by that agency is not likely to adversely impact threatened or endangered species or result in destruction of critical habitat. Coordination with U.S. Fish & Wildlife Service



**Wetland Area near Addis Boulevard and US 11.**

(USFWS), Louisiana Ecological Services Office, and the Louisiana Natural Heritage Program (LNHP) was made as part of the Solicitation of Views (SOV) process to determine if known rare, threatened, or endangered species exist within the Study Area.

In a response to a request for federal trust resources review for the proposed project

(Appendix C), the USFWS responded to the proposed project stating it "is not likely to adversely affect those resources. This finding fulfills the requirements under Section 7(a)(2) of the Act."

The LNHP maintains a database with known locations of federally listed threatened and endangered species as well as state species of special concern. The LNHP responded to the SOV stating that no impacts to rare, threatened, or endangered species or critical habitats are anticipated for the proposed project. The response also stated that no state or federal parks, wildlife refuges, scenic streams, or wildlife management areas are known to be at the project location (Appendix C).

The Study Area is located within an urban setting and proposed project improvements will primarily occur along the existing US 11 roadway facility which is bounded by railroad, commercial, and residential development. Additional ROW required for roadway widening will include previously developed or disturbed areas. Previous disturbance within the Study Area has resulted in habitat that is mainly early successional stands or young forests, weedy areas, and shrub thickets. Wetlands within the Study Area are a consequence of impoundment from construction of the railroad, US 11, the US 11 bridge embankments, and collector roads.

The Study Area does not likely contain habitat that is suitable to support rare, threatened, or endangered species.

The No Build Alternative would not impact threatened and endangered species or critical habitat.

## 4.6 Historic and Cultural Resources

Section 106 of the NHPA of 1966 (as amended) protects those properties that are listed in or eligible for listing in the NRHP. In accordance with the requirements of Section 106, an assessment was made of the cultural resources within the Study Area.

Methods used in this review and assessment were consistent with the applicable federal and Louisiana guidelines for conducting cultural and historic resource studies. Project-specific cultural resources data, as well as recorded archaeological sites and historic standing structures, were obtained from a review of archaeological site forms and reports on previous cultural resources surveys on file at the Division of Historic Preservation (DHP) Louisiana Department of Culture, Recreation & Tourism (LDCRT), and the SHPO.

A Phase 1 cultural resources survey of the proposed project area was conducted. The direct Area of Potential Effects (APE), including existing and required ROW for all alternatives, was examined (Figure 13). The direct APE comprised approximately 35 acres. The standing structure survey examined the indirect APE, which included the existing and required ROW including a 0.25-mile buffer to either side of the highway. This area is comprised of approximately 45 acres (Figure 12).

#### 4.6.1 Archaeological Resources

Identification and assessment of potential cultural resources were conducted for the APE and included all areas that could include cultural resources and be directly or indirectly impacted by the proposed project. A geomorphological assessment of the APE was completed in order to determine the potential for the area to have fostered human development or to have been preserved. An overview of the region's prehistory is provided in the *Phase 1 Cultural Resources Survey for the Environmental Assessment of The US 11 Norfolk Southern Railroad, Route US 11, St. Tammany Parish, Louisiana* (2014) which has been submitted to LDCRT as required under Section 106 of the NHPA.

A cultural resource investigation was completed in order to locate all archaeological remains within the APE and to assess their significance. A records search was conducted at the Division of Archaeology (DOA). The DOA maintains archaeological site information for the State of

Louisiana including USGS 7.5-minute quadrangle maps depicting the locations of all recorded archaeological sites, site forms, and corresponding reports. Examination of these records indicates that three archaeological sites exist within the direct APE; however, none of the sites are listed on or are eligible for nomination to the NRHP. The field survey revealed no evidence for intact archaeological deposits based on shovel tests excavated within the APE.

The Build Alternative and No Build Alternative would not impact archaeological resources.

#### 4.6.2 Historic Resources

Identification and assessment of potential historic resources was conducted for the direct and indirect APE (Figure 13) and included all areas that could include historic resources and be directly or indirectly impacted by the proposed project.

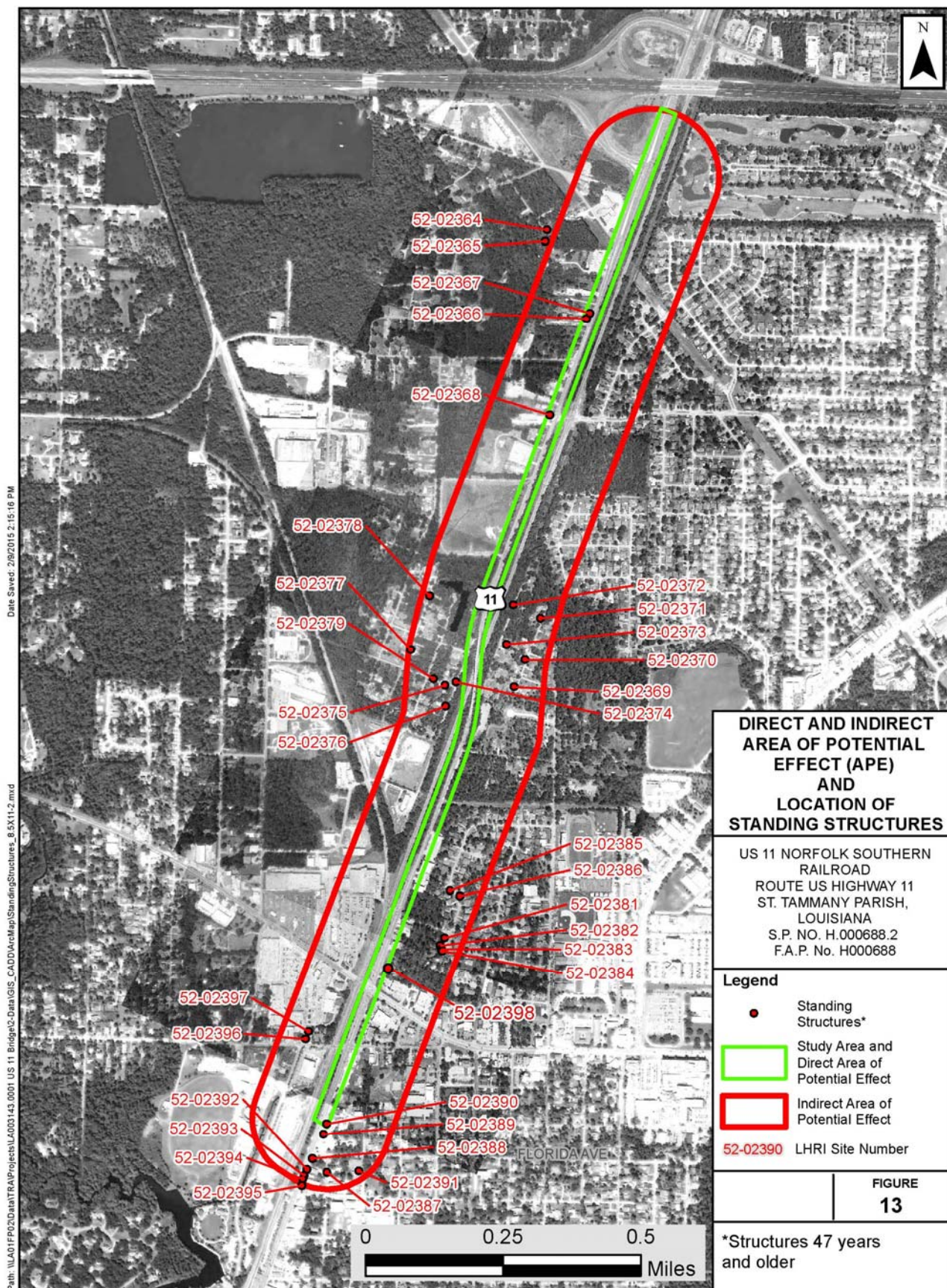
Prior to the field survey, ESI conducted a records search at the DHP. The DHP maintains Louisiana Historic Resource Inventory (LHRI) and NRHP files for the State of Louisiana. Review of these files indicated that there are no NRHP properties within the indirect APE for the project. All structures that predate 1967 were recorded on LHRI forms and photo documented.

The field survey identified 35 buildings within the indirect APE that are at least 47 years of age or



1920s Residence located at 1184 Front Street (US 11) (LHRI 52-02398).







greater (Figure 12). Most of the structures, which are a mix of single-family and commercial buildings, are not individually eligible for nomination to the NRHP or as contributing elements to an NRHP district. Seven structures located within the APE have been recommended as eligible nominations to the NRHP. Five of the structures contribute to the Old Town Slidell historic district and were identified as The Rock Church (52-02392), the Furniture Shop (52-02395), Chateau Bleu (52-00078), the St. Tammany Chamber of Commerce (52-00079), and 208 West Hall Avenue (52-00080). Two structures, the Old Slidell Icehouse (52-02387) and a 1920s residence (52-02398), are located along US 11.

Proposed improvements for the Build Alternative will avoid impacts to the seven structures identified as eligible NRHP listings.

The No Build Alternative would not impact historic resources.

#### 4.6.3 Aesthetic and Visual Resources

Louisiana's aesthetic and visual resources are an important component of the state's tourism industry and contribute significantly to the quality of life in Louisiana. These resources include a broad range of natural and developed areas from the coastal marshlands and swamps along the Gulf Coast to the rich cotton fields of North Louisiana, from its historic cities and towns to its forestlands and wildlife.

The visual experience and aesthetic quality of an area depend upon the pattern of land or topography, the pattern of water bodies, vegetation, and human development (FHWA 1990). More specifically, factors used to assess a person's visual experience and the aesthetic quality of an area may include:

- Scale of the elements in the scene;
- Number of potential viewers;
- Duration of the view; and
- Amount of disturbance to the landscape.

The Study Area includes part of a state highway system adjacent to an area that is suburban residential and commercial in character. Human development is visually prominent along US 11 at Gause Boulevard where the architecture is typical late 20th century shopping center and at the US 11 and I-12 interchange with residential development predominantly comprised of 1960s and older style homes.

The viewshed also includes the US 11 bridge over the NS railroad, billboard and business signs, metal canopies, utility poles, concrete driveways, and parking lots. There would be no change to the nightscape, which is moderately accented with artificial light from shopping centers, fuel stations, street lights, and residential security lights.

The Selected Alternative would not noticeably change the obscured view of the landscape from ground level.

Temporary construction impacts due to clearing will detract from the view at Addis Boulevard, Indiana Avenue, and Harvey Avenue. Tree growth would restore the current viewshed and partially obscure the build alternative within 15 years. The viewshed throughout the remainder of the Study



Viewshed looking north along US 11 from US 190.

Source: Google Maps ©2014 Google



Area will be minimally disturbed because the widening will be implemented along the existing US 11 alignment. The Selected Alternative is anticipated to have minimal adverse impacts to the aesthetic and visual resources in the Study Area.

The No Build Alternative would not impact aesthetic and visual resources.

### 4.7 Section 4(f) and 6(f) Resources

Section 4(f) of the DOT Act of 1966 stipulates that FHWA cannot approve the use of land from publicly owned parks, recreational areas, wildlife and waterfowl refuges, or public and private historic sites, unless there is no feasible and prudent avoidance alternative following all possible planning to minimize harm to the property; or if the use of the land would have only a *de minimis* (very minor and acceptable) impact, or no adverse effect, to key features of the property.

The bridge over the NS railroad was identified as eligible for the NRHP under the Louisiana Historic Bridge Inventory (Mead & Hunt 2013). The bridge was identified as eligible under Criterion A for its association with important “events that have made a significant contribution to the broad patterns of our history”. During the 1930s, increased attention was given to creating grade separations between railroads and roadways. Specific federal legislation was passed which resulted in the U.S. Works Grade Crossing Program funding. The US 11 bridge over the NS railroad was constructed utilizing funds from this program. This information was submitted to the SHPO, who concurred that the existing US 11 Bridge over the NS railroad was eligible for the NRHP. This determination signified that the adverse effect from the potential demolition of the bridge would require compliance with the statewide *Programmatic Agreement Regarding Management of Historic Bridges in Louisiana* (Sept 21, 2015) to demonstrate that FHWA and LADOTD have complied with Section 106 of the NHPA.

Because the bridge cannot be rehabilitated to meet the purpose and need of the project without adversely affecting its historic integrity, it must be taken out of service by demolition and replaced. Requirements to assess whether all possible planning has been considered and whether there is a feasible and prudent avoidance alternative is specified in the *Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges* (FHWA 1983). These requirements were applied to ensure that mitigation of adverse effects resulting from replacement of the bridge are considered. The proposed project meets all the applicability criteria specified in the *Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges* and may be approved for transportation use based on the programmatic evaluation provided in Appendix A.

Section 6(f) of the Land and Water Conservation Act requires that unavoidable conversion of lands or facilities acquired or developed with Land and Water Conservation Act funds be replaced in kind or coordinated with the Department of Interior. No Section 6(f) lands would be impacted by the proposed project.

The No Build Alternative would not impact parks and public lands.

### 4.8 Noise

Noise, by definition, is unwanted sound that interferes with normal activities and would not be considered a resource, but rather a condition that potentially affects both the human and natural environment. Noise is described in terms of loudness, frequency, and duration and is emitted from many sources, including airplanes, factories, railroads, power generating plants, and highway vehicles. Highway noise, or traffic noise, is usually a composite of noises from engine exhausts, drive trains, and tire-roadway interaction.

The magnitude of noise is usually described by its sound pressure. Because the range of sound pressure varies greatly, a logarithmic scale is used to relate sound pressures to some common reference level, particularly the decibel. Sound

pressures described in decibels are called sound pressure levels and are often defined in terms of frequency-weighted scales (A, B, C, or D).

For a community noise impact assessment, the A-weighted scale is used almost exclusively in vehicle noise measurements because it places most emphasis on the frequency characteristics that correspond to a human's subjective response to noise (1,000 to 6,000 Hertz). Sound levels measured using A-weighting are often expressed as A-weighted decibels (dBA).

A noise monitoring program was conducted within the Study Area (Appendix CD-3) to establish existing sound levels in accordance with the LADOTD Highway Traffic Noise Policy (2011). Fourteen field-measured noise locations were identified for the collection of existing sound levels along roadways within the Study Area. Existing noise levels ranged from 45.1 to 66.1 dBA. Traffic noise Site ID E2 (collected during traffic peak) had the highest noise level at 66.1 dBA measured north of the intersection of US 11 with Lafayette Street. The lowest noise level measured was 45.1 dBA at traffic noise Site ID C3 (collected during traffic off peak) northwest of the intersection of Ben Thomas Road and Carnation Street.

The dominant noise source at each receiver site is existing traffic including automobiles, heavy

trucks, and medium trucks and is usually a composite of noises from engine exhausts, drive trains, and tire/roadway interaction.

Future traffic noise level predictions were performed using the FHWA Traffic Noise Model 2.5 (TNM 2.5). The difference between the field-measured sound levels and TNM-calculated sound levels is within the acceptable range of  $\pm 3$  dBA (the amount of sound that is barely perceptible by the human ear) at all locations where existing measurements were taken.

A total of 199 noise receivers (representing a total of 285 dwelling units) were modeled within the Study Area.

As presented in Table 14 and shown on Figure 14, the 2013 existing conditions exterior sound levels at 4 receiver locations approach or exceed the Noise Abatement Criteria (NAC).

In the 2042 Alternative 1 build scenario, growth in traffic volumes and the proposed changes in horizontal and vertical alignment will cause exterior sound levels at 10 receiver locations to approach or exceed the NAC. None of these receiver locations will experience a substantial increase in noise level (Figure 14). The 2042 Alternative 1R and 1R Modified build scenario results showed no noticeable differences when compared to Alternative 1 results (Figure 14).

**Table 14: Traffic Noise Impact Summary by Alternative**

Conditions		Total Number		Approaching or Exceeding LADOTD NAC		Impacted under Substantial Increase Criteria		Total Impacted	
		R	DU	R	DU	R	DU	R	DU
2013 Existing Conditions		199	285	4	4	N/A	N/A	4	4
2042 No Build Conditions		199	285	7	7	0	0	7	7
2042 Build Conditions	Alternative 1	193	272	10	10	0	0	10	10
	Alternative 1R	193	272	10	10	0	0	10	10
	Alternative 1R, Modified	193	272	5	5	0	0	5	5

N/A Not applicable for the listed alternative  
 Alt Alternative  
 R Receiver  
 DU Dwelling Unit  
 NAC Noise Abatement Criteria

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In the 2042 No Build condition, growth in traffic volumes will cause exterior sound levels at 7 receiver locations to approach or exceed the NAC. None of these receiver locations will experience a substantial increase in noise level (Figure 15).

Traffic noise impacts occur when the predicted traffic sound levels equal or exceed the NAC, or when the predicted traffic sound levels exceed existing levels by 10 dBA.

Table 15 describes the LADOTD NAC threshold values that represent the noise level at which abatement measures, like noise walls, must be evaluated.

Noise abatement with vegetative or structural barriers is evaluated for both feasibility and reasonableness. For feasibility, a 5-dBA reduction in noise is considered to be a benefited receptor and at least one benefited receptor must receive an 8-dBA reduction in noise and the average cost per benefited receptor must not exceed \$35,000 to be considered reasonable.

Various noise abatement measures were reviewed to mitigate noise impacts and protect public health in the vicinity of the proposed project. All impacted receivers were reviewed in detail for noise abatement. The types of abatement considered include: acquisition of ROW and land use designations, traffic management strategies, alignment alterations, and use of vegetative or structural barriers. None of the abatement measures considered were considered to be feasible. Land use in the area is already established. Traffic management cannot be enforced along this route due to its intended use as a principle arterial. Alignment alterations is not feasible due to the nature of this established roadway in the project area. Barriers were not feasible due to impacts to driveways along the corridor. Any barrier constructed providing a reasonable noise reduction would prevent access to the properties they shield. Noise barriers were also not considered reasonable due to the generalized cost of the structures. Results of the noise abatement analyses are included in Appendix CD-3.

**Table 15: Noise Abatement Criteria**

Activity Category	Hourly A-weighted Decibels <sup>1</sup>	Activity Category Description
A	56 (exterior)	Land on which serenity and quiet are of extraordinary significance and serve an important public need, and where the preservation of those qualities is essential if the area is to continue to serve its intended purpose.
B	66 (exterior)	Residential.
C	66 (exterior)	Active sports areas, amphitheaters, auditoriums, campgrounds, cemeteries, daycare centers, hospitals, libraries, medical facilities, parks, picnic areas, places of worship, playgrounds, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, recreation areas, Section 4(f) sites, schools, television studios, trails, and trail crossings.
D	51 (interior)	Auditoriums, daycare centers, hospitals, libraries, medical facilities, places of worship, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, schools, and television studios.
E	71 (exterior)	Hotels, motels, offices, restaurants/bars, and other developed land, properties, or activities not included in A through D or F.
F	–	Agriculture, airports, bus yards, emergency services, industrial, logging, maintenance facilities, manufacturing, mining, rail yards, retail facilities, shipyards, utilities (water resources, water treatment, electrical), and warehousing.
G	–	Undeveloped land that is not permitted.

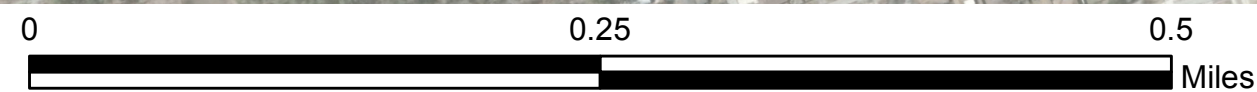
<sup>1</sup>Hourly A-weighted equivalent noise level in dBA - L<sub>eq</sub> (hour).  
Source: LADOTD Noise Policy.





**Legend**

- 2042 Impact
- ✚ 2013 Impact & 2042 Impact



**NOISE RECEIVER IMPACTS, 2013  
EXISTING CONDITIONS AND  
2042 NO BUILD  
FIGURE 14**

US 11 NORFOLK SOUTHERN RAILROAD  
ROUTE US HIGHWAY 11  
ST. TAMMANY PARISH, LOUISIANA  
S.P. NO. H.000688.2 F.A.P. No. H000688

Path: \\UA01FP020Data\TRA\Projects\LA003143.0001 US 11 Bridge\6-TRF2 Analysis\GIS - Noise\mxd\Figure 4 - Noise Receiver Impacts 2013 and 2042 Conditions\3.mxd  
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Service Layer Credits: Source: Esri, DigitalGlobe, GeoEye, i-cubed, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community



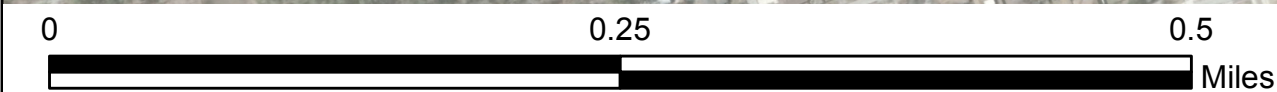
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**Noise Receiver Impacts,  
2042 Alternative 1  
and 1R Conditions**  
**FIGURE 15**  
US 11 NORFOLK SOUTHERN RAILROAD  
ROUTE US HIGHWAY 11  
ST. TAMMANY PARISH, LOUISIANA  
S.P. NO. H.000688.2 F.A.P. No. H000688



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## 4.9 Air Quality

The Clean Air Act Amendments of 1990 (CAAA) requires that a proposed project not cause any new violation of National Ambient Air Quality Standards (NAAQS), or increase the severity of existing violations, or delay attainment of NAAQS. National and state ambient air quality standards, developed for specific (criteria) pollutants to protect public health, safety, and welfare, are established in the CAAA.

The U.S. Environmental Protection Agency (USEPA) and Louisiana Department of Environmental Quality (LDEQ) are responsible for the protection of air quality within Louisiana. The USEPA established NAAQS for six air pollutants: carbon monoxide (CO), lead (Pb), nitrogen dioxide (NO<sub>2</sub>), sulfur dioxide (SO<sub>2</sub>), ozone (O<sub>3</sub>), and particulate matter of 10 microns (PM-10) or less in size. NAAQS requires the transportation sector to meet specified standards for PM-10, CO, and ozone at ground level. Unlike PM-10 and CO, ozone is not directly emitted, but created by a chemical reaction between nitrogen oxides (NO<sub>x</sub>) and volatile organic compounds (VOCs) in the presence of sunlight. Ground-level ozone is the primary component of smog.

Air quality is defined by primary standards which refer to air quality levels required to protect public health within an adequate margin of safety. Secondary standards refer to air quality levels required to safeguard visibility, comfort, animals, and property from poor air quality. The CAAA requires that transportation plans, programs, and projects funded or approved by FHWA be in conformity with the State Implementation Plan (SIP), which represents the state's plan to either achieve or maintain the NAAQS for a particular pollutant.

Transportation conformity is a process required of MPOs, pursuant to the CAAA, to ensure that federal funding and approval are given to those transportation activities that are consistent with air quality goals. As the agency responsible for regional transportation planning, the RPC leads the analysis of the region's transportation sector

impacts to air quality. Currently, the Greater New Orleans region is designated as an area in attainment. St. Tammany Parish, the City of Slidell, and the Study Area are in "attainment" for the criteria pollutants.

Due to the region's compliance with NAAQS, the RPC is not required to produce an air quality conformity analysis at this time. The region's last air quality conformity analysis was performed in 2004 in conjunction with the development of the 2027 MTP.

There are no air quality impacts for the Selected Alternative or No Build Alternative.

## 4.10 Hazardous Materials Sites, Underground Storage Tanks, Pipelines, and Wells

A standard environmental records review and site reconnaissance was conducted to locate sites of potential concern for hazardous materials or previously identified recognized environmental conditions (RECs) on properties within the Study Area. This environmental site assessment was completed utilizing the standard practices outlined in *ASTM International E1527-05: Standard Practice for Environmental Site Assessments: Phase I Environmental Site Assessment Process* in conjunction with 40 CFR Part 312.

Contamination of soils, groundwater, or surface waters can result from former use, storage, or disposal of hazardous materials on subject properties, or from migration of contaminants from adjacent properties. The purpose of conducting an environmental site assessment is to determine a property's potential for containing soil, groundwater, or surface water contamination with respect to the range of contaminants within the scope of the Comprehensive Environmental Response, Compensation and Liability Act (CERCLA) and petroleum products.

A REC is defined as the presence or likely presence of hazardous substances or petroleum products on a property under conditions that indicate an existing release, a past release, or an

## ENVIRONMENTAL ASSESSMENT

observable or obvious threat of a release of hazardous substances or petroleum products into structures on the property or into the ground, groundwater, or surface water of the property, excluding *de minimis* conditions that generally do not present a threat to human health or the environment and that generally would not be the subject of an enforcement action. A historical recognized environmental condition (HREC) is defined as an environmental condition that would have been considered a REC in the past, but may or may not be considered a REC currently.

A records search was conducted by Environmental Data Resources (EDR), Inc. (Appendix CD-4) for the Study Area and immediate surrounding area. Because EDR locates sites based on addresses, which are not always representative of the actual location of a site, the results of the EDR search were supplemented with a review of the LDEQ Electronic Document Management System (EDMS) records (Appendix CD-4). EDMS is LDEQ's electronic repository of official records that have been created or received by LDEQ.

Sites determined to be outside the Study Area were removed from consideration for further investigation because they would not present a material risk of harm to public health or the environment. Other sites listed in the EDR report are considered to represent *de minimis* conditions that generally do not present a material risk of harm to public health or the environment. These sites were also removed from further investigation.

None of the unmapped sites identified in the EDR report are located within the Study Area and have been removed from further investigation.

In addition, historical aerial photographs, historical topographic maps, and Sanborn® Fire Insurance Maps of the Study Area and adjoining properties were reviewed for evidence of environmental concerns.

Database searches were followed by a field reconnaissance of the Study Area, which also identified sites not documented in the environmental databases. Figure 16 shows

potential hazardous materials sites identified from the EDR report and EDMS review that are in proximity to all alternatives.

No HRECs were identified but several RECs were identified within the Study Area. Site No. 1, Jubilee Express Store #4815 (as of June 2019 Site No. 1 is a RaceTrac fuel station), and Site No. 9, Circle K #2896 (as of June 2019 Site No. 9 is vacant), are located within the Study Area. Site No. 8, Western International Gas & Cylinder, is located adjacent to the Study Area.

The following observation was made regarding Site No. 7, the former McMurray filling station. Historic State Highway Commission plans for the Slidell Overpass indicate the presence of a McMurray filling station located on the east side of US 11 approximately 150 feet south of Indiana Avenue. The location of the area identified as the new entrance on the historic plans is visible and extends east from the current location of US 11. The remainder of the site area is overgrown with vegetation. No other information indicating closure of underground storage tanks (USTs) or other regulated activities was available for review through EDMS.



Circle K located at 1150 Front Street (US 11) north of US 190 (vacant property as of June 2019).

Source: Arcadis







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Required ROW for lane widening along, and intersection improvements associated with, the Selected Alternative may impact Site Nos. 1 and 7. It is anticipated that UST facilities at Site Nos. 1 and 9 will not be impacted by required ROW although appurtenant piping may be impacted. Due to limited historical information regarding Site No. 7, direct impacts are not known. Caution should be taken during construction-related activities.

The No Build Alternative would not impact sites identified to have known potential environmental conditions that may have the presence or likely presence of hazardous substances or petroleum products or that pose a material threat of release.

Oil and gas and water well information was obtained from the Louisiana Department of Natural Resources (LDNR) Strategic Online Natural Resource Information System (SONRIS) database and a response from the LDNR Office of Conservation (Appendix C). Information collected indicates five active and six plugged and abandoned water wells are located within the Study Area. No recorded oil and gas wells are located within the Study Area. Figure 15 identifies the water wells within the Study Area.

The Selected Alternative and No Build Alternative would not impact water wells identified within the Study Area.

### 4.11 Traffic Pattern Changes

Due to the increased length of the proposed bridge crossing the NS railroad, Indiana Avenue will no longer have direct access to US 11.

Indiana Avenue currently exists as a through street from US 11 to 9th Street. The intersection of Indiana Avenue and US 11 is located immediately south of the existing bridge and is signed as a right in/right out intersection. Traffic will continue to utilize 9th Street to access US 11 via Dale Drive/Lafayette Street or US 190 (Figure 17).

Following the December 15, 2015, Public Hearing, realignment of Addis Boulevard was considered

and is a component of the Selected Alternative (Appendix B-3).

### 4.12 Temporary Construction Impacts

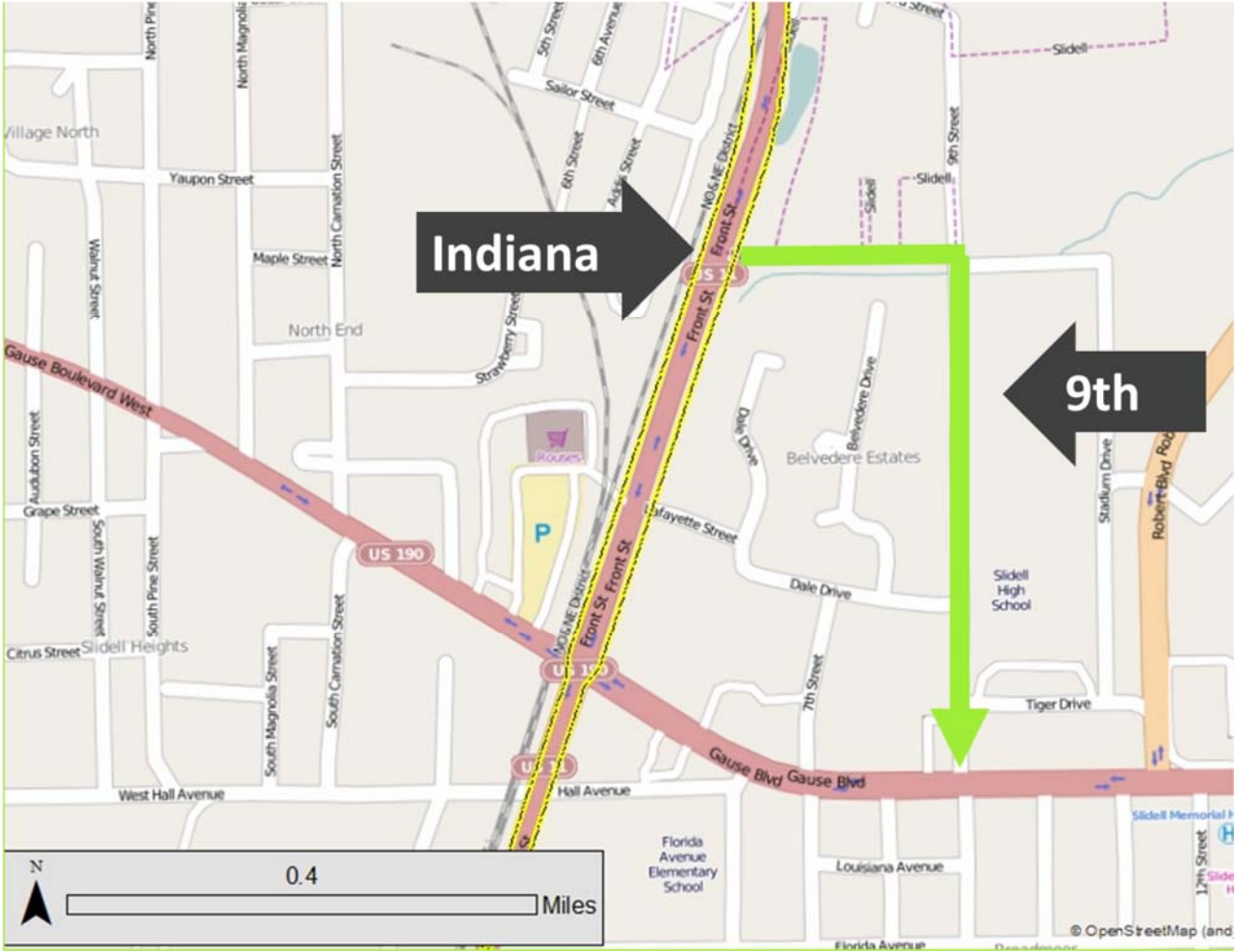
Short-term impacts associated with construction of the Selected Alternative are anticipated including erosion of areas cleared for construction, temporary increases in noise levels, and fugitive dust from use of heavy construction equipment. Temporary impacts to traffic flow and travel patterns are anticipated with construction of the Selected Alternative. These impacts would occur along existing roads and at intersections during construction activities. The Selected Alternative would impact traffic flow along US 11 and US 190. Local and through traffic would be maintained during construction in accordance with LADOTD's *Standard Specifications for Roads and Bridges*.

Utilization of maintenance of traffic flow practices including construction sequencing, timing, and signing would be implemented.

Because much of the proposed work will take place adjacent to high-volume roadways, worker and motorist safety is of paramount concern. Traffic control standards will be used to establish and maintain a safe work Alternative would impact traffic flow along US 11 and US 190. Local and through traffic would be maintained during construction in accordance with LADOTD's *Standard Specifications for Roads and Bridges*.

Utilization of maintenance of traffic flow practices including construction sequencing, timing, and signing would be implemented.

Because much of the proposed work will take place adjacent to high-volume roadways, worker and motorist safety is of paramount concern. Traffic control standards will be used to establish and maintain a safe work zone. Workers are required to meet LADOTD standards for worker visibility and equipment driven on roadways must meet proper signage and licensing requirements. The contractor will take appropriate measures to



**Figure 17. Indiana Avenue Access.** Indiana Avenue to 9th Street via existing roadways to US 11 or US 190.

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prevent, minimize, and control the spill of hazardous materials in the construction area.

The use of construction equipment within sensitive areas should be minimized and all construction materials used for this project should be removed as soon as the work schedule permits. Any unanticipated hazardous materials and/or petroleum contamination encountered during construction should be handled according to applicable federal and state regulations for handling emergency discovery of hazardous materials.

By adopting the safety and coordination efforts described above, it is anticipated that the Selected Alternative could be constructed with no adverse impacts to human health and safety or the environment.

### 4.13 Indirect and Cumulative Impacts

The Council on Environmental Quality regulations (40 CFR Parts 1500 through 1508) define three types of impacts routinely assessed for proposed federal actions. Direct impacts, which are effects caused by the action and occur at the same time; indirect impacts which are caused by an action and are later in time or farther removed in distance, but reasonably foreseeable; and cumulative impacts. Cumulative impacts include the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions which may become substantial in the aggregate as time passes.

NEPA requires that the effects of the proposed project be considered in combination with effects

from unrelated past, present, and reasonably foreseeable future actions as part of the decision-making process.

The Selected Alternative would convert a small amount of previously disturbed undeveloped land into transportation use. This will improve accessibility and may induce further residential and commercial development within or near the Study Area.

Future development could cause additional loss of natural resources from development, and it is reasonable to predict that land values adjacent to improvements may increase.

Additional planned developments are reasonably expected to occur near the Study Area and would be considered a foreseeable action. Future planned development is reasonably expected to occur under either the Selected Alternative or No Build Alternative and will have corresponding development effects to the social, natural, and cultural environments within the Study Area.

Predominant cumulative effects from construction of the Selected Alternative include change in land use and growth in traffic through the Study Area.





## SECTION 5

# Coordination & Public Involvement

*In This Section.....* Participation in the decision making process included community leaders, federal and state agencies, Native American Tribes, and the public. Outreach milestones include:

- ◆ Solicitation of Views
- ◆ Public Outreach
- ◆ Community Leaders
- ◆ Native American Tribal Outreach
- ◆ Agency Coordination

### 5.1 Introduction

Community leaders, federal and state agencies, Native American Tribes, and the public were invited to participate in the decision making process for this project. The outreach program is intended to initiate and continue discussion with stakeholders throughout the study duration. Outreach efforts including meeting dates, times, and locations and summaries of events are discussed below.

### 5.2 Solicitation of Views

The SOV process is designed to inform interested agencies and persons of the proposed project and request early comments regarding potential adverse economic, social, or environmental effects or other related concerns. Federal, state, and local agencies were invited to participate in the SOV process. LADOTD invited Federal tribes to participate in the SOV process. An SOV packet, including a project overview and figure of the Study Area boundaries, was mailed to various federal, state, and local agencies and Native American tribes requesting their views. In addition to identifying any concerns or issues as mentioned above, consultation to address cultural and historic resource issues pursuant to Section 106 of the NHPA (36 CFR Part 800) was

also requested. The SOV packet and distribution list are included in Appendix C.

### 5.3 Public Outreach

#### 5.3.1 Public Meeting

LADOTD invited Federal tribes to the public officials and key stakeholders meeting at the Slidell City Auditorium on August 22, 2013, from 3:00 p.m. to 4:00 p.m. In addition, letters of invitation reminded the addressees that the public meeting would be held at the same location on the same day from 5:00 p.m. to 7 p.m.

The public was invited to participate in a public meeting on August 22, 2013, held at the Slidell City Auditorium, Slidell, Louisiana, from 5:00 p.m. to 7:00 p.m. The purpose of the public meeting was to present an overview of the project, present the preliminary alternatives and intersection configurations, and give the public an opportunity to provide comments that will help the study team in the evaluation of alternatives. In addition, the meeting was an opportunity for any interested parties to request participation in Section 106 of the NHPA consultation to address cultural and historical resource issues related to the proposed project.



## ENVIRONMENTAL ASSESSMENT

The public meeting notice was published in The Times-Picayune on August 9, and August 16, 2013.

The public meeting was attended by 47 persons registering their attendance on the sign-in sheets. Of these persons, 13 were public officials, 19 were members of the public, 10 were representatives from LADOTD and FHWA, and 8 were members of the project consultant team. Four verbal comments were recorded by the transcriber at the public meeting, and five written comments were received through the close of the comment period on September 2, 2013. Due to the Labor Day Holiday, comments were received through September 3, 2013.

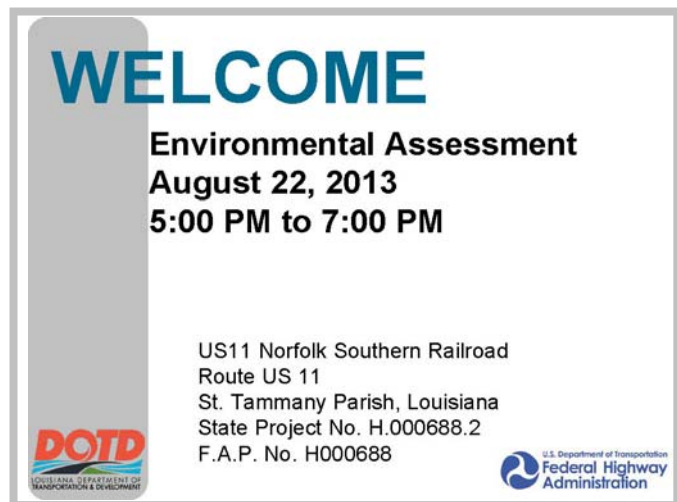
The meeting handout included the alternatives and a comment form. Commenters expressed support for the bridge replacement indicating that it is a needed improvement. Concern was expressed regarding alternate access to, safety of, increased traffic, and emergency response time to the Addis Boulevard neighborhood. Two options for access were presented. One commenter suggested providing both access options.

Additional concern that was expressed included closing Indiana Avenue and re-routing traffic traveling northbound on US 11 along 9th Street. Questions were asked regarding the location of median openings, traffic patterns, and impacts to business.

The construction schedule and duration of detours were issues of public concern, and two commenters asked if bike lanes would be included along the roadway.

A public meeting summary and transcript of verbal comments received was prepared for the August 22, 2013, public meeting. The summary includes a discussion of the public meeting events, attendance, comments, and outreach following the public meeting. A description of the meeting format, copies of handouts, meeting

sign-in sheets, and written comments received by the close of the comment period, September 2, 2013, are appended to the summary. The public meeting summary was distributed to federal and state agencies and local governments. The full record of this public meeting is available at LADOTD Headquarters in Baton Rouge.



Welcome Slide from the August 22, 2013, public meeting.

Following the August 22, 2013, Public Meeting, correspondence in the form of a signed petition was received from residents of the Addis Boulevard neighborhood (Soldier Town) and Javery Road regarding the Addis Boulevard access options. Residents were strongly opposed to the Javery Road connection citing increased traffic and safety concerns for families and children along Javery Road. The petitioners were in favor of the Strawberry Street Connection.

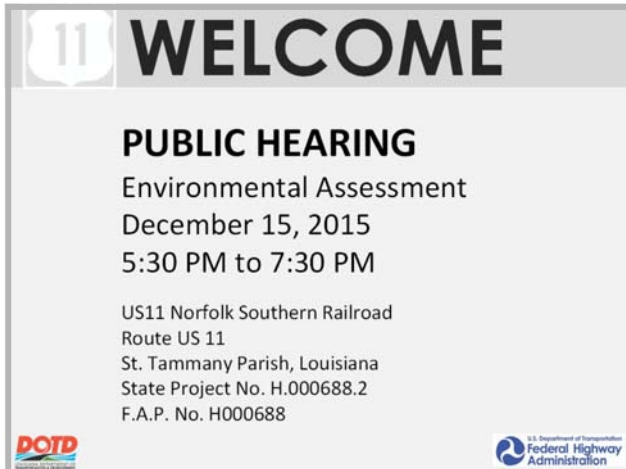
### 5.3.2 Public Hearing

LADOTD invited federal, state, and local officials and Federal tribes to an officials meeting at the Slidell Jr. High School on December 15, 2015, from 4:00 p.m. to 4:30 p.m. In addition, letters of invitation reminded the addressees that the public hearing would be held at the same location on the same day from 5:30 p.m. to 7:30 p.m.

The public was invited to participate in a public hearing on December 15, 2015, held at the Slidell Jr. High School, Slidell, Louisiana, from 5:30 p.m. to 7:30 p.m. The purpose of the public hearing was to present an overview of the project and provide an opportunity to obtain comments.

The public hearing notice was published in The St. Tammany Farmer on November 12, 2015, and December 3, 2015.

A total of 67 persons registered their attendance on the sign-in sheets. Of these persons, 33 were public or agency officials, 28 were members of the public, and 6 were members of the project consultant team. Five verbal comments were recorded by the transcriber at the public hearing and twenty written comments were received through the close of the comment period on December 26, 2015.



Welcome Slide from the December 15, 2015, public hearing.

The meeting handout included the Selected Alternative and a comment form.

Six commenters expressed their support of the project and three were not in favor. Commenters expressed an overall concern regarding elimination of left turn movements at US 11 and Gause Boulevard. Commenters that currently utilize North Boulevard for ingress/egress expressed concern regarding eliminating left turn

movements and were not in favor of the intersection configuration shown.

Commenters expressed concern regarding ingress/egress to Hall Avenue and were not in favor of the US 11/Hall Avenue intersection configuration shown. Commenters expressed concern regarding ingress/egress to the Soldier Town neighborhood via Strawberry Street. Five were in favor of the Strawberry Street access, two were opposed, and one was concerned. Commenters were expressly opposed to the Javery Road connection to Soldier Town.

Additional comments expressed concern regarding property impacts, primarily along the west side of US 11 north of the bridge crossing the NS railroad. Two individuals expressed concern over impacts to gas meter and mailbox locations on US 11 north of the bridge. One individual expressed concerns about construction-related noise impacts.

A public hearing summary and transcript of verbal comments received was prepared and includes a discussion of the public hearing events, attendance, comments, and outreach following the public hearing. A description of the meeting format, copies of handouts, meeting sign-in sheets, and written comments received are appended to the summary.

The public hearing summary was distributed to federal and state agencies and local governments. The full record of this public meeting is available at LADOTD Headquarters in Baton Rouge.

Following the December 15, 2015, officials meeting and public hearing, potential modifications to surface streets accessing the Addis Boulevard neighborhood were reviewed along with elimination or revision of proposed intersection configurations.

## ENVIRONMENTAL ASSESSMENT

The City of Slidell expressed concern over the proposed improvements making through traffic more efficient, which is inconsistent with objectives to slow traffic and re-connect neighborhoods with the Slidell business district. This was of particular concern from Gause Boulevard south to Hall Avenue.

The City also expressed concern over the Addis Boulevard neighborhood access including the proposed connection from Strawberry Street to Carnation Street that would allow access to Gause Boulevard to the south or Ben Thomas Road to the north. The addition of traffic at the Gause Boulevard-Carnation Street intersection and increase in congestion were of concern.

Additional comments received expressed concern regarding proposed intersection improvements and associated turning movements along the corridor.

Upon further review of comments received following the public hearing and subsequent coordination with the City of Slidell, a revision to Alternative 1R resulted in Alternative 1R Modified (Selected Alternative). This alternative includes reducing construction limits by beginning approximately 225 feet north of the intersection of US 11 with Lafayette Street and terminating at US 11 and Powell Drive. Improvements south of Lafayette Street are no longer included under the Selected Alternative. Additional improvements include dual right-turn lanes with a recommended signal at the I-12 eastbound exit ramp, a roundabout at Powell Drive, an Addis Boulevard realignment with right-in/right-out access, right-in/right-out access at North Boulevard, U-turns, and turn lanes. The previously proposed connections from Strawberry Street to Carnation Street and from Addis Boulevard to 4th Street to Javery Road have been eliminated.

Following these changes, a Public Information Meeting was held at Slidell Junior High School on April 10, 2018, to present Alternative 1R Modified to the public. The meeting was held from 5:00 p.m. to 7:00 p.m. and was open to all members of the public. An officials meeting was

also held on the same day at the same location from 4:00 p.m. to 5:00 p.m.

As documented in the June 4, 2018, Transcript of the Open-House, 55 persons attended the meeting: 37 were members of the public and 9 were public or agency officials. The remaining attendees were consultant/LADOTD project staff.

Two written comments were received at the public meeting and two comments were received via mail. One of the comments received via mail was a duplicate comment from someone who left a written comment at the public meeting. A total of four comments were received from three people.

One commenter expressed their support of the project, and two commenters were not in favor of the project. The commenter who supported the project stated that the US 11 bridge over the Norfolk Southern Railroad is unsafe and should be replaced. This commenter also stated that removing the left-turn movement at North Boulevard would be inconvenient, but he understood the rationale for doing so. The commenter also liked that the US 11 bridge would remain open during construction.

One commenter who was not in favor of the project was concerned the bridge would be constructed too close to her house. This commenter stated construction noise and vibrations would reduce the value of her house. This commenter requested her property be purchased by LADOTD for the project. The other commenter was not in favor of the project because she is unwilling to sell her property as required ROW for the Selected Alternative.



## SECTION 6

## References

- 
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## ENVIRONMENTAL ASSESSMENT

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# APPENDIX A

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Programmatic Section 4(f) Evaluation and Approval for  
FHWA Projects that Necessitate the Use of Historic Bridges

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PROGRAMMATIC SECTION 4(f) EVALUATION  
STATE AND FEDERAL AID PROJECT NO. H.000688  
US 11 NORFOLK SOUTHERN RAILROAD  
STR. NO. 625200180400531/RECALL NO. 059090  
ROUTE US 11  
ST. TAMMANY PARISH

APPLICABILITY

1. The bridge is to be replaced with Federal funds.
2. The project will require the use of a historic structure which is eligible for the National Register of Historic Places (NRHP). The bridge was determined eligible under Criterion A (Politics/Government and Transportation) for its association with important trends or events that have made a significant contribution to the broad patterns of Louisiana history.
3. The bridge is not a National Historic Landmark.
4. The Federal Highway Administration (FHWA) Division Administrator has determined that the facts of the project match those set forth in the sections of this document labeled Alternatives and Existing Conditions & Impacts.
5. Agreement between the FHWA, the Advisory Council on Historic Preservation (ACHP), Louisiana Department of Transportation and Development (LADOTD), and Louisiana State Historic Preservation Officer (SHPO) has been reached through procedures pursuant to Section 106 of the National Historic Preservation Act. A Programmatic Agreement (PA) for the treatment of Louisiana historic bridges was executed on September 21, 2015.

ALTERNATIVES

1. Do nothing.
2. Build Alternative 1, Bridge 1 crosses the railroad at a 74-degree angle requiring a 390-foot main span length with bridge approaches partially located on embankment. This bridge alternative will allow for maintenance of traffic during construction. By utilizing phased construction, the traffic can utilize a portion of the existing bridge while the new bridge is being built.
3. Build Alternative 1, Bridge 2 crosses the railroad at a 65-degree angle requiring a 250-foot main span length with bridge approaches partially located on embankment. This bridge alternative will not allow for maintenance of traffic during construction. Complete removal of the existing bridge will be required prior to any construction.

FINDINGS

1. Do nothing. NEPA requires that doing nothing to address the project need be considered during the environmental review process. This alternative was designated as the No Build Alternative. Although this alternative does not meet the US 11 Norfolk Southern Railroad purpose and need for the project because it would not address bridge deficiencies related to its age and design, it is considered in the EA as a baseline for comparison.
2. Remove the bridge and build on existing alignment. This alternative would require the roadway to be closed for an extended period of time. The volume of traffic on this roadway would make a lengthy road closure unreasonable.



2 | Page

3. Build a new structure on an alternate alignment. This alternative would require adjustments to US 11 and the current bridge to accommodate the new alignment. Phasing construction would allow for current traffic to utilize the existing structure while the new structure was being constructed. This alternative is preferred in the EA document.
4. Rehabilitation without Affecting the Historic Integrity of the Bridge. This alternative would include addressing the current width, height, and weight limits of the existing bridge. To address the deficiencies would require significant replacement of key members of the bridge.
  - a. The bridge is so structurally deficient that it cannot be rehabilitated to meet minimum acceptable load requirements, because the bridge was designed for a smaller live load than required by current specifications.
  - b. The bridge is geometrically obsolete and cannot be widened to meet minimum required capacity. The bridge consists of two 12-foot travel lanes and no shoulders. The clear roadway width is 24 feet.

MEASURES TO MINIMIZE HARM.

The executed PA specifies measures intended to identify, avoid, minimize, and/or mitigate effects on historic bridges. The PA lists historic bridges by their treatment category of preservation priority, preservation candidate, or non-priority. The US 11 NO & NE Railroad Bridge (as listed in the PA) is listed as a non-priority bridge (see PA Attachment 1 - page 17).

Stipulation IX of the PA (Stewardship, Public Outreach, Education, and Funding) outlines efforts to be carried out as mitigation for adverse effects to historic bridges contemplated under the PA, including potential replacement of non-priority bridges.

These efforts include; maintaining an historic bridge website and list of interested parties with a potential use for a relocated historic bridge, a publication highlighting descriptive and historical information for each historic bridge, historic bridge training workshops, representative HAER documentation of bridge types, dedicated funding for preservation, preventative maintenance and rehabilitation of historic bridges, and marketing of historic bridges.

In addition, Stipulation VII D of the PA (Treatment of Louisiana Historic Bridges) outlines the steps to be taken should a non-priority bridge be proposed for replacement. These steps include sending out a Solicitation of Views letter for the proposed replacement with 45-day response period and marketing the bridge for a 90-day period. A Solicitation of Views letter was sent out on May 3, 2013. No objections were received. To encourage relocation and

PROGRAMMATIC SECTION 4(f) EVALUATION  
STATE AND FEDERAL AID PROJECT NO. H.000688  
US 11 NORFOLK SOUTHERN RAILROAD  
STR. NO. 625200180400531/RECALL NO. 059090  
ROUTE US 11  
ST. TAMMANY PARISH

3 | Page

adaptive reuse the bridge will be marketed per the steps outlined in Attachment 6 of the PA.  
US 11 Norfolk Southern Bridge marketing will occur after the EA document has been  
approved by FHWA.

5/24/2024

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Date

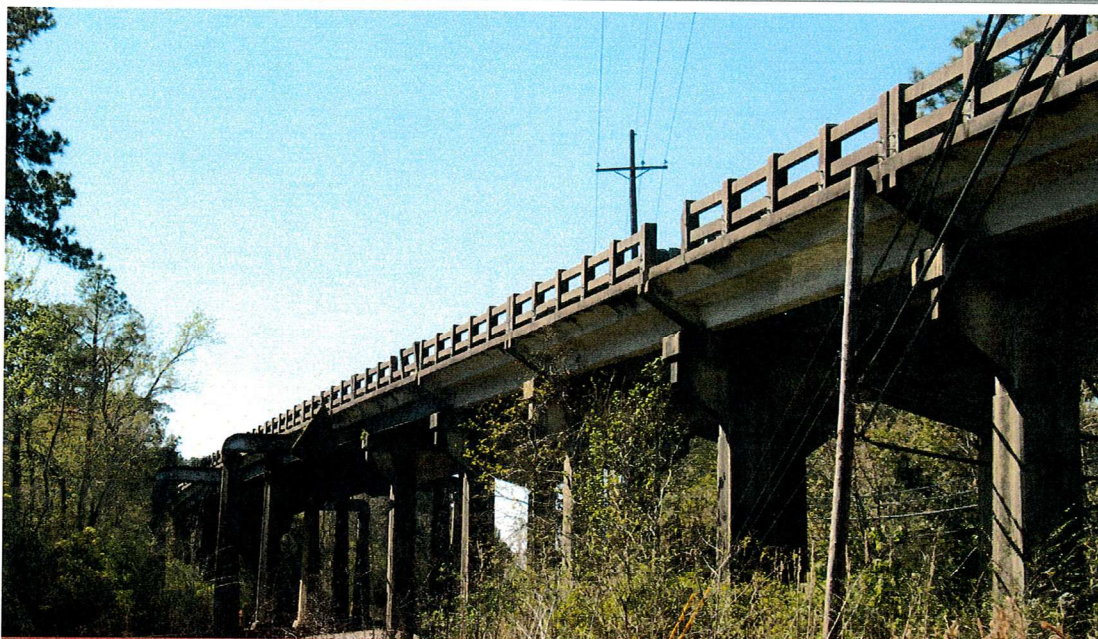
RAEGAN M  
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Date: 2024.05.24  
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For Melinda Roberson  
Division Administrator





### NO & NE Railroad Bridge

Recall Number .....	059090	Latitude .....	30.293333
Parish .....	St. Tammany	Longitude .....	-89.776417
City .....	Slidell	Year built .....	1937
Feature carried .....	US 11	Owner .....	State of Louisiana
Feature crossed .....	NO & NE Railroad	National Register status .....	Eligible
Type .....	Steel I- beam		

### SIGNIFICANCE

This steel I-beam bridge has significance for its association with important trends or events that have made a significant contribution to the broad patterns of Louisiana history. The significance of this grade separation structure is directly related to its funding through the U.S. Works Program Grade Crossing program during the Depression-era to provide safe railroad-highway crossings. In the 1930s, increased attention was given to creating grade separations between railway lines and roads and specific legislation was passed to provide funds for highway/rail grade separations through the National Industrial Recovery Act (1933), Hayden Cartwright Act (1934), and

Emergency Relief Appropriation Act (1935). The U.S. Works Program Grade Crossing program resulted from the Emergency Relief Appropriation Act funding. This bridge's association with a federal Depression-era program is documented in the Louisiana Highway Commission Biennial Reports. The bridge exhibits alterations to the railing with the addition of guardrail that results in a minor loss of integrity, but continues to convey its significance as the work of the Emergency Relief Appropriation Act. This bridge is eligible for the National Register under Criterion A: Politics/Government and Transportation.

## Attachment 1 - Historic Bridges and Treatment Category

Bridge Type: Steel beam and girder

Bridge Configuration	Preservation Category	Recall Number	Parish	Bridge Name	Facility Carried and Feature Crossed	Year Built	Owner
Steel beam and girder	Preservation Priority	008120	St. Landry	COURTABLEAU BAYOU	LA0103 over BAYOU COURTABLEAU	1937	State of Louisiana
Steel beam and girder	Preservation Priority	014400	Caddo	ILLINOIS CENTRAL R/R	US0071 over ICG RR	1937	State of Louisiana
Steel beam and girder	Preservation Priority	610023	East Baton Rouge	PERKINS RD. OVERPASS	CITY STREET over K.C.S. RR	1937	Parish Highway Agency
Steel beam and girder	Preservation Candidate	019040	Webster	L. & A. RAILROAD (MINDEN)	US0371 over KCS RR MINDEN	1935	State of Louisiana
Steel beam and girder	Preservation Candidate	023620	Morehouse	MISSOURI PACIFIC RAIL/RD	US0165 over MISSOURI PACIFIC RAILROAD	1938	State of Louisiana
Steel beam and girder	Preservation Candidate	059730	St. Tammany	G. M. & O. RAILROAD	LA0036 over ICG RAILROAD	1937	State of Louisiana
Steel beam and girder	Non-Priority	014410	Caddo	ILLINOIS CENTRAL R/R	US0071 over ICG RR	1940	State of Louisiana
Steel beam and girder	Non-Priority	014420	Caddo	ILLINOIS CENTRAL R/R	US0071 over ICG RR	1940	State of Louisiana
Steel beam and girder	Non-Priority	055130	West Baton Rouge	T & P RAILROAD OVER PASS	US0190 over LA 415/M P RR @ LOBDELL	1939	State of Louisiana
Steel beam and girder	Non-Priority	059090	St. Tammany	N. O. & N. E. RAILROAD	US0011 over NO&NE RAILROAD	1937	State of Louisiana





Mr. Greg Solvey  
Attn: Myra G. Diaz,  
Natural Hazards Program Specialist  
FEMA Region VI  
800 North Loop 288  
Denton, TX 76201

Subject:  
Solicitation of Views and  
Initiation of Section 106 Consultation  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana  
State Project No. H.000688.2  
F.A.P No. H000688

Dear Mr. Solvey:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns and reach agreeable decisions while taking into account the interests of all parties.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A Stage 0 Feasibility Study for this project was completed in 2008. A project summary and figures are attached for your review. If you would like to review the Stage 0 Study in its entirety, please request a copy from Beth Beam by e-mail at [elizabeth.beam@arcadis-us.com](mailto:elizabeth.beam@arcadis-us.com) or by U.S. mail to Ms. Beth Beam, ARCADIS, 10352 Plaza Americana Drive, Baton Rouge, Louisiana 70816. A project overview, project study area map, and figures are attached for your review.

We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

Imagine the result

ARCADIS U.S., Inc.  
10352 Plaza Americana Drive  
Baton Rouge  
Louisiana 70816  
Tel 225 292 1004  
Fax 225 218 9677  
[www.arcadis-us.com](http://www.arcadis-us.com)

INFRASTRUCTURE

Date:  
3 May 2013

Contact:  
Beth Beam

Extension:  
215

Email:  
[elizabeth.beam@arcadis-us.com](mailto:elizabeth.beam@arcadis-us.com)

Our ref:  
LA003143.0000  
LDOTD/3143/C/1/kp



Mr. Greg Solvey  
3 May 2013

**Closing**

On behalf of LADOTD, ARCADIS U.S., Inc. (ARCADIS) requests that you review the attached information and furnish us with your views and comments by **Monday, June 3, 2013**. Replies should be sent to Beth Beam by e-mail or by U.S. mail at the addresses provided. Please reference State Project No. H.000688.2 in your reply.

Sincerely,

ARCADIS U.S., Inc.

A handwritten signature in black ink that reads 'Elizabeth Beam'.

Elizabeth Beam, AICP  
Senior Planner/Scientist

Attachments

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# APPENDIX B

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Design Criteria, Typical Sections, and Plan & Profiles

B-1 Design Report and Structural Design Criteria

B-2 Typical Roadway and Bridge Sections

B-3 Plan & Profiles, Alternative 1R Modified



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# APPENDIX B-1

## Design Report and Structural Design Criteria

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Design Report  
for  
2017 Minimum Design Guidelines

Status:  
Preliminary  
Final  
Revised

Project Information:

State Project No.	
Federal Aid Project No.	
Control Section(s)	
Project Name	
Route(s)	
Parish	

Description of Work (or Revision Description)

--

Traffic

Control Section		
Current ADT		
Design ADT		
D		
K		
T		
TDDHV		

Design Waivers

--

Design Exceptions

--

Route and Design Classification

Urban	Rural				
Freeway	Arterial	Collector	Local	Ramp	

Work Classification

Work Type	System	Oversight
New/Reconstruction	NHS	PoDI
Major Rehabilitation	Non NHS	Assumed
Structural Improvement		None
Spot Replacement		
Minor Rehabilitation		
Preventive Maintenance		

Recommended By:

Engineer of Record:	Title:
Signature:	Date:
DOTD Technical Task Manager (Road):	Title:
Signature:	Date:
DOTD Technical Task Manager (Bridge):	Title:
Signature:	Date:
DOTD Project Manager:	Title:
Signature:	Date:
Approved By:	
Print Name:	Title:
Signature:	Date:

Design Guideline Waivers are approved as noted in the attached instructions. All Design Exceptions must be approved by the Chief Engineer with the Design Exception form.



State Project No.

Route

Control Section

Roadway Features:

Design Feature	Preferred	Acceptable	Proposed Value	Design Waiver Required	Design Exception Required	Remarks or Explanation for Proposed Value	
Design Speed (mph)							
Lane Width (ft)							
Shoulder Width (ft)							
Inside							
Outside							
Shoulder Type							
Inside							
Outside							
Lateral Offset (ft)							
Clear Zone (ft)							
Cross Slope (%)							
Longitudinal Grade							
Slopes (ft/ft)							
Fore Slope							
Back Slope							
Median Width (ft)							
Stopping Sight Distance: Vertical and horizontal distances must be met.							
Do plans meet Stopping Sight Distance requirements?						Design Exception Required	
Yes						No	
Complete Streets: Accommodations for bikes and pedestrians must be considered. See Design Guidelines for accommodation requirements.							
Do plans meet Complete Streets accommodations?						Design Exception Required	
Yes						No	
Horizontal Curves Radius/Superelevation:							
Max Super-elevation rate (%) $e_{max}$	Design Speed (mph)	Required Minimum Radius (ft)			Minimum radius and appropriate superelevation are being used for all curves?	Design Exception Required	Remarks or Explanation for Proposed Value
		Normal Crown	Reverse Crown	Full Super			
					Yes	No	
					Yes	No	

2 of 3

State Project No. Control Section Route

Bridge Features:						
Design Feature	Preferred	Acceptable	Proposed Value	Design Waiver Required	Design Exception Required	Remarks or Explanation for Proposed Value
Bridge Width (ft)						
Curb						
Shoulder						
Structural Capacity:						
Do all structures meet requirements for Structural Capacity?					Design Exception Required	Remarks or Explanation for Proposed Value
Yes		No				
Vertical Clearance:						
Are minimum required roadway clearances met for all structure types?					Design Exception Required	Remarks or Explanation for Proposed Value
Yes		No				
Additional Comments:						

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## Structural Design Criteria \*

### US 11 Norfolk Southern Railroad EA (H.000688)

Reference Publications	
State of Louisiana Department of Transportation and Development (LADOTD) Bridge Design Manual	4th Edition, 2005
State of Louisiana Department of Transportation and Development (LADOTD) LRFD Bridge Design Manual	1 <sup>st</sup> Edition, September 17, 2008
AASHTO Standard Specifications for Highway Bridges	17th Edition
AASHTO LRFD Bridge Design Specifications	6th Edition, 2012
American Railway Engineering and Maintenance-of-Way Association, Manual for Railway Engineering	2010
Norfolk Southern – Guidelines for Design and Construction of Privately Owned Industry Tracks	2011
American Society of Civil Engineers, Minimum Design Loads for Buildings and Other Structures, ASCE 7	2010
American Institute of Steel Construction (AISC), Manual of Steel Construction, Load and Resistance Factor Design (LRFD)	13 <sup>th</sup> Edition
American Concrete Institute (ACI), Building Code Requirements for Reinforced Concrete, ACI 318	2011
American Welding Society, Structural Welding Code, Steel, AWS D1.1/D1.1M	2010
Bridge Welding Code, AWS D1.5/D1.5M	2010
Bridge Type & Specifications	
Span Length	Roughly 250 feet. Length may change later.
Approach Spans Girder Types	AASHTO Types II, III, IV & Bulb-T based on the span lengths.
Main Span Girder Options	Steel plate girder, concrete spliced girders, or bulb-T girders (depends on right-of-way accessibility within the existing and proposed Norfolk Southern tracks).
Bridge Deck	Cast-in-situ concrete slab.
Barrier Type	Concrete F Shape PL-2.
Approach Span Substructure	Concrete bents founded on piles.
Main Span Substructure	Concrete piers with spread footing founded on pile clusters/shafts.
Bridge Design Loads	
Governing load based on Louisiana Design Vehicle Live Load 2011 (LADV-11), Lane Load	
Design vehicular speed – 45 mph (UA-2 functional classification)*	

\* The US 11 bridge over the NS railroad was planned with design criteria, which was established at the time the EA was first produced (2015). The UA-2 functional classification is referred to as UA in current design criteria.



## Structural Design Criteria\*

### US 11 Norfolk Southern Railroad EA (H.000688)

Reference Publications	
State of Louisiana Department of Transportation and Development (LADOTD) Bridge Design Manual	4th Edition, 2005
State of Louisiana Department of Transportation and Development (LADOTD) LRFD Bridge Design Manual	1 <sup>st</sup> Edition, September 17, 2008
AASHTO Standard Specifications for Highway Bridges	17th Edition
AASHTO LRFD Bridge Design Specifications	6th Edition, 2012
American Railway Engineering and Maintenance-of-Way Association, Manual for Railway Engineering	2010
Norfolk Southern – Guidelines for Design and Construction of Privately Owned Industry Tracks	2011
American Society of Civil Engineers, Minimum Design Loads for Buildings and Other Structures, ASCE 7	2010
American Institute of Steel Construction (AISC), Manual of Steel Construction, Load and Resistance Factor Design (LRFD)	13 <sup>th</sup> Edition
American Concrete Institute (ACI), Building Code Requirements for Reinforced Concrete, ACI 318	2011
American Welding Society, Structural Welding Code, Steel, AWS D1.1/D1.1M	2010
Bridge Welding Code, AWS D1.5/D1.5M	2010
Bridge Type & Specifications	
Span Length	Roughly 250 feet. Length may change later.
Approach Spans Girder Types	AASHTO Types II, III, IV & Bulb-T based on the span lengths.
Main Span Girder Options	Steel plate girder, concrete spliced girders, or bulb-T girders (depends on right-of-way accessibility within the existing and proposed Norfolk Southern tracks).
Bridge Deck	Cast-in-situ concrete slab.
Barrier Type	Concrete F Shape PL-2.
Approach Span Substructure	Concrete bents founded on piles.
Main Span Substructure	Concrete piers with spread footing founded on pile clusters/shafts.
Bridge Design Loads	
Governing load based on Louisiana Design Vehicle Live Load 2011 (LADV-11), Lane Load	
Design vehicular speed – 45 mph (UA-2 functional classification)*	

\* The US 11 bridge over the NS railroad was planned with design criteria, which was established at the time the EA was first produced (2015). The UA-2 functional classification is referred to as UA in current design criteria.



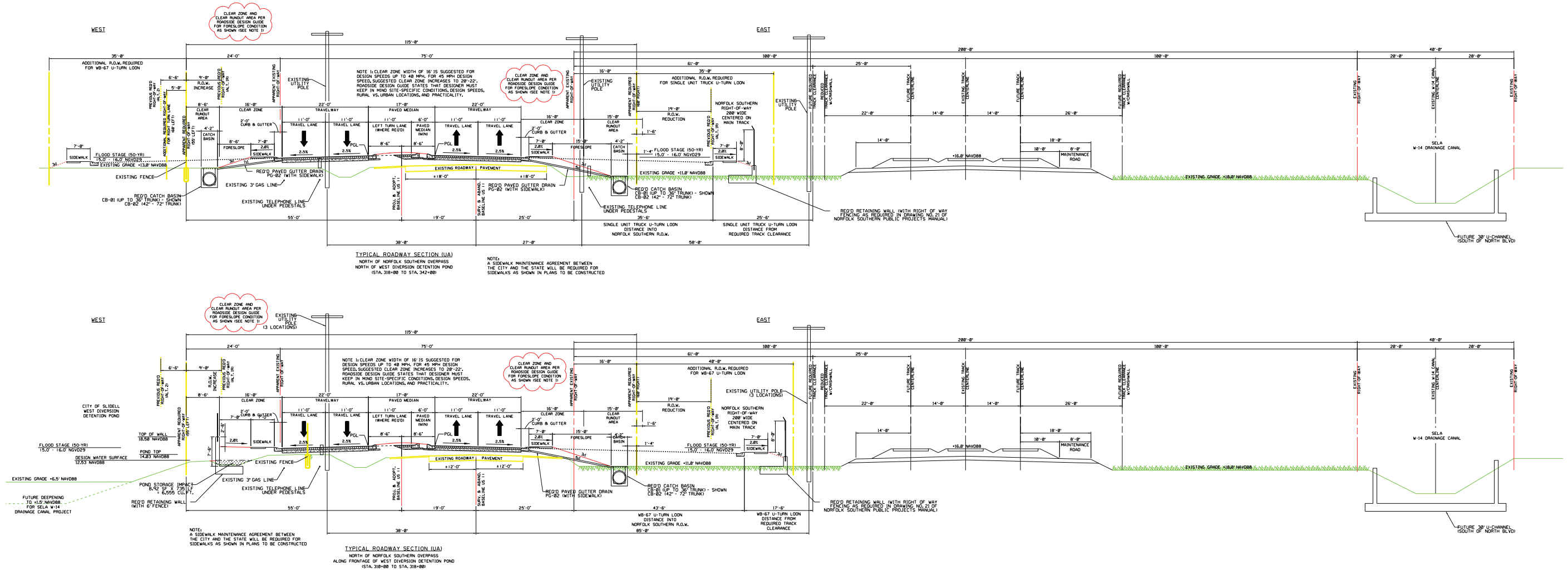
# APPENDIX B-2

## Typical Roadway and Bridge Sections


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**PRELIMINARY AND SUBJECT TO CHANGE**

<i>Conceptual Line and Grade</i>	
<i>Preliminary</i>	
<i>FOR REVIEW ONLY</i>	<i>ENGINEER: Toby D. Picard</i> <i>LICENSE #: LA29911</i> <i>DATE: 6/5/2019</i>

[illegible]

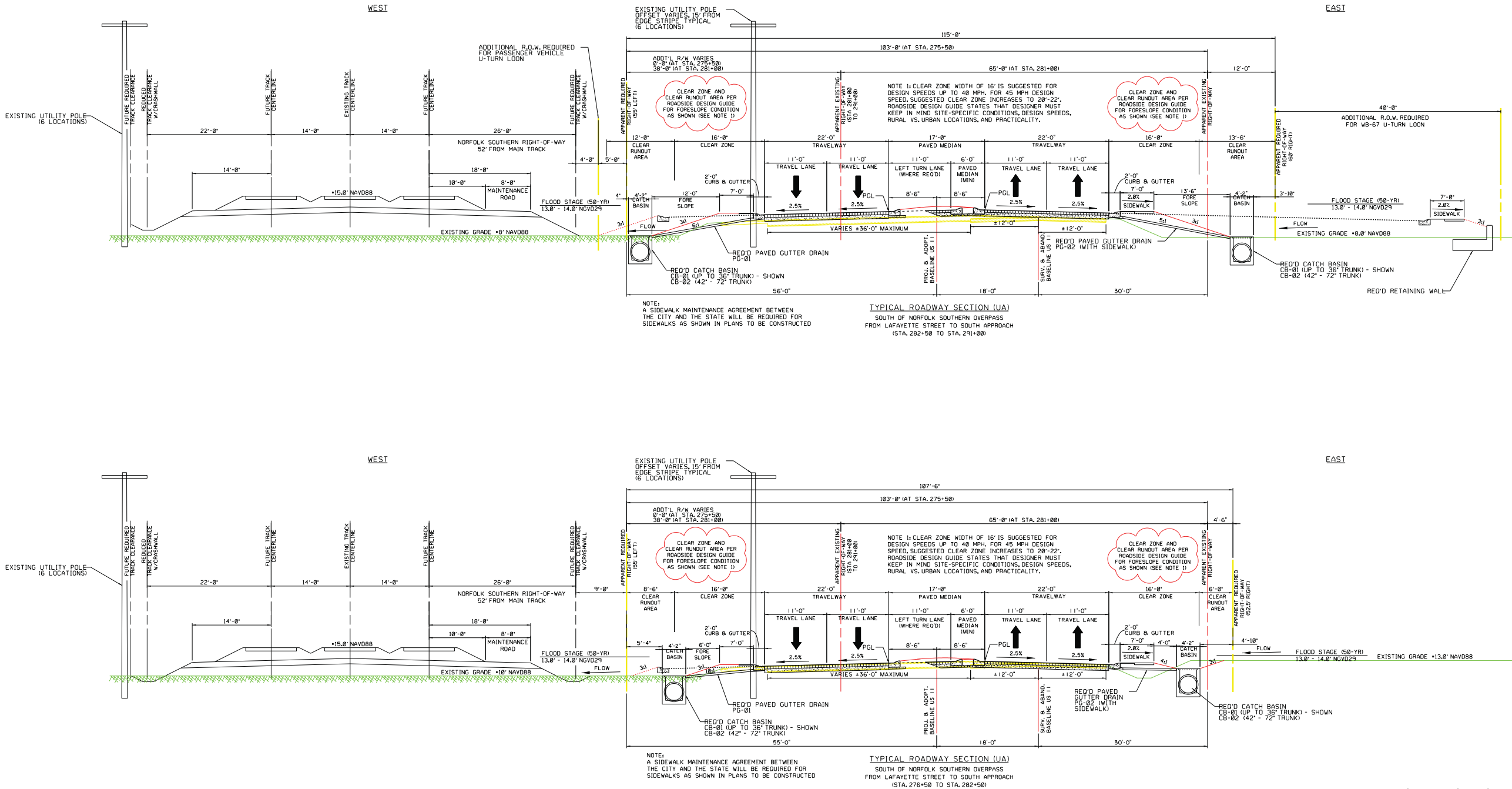
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PRELIMINARY AND SUBJECT TO CHANGE



Conceptual Line and Grade

Preliminary

FOR REVIEW ONLY

ARCADIS

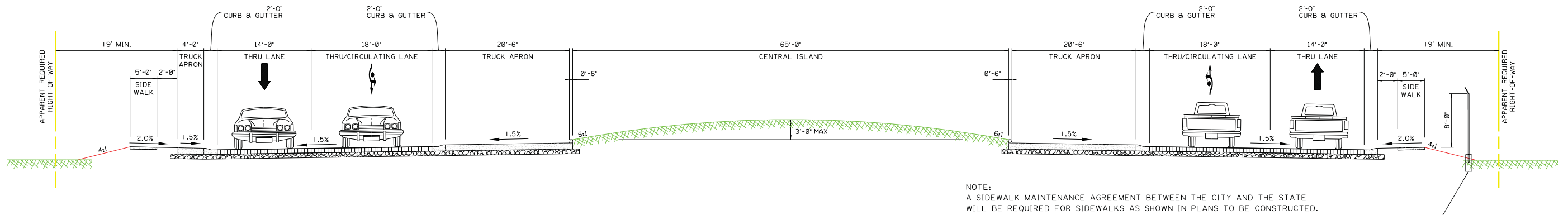
ENGINEER: Toby D. Picard  
LICENSE #: LA29911  
DATE: 6/5/2019

SHEET NUMBER	20
DESIGNED	ST. TAMMANY
CHECKED	
DETAILS	018-04
CHECKED	
SERIES	H.000688.2
NUMBER	
BY	
NO.	
DATE	
REVISION OR CHANGE ORDER DESCRIPTION	
TYPICAL SECTIONS	
US 11 NORFOLK SOUTHERN RAILROAD BRIDGE	

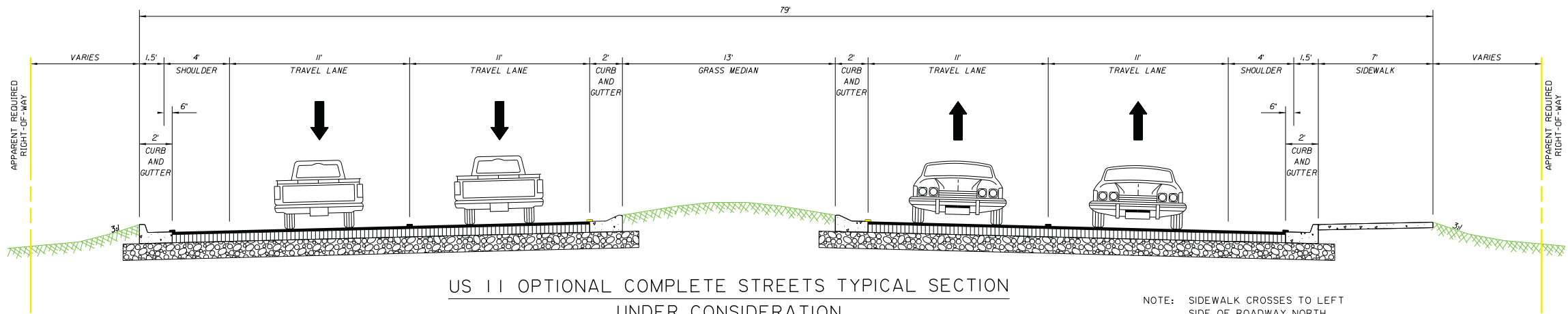
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TYPICAL ROUNDABOUT SECTION  
US 11 AT POWELL INTERSECTION



US 11 OPTIONAL COMPLETE STREETS TYPICAL SECTION  
UNDER CONSIDERATION  
(SUBJECT TO AGREEMENT & LOCAL SUPPORT)

NOTE: SIDEWALK CROSSES TO LEFT  
SIDE OF ROADWAY NORTH  
OF NORTH BLVD.

PRELIMINARY AND SUBJECT TO CHANGE

Conceptual Line and Grade

Preliminary

FOR REVIEW ONLY


ARCADIS

ENGINEER: Toby D. Picard  
LICENSE #: LA29911  
DATE: 6/5/2019

SHEET NUMBER	2b
ST. TAMMANY	PARISH
018-04	CONTROL SECTION
H.000688.2	STATE PROJECT
DESIGNED	CHECKED
DETAILED	CHECKED
SERIES NUMBER	BY
REVISION OR CHANGE ORDER DESCRIPTION	NO.
DATE	DATE
TYPICAL SECTIONS	US 11 NORFOLK SOUTHERN RAILROAD BRIDGE
ARCADIS	

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\$TIME\$

<i>Conceptual Line and Grade</i>	
<i>Preliminary</i>	 <b>ARCADIS</b>
<i>FOR REVIEW ONLY</i>	<i>ENGINEER: Toby D. Picard</i> <i>LICENSE #: LA29911</i> <i>DATE: 6/5/2019</i>

[illegible]

DESIGNED CHECKED	PARISH	ST. TAMMANY	SHEET NUMBER	2c
DETAILED CHECKED	CONTROL SECTION	018-04		
SERIES	STATE	H.000688.2		

PARISH	ST. TAMMANY
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CONTROL SECTION	018-04
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STATE PROJECT	H.000688.2
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# APPENDIX B-3

## Plan & Profiles Alternative 1R Modified

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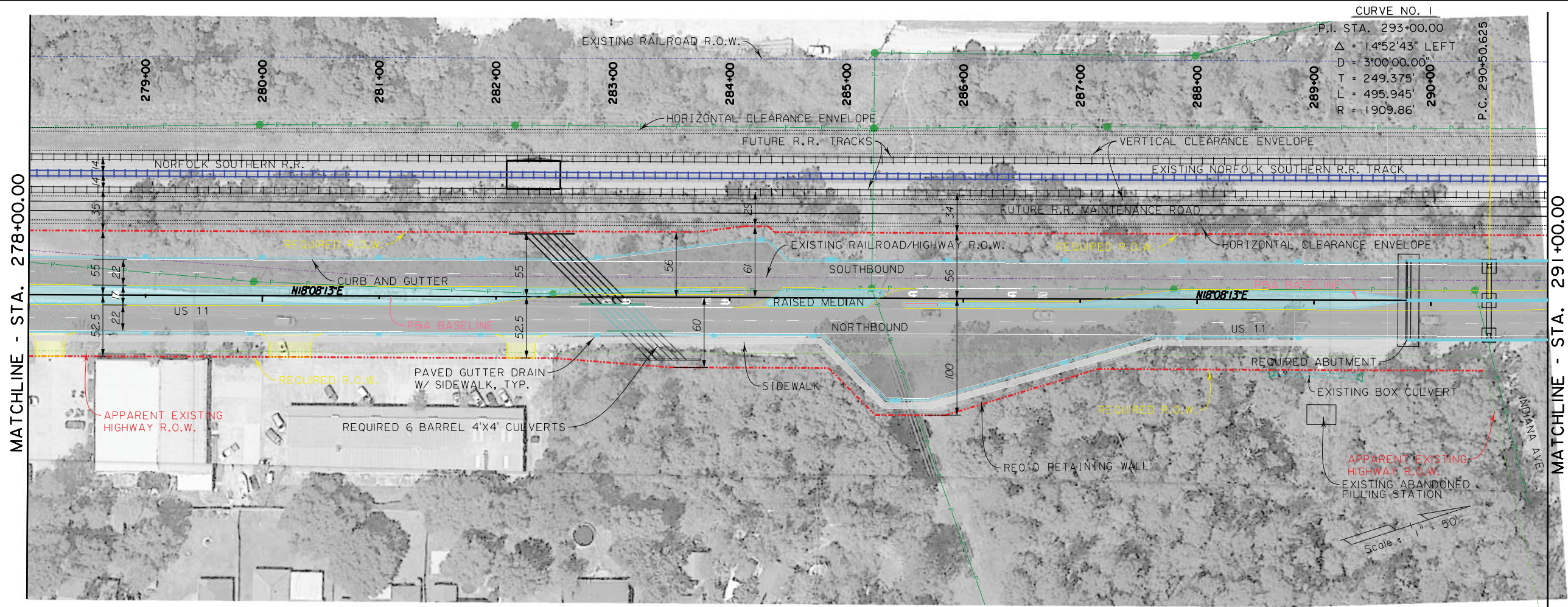
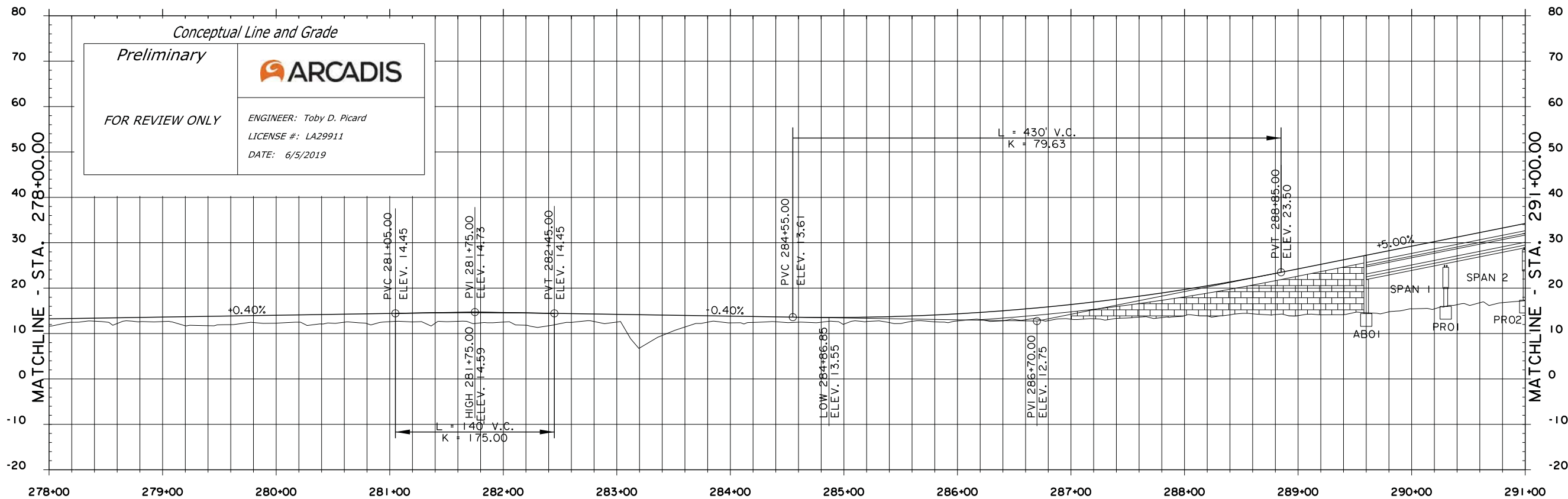
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PRELIMINARY AND SUBJECT TO CHANGE



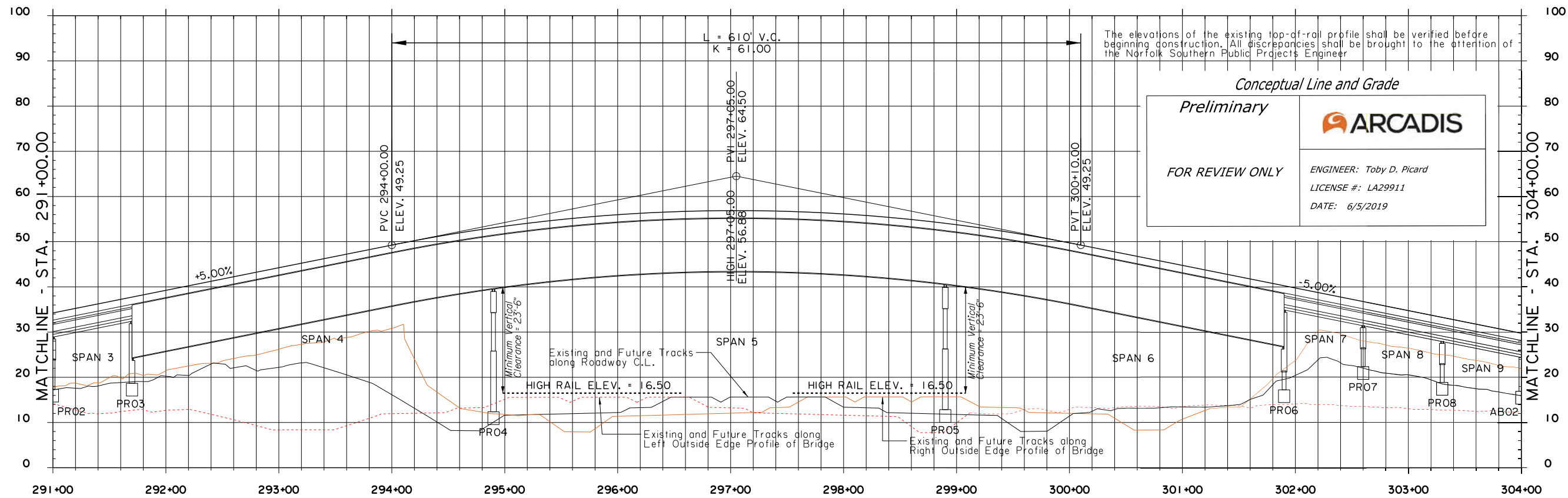
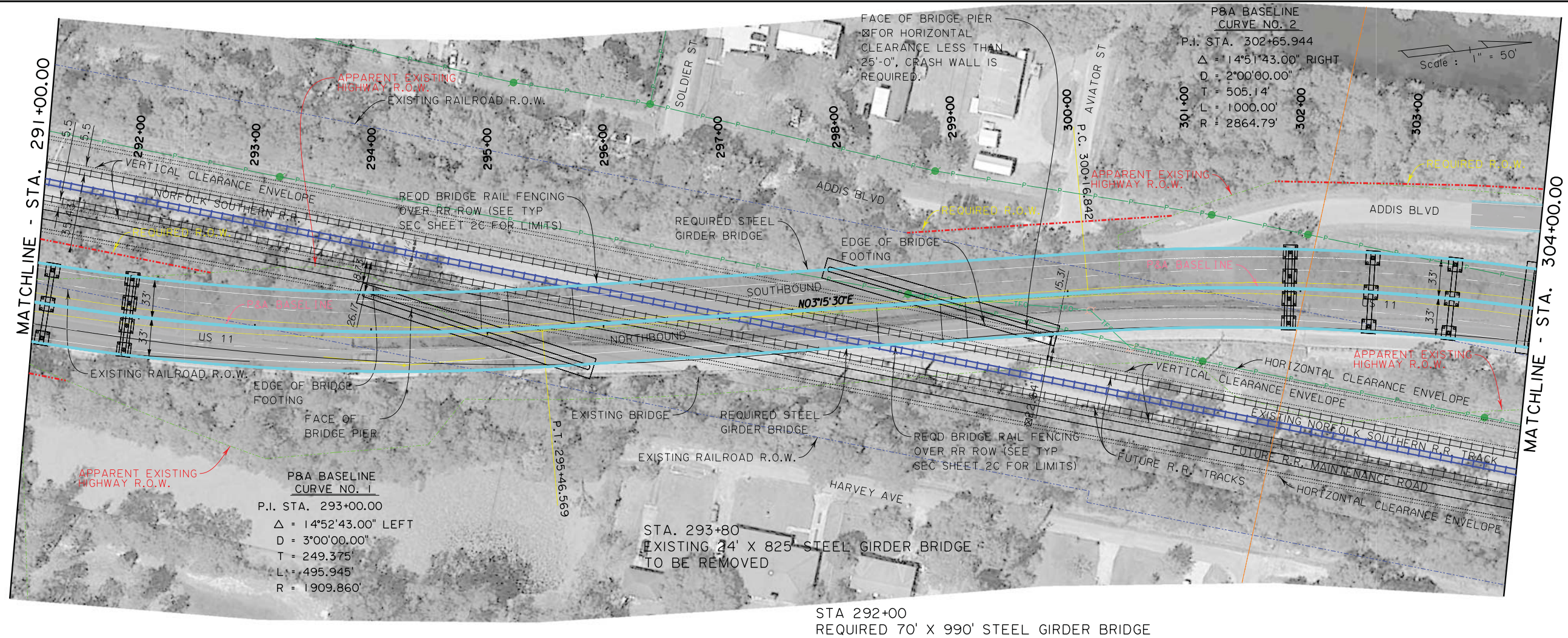
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	REVISION OR CHANGE ORDER DESCRIPTION		BY									
51												
PLAN AND PROFILE ALTERNATIVE NO. 1R MODIFIED STA. 278+00 TO STA. 291+00 US 11 NORFOLK SOUTHERN RAILROAD BRIDGE												
DOTD ARCADIS												

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PRELIMINARY AND SUBJECT TO CHANGE

 		<p>PLAN AND PROFILE</p> <p>ALTERNATIVE NO. 1R MODIFIED</p> <p>STA. 291+00 TO STA. 304+00</p> <p>US 11 NORFOLK SOUTHERN RAILROAD BRIDGE</p>										<p>DESIGNED FULKS/ PICARD</p> <p>CHECKED L. PORTA</p> <p>DETAILER J. HOWARD</p> <p>CHECKED D. FULKS</p> <p>SERIES NUMBER</p> <p>BY</p> <p>REVISION OR CHANGE ORDER DESCRIPTION</p> <p>NO. DATE</p>		<p>SHEET NUMBER</p> <p>67</p>	
												ST. TAMMANY			
												CONTROL SECTION		018-04	
												STATE PROJECT		H.000688.2	

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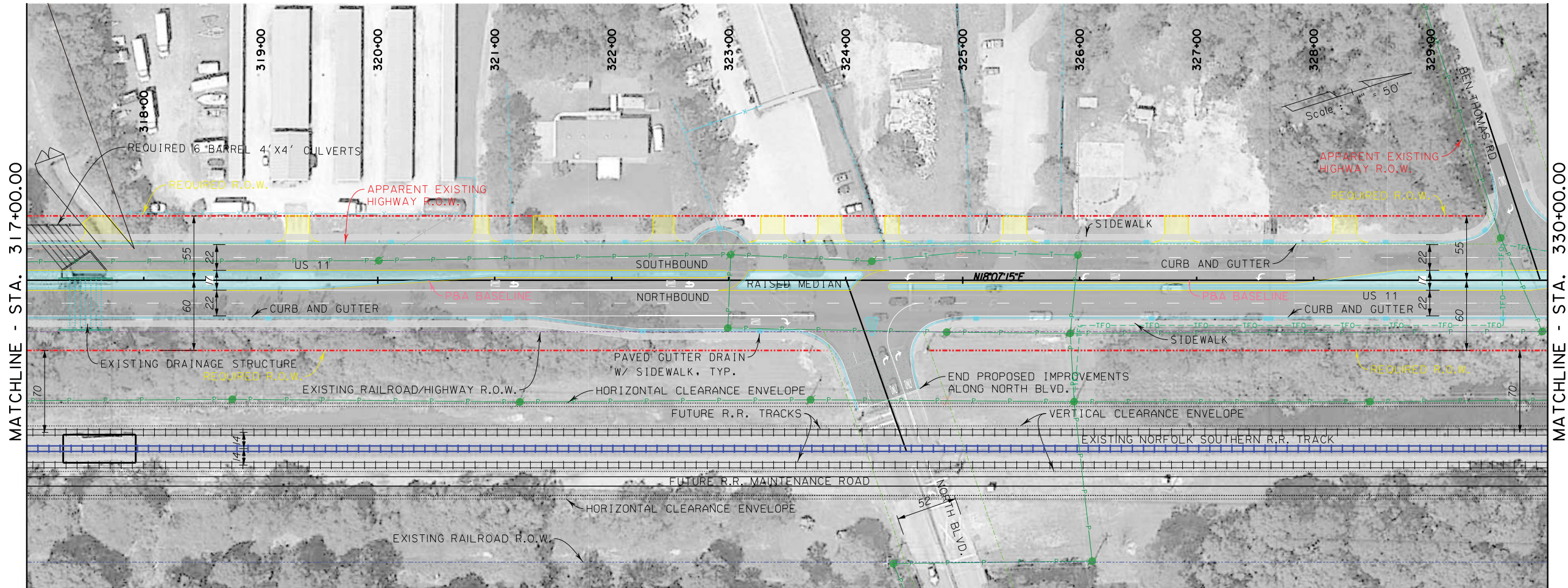
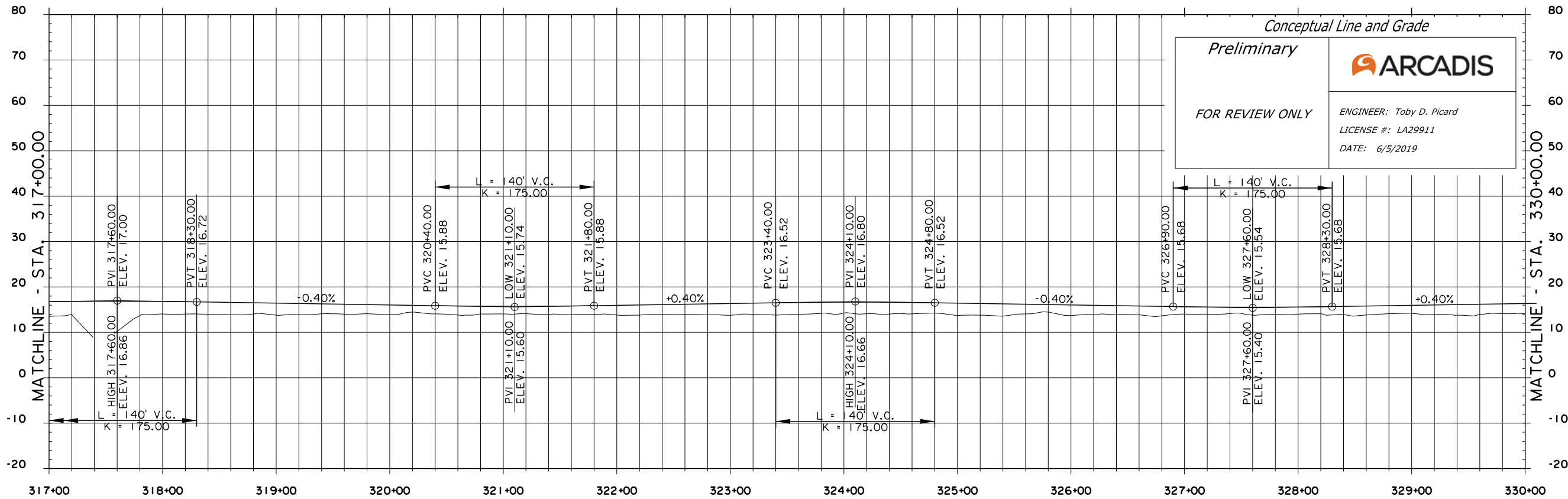







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				SHEET NUMBER	
				PARISH	
				CONTROL SECTION	
				STATE PROJECT	
				H.000688.2	
				SERIES NUMBER	
				BY	
				REVISION OR CHANGE ORDER DESCRIPTION	
				NO.	
				DATE	
				NO.	
				DATE	

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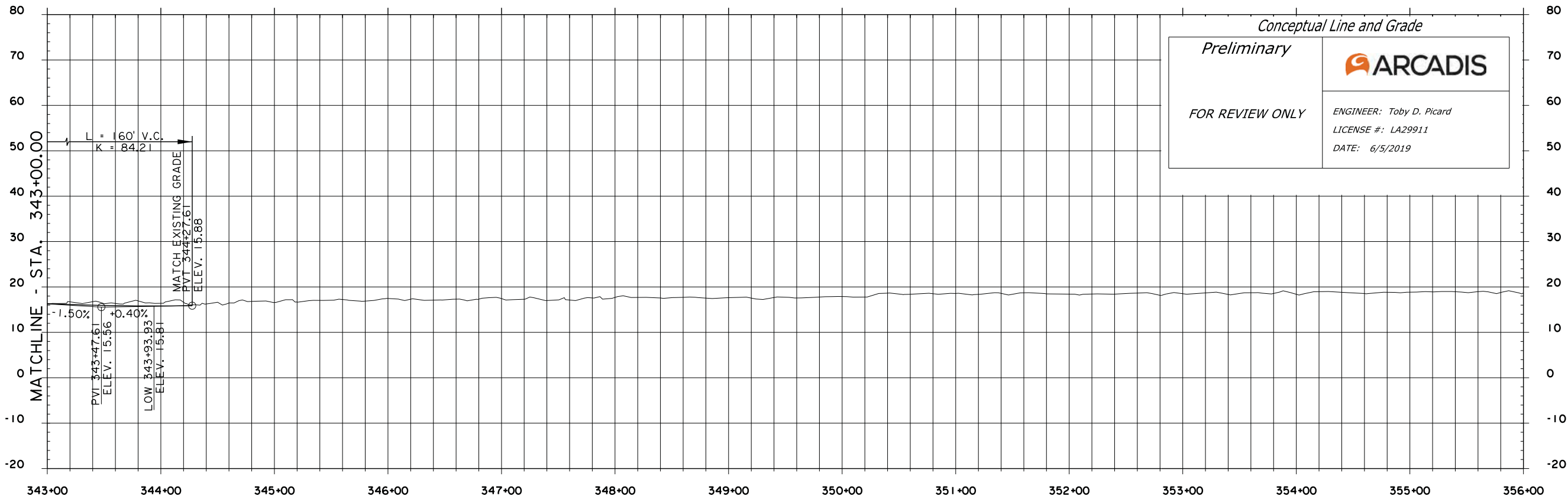
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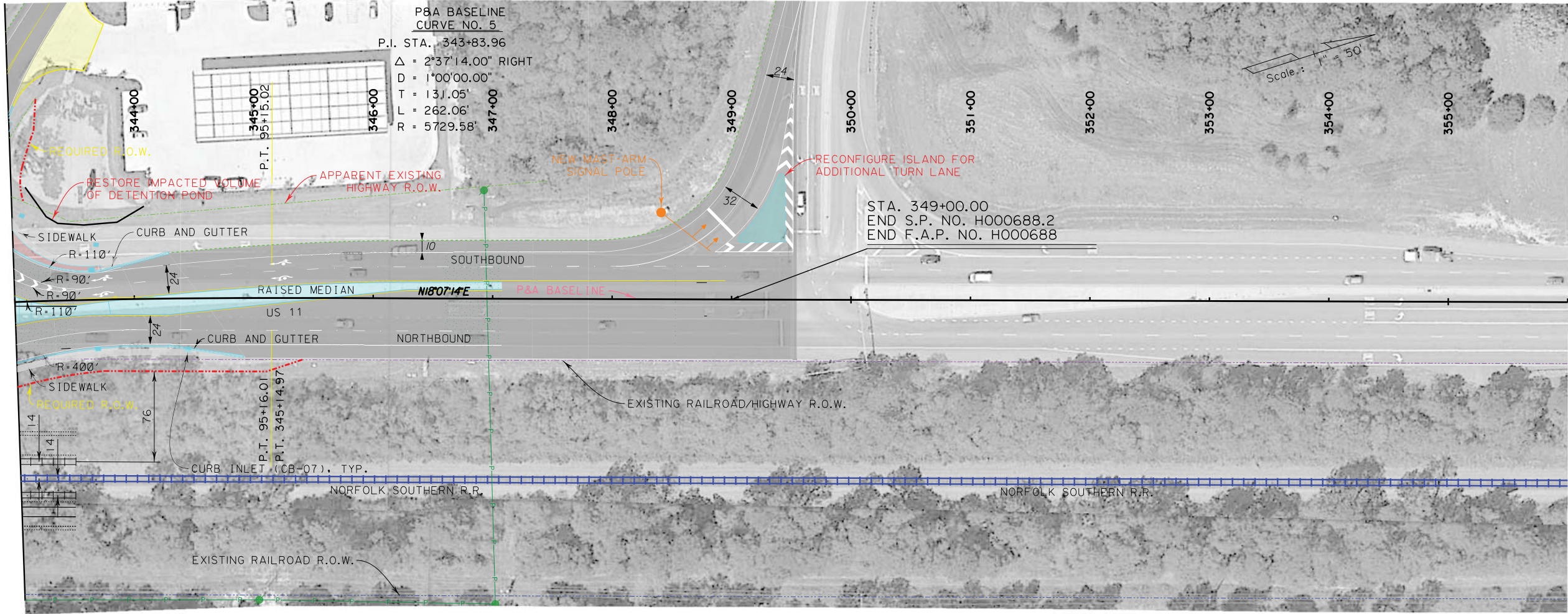
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PRELIMINARY AND SUBJECT TO CHANGE



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						DESIGNED CHECKED	FULKS/ PICARD L. PORTA	PARISH	ST. TAMMANY
						DETAILED CHECKED	J. HOWARD D. FULKS	CONTROL SECTION	018-04
						SERIES NUMBER		STATE PROJECT	H.000688.2
NO.	DATE					REVISION OR CHANGE ORDER DESCRIPTION		BY	

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# APPENDIX C

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## Agency Coordination

C-1 Solicitation of Views and Responses

C-2 Solicitation of Views - Tribes

C-3 Section 106

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# APPENDIX C-1

## Solicitation of Views and Responses

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«Courtesy» «First» «M» «Last\_Name»  
«Title»  
«Org\_1»  
«Org\_2»  
«Address»  
«City», «State» «Zip»

Subject:

Solicitation of Views and  
Initiation of Section 106 Consultation  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana  
State Project No. H.000688.2  
F.A.P No. H000688

Dear «Salutation»:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns and reach agreeable decisions while taking into account the interests of all parties.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A Stage 0 Feasibility Study for this project was completed in 2008. A project summary and figures are attached for your review. If you would like to review the Stage 0 Study in its entirety, please request a copy from Beth Beam by e-mail at [elizabeth.beam@arcadis-us.com](mailto:elizabeth.beam@arcadis-us.com) or by U.S. mail to Ms. Beth Beam, ARCADIS, 10352 Plaza Americana Drive, Baton Rouge, Louisiana 70816. A project overview, project study area map, and figures are attached for your review.

We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

Imagine the result

ARCADIS U.S., Inc.  
10352 Plaza Americana Drive  
Baton Rouge  
Louisiana 70816  
Tel 225 292 1004  
Fax 225 218 9677  
[www.arcadis-us.com](http://www.arcadis-us.com)

INFRASTRUCTURE

Date:  
3 May 2013

Contact:  
Beth Beam

Extension:  
215

Email:  
[elizabeth.beam@arcadis-us.com](mailto:elizabeth.beam@arcadis-us.com)

Our ref:  
LA003143.0000  
LDOTD/3143/C/1/kp



«Courtesy» «First»  
«Last\_Name»  
3 May 2013

## Closing

On behalf of LADOTD, ARCADIS U.S., Inc. (ARCADIS) requests that you review the attached information and furnish us with your views and comments by **Monday, June 3, 2013**. Replies should be sent to Beth Beam by e-mail or by U.S. mail at the addresses provided. Please reference State Project No. H.000688.2 in your reply.

Sincerely,

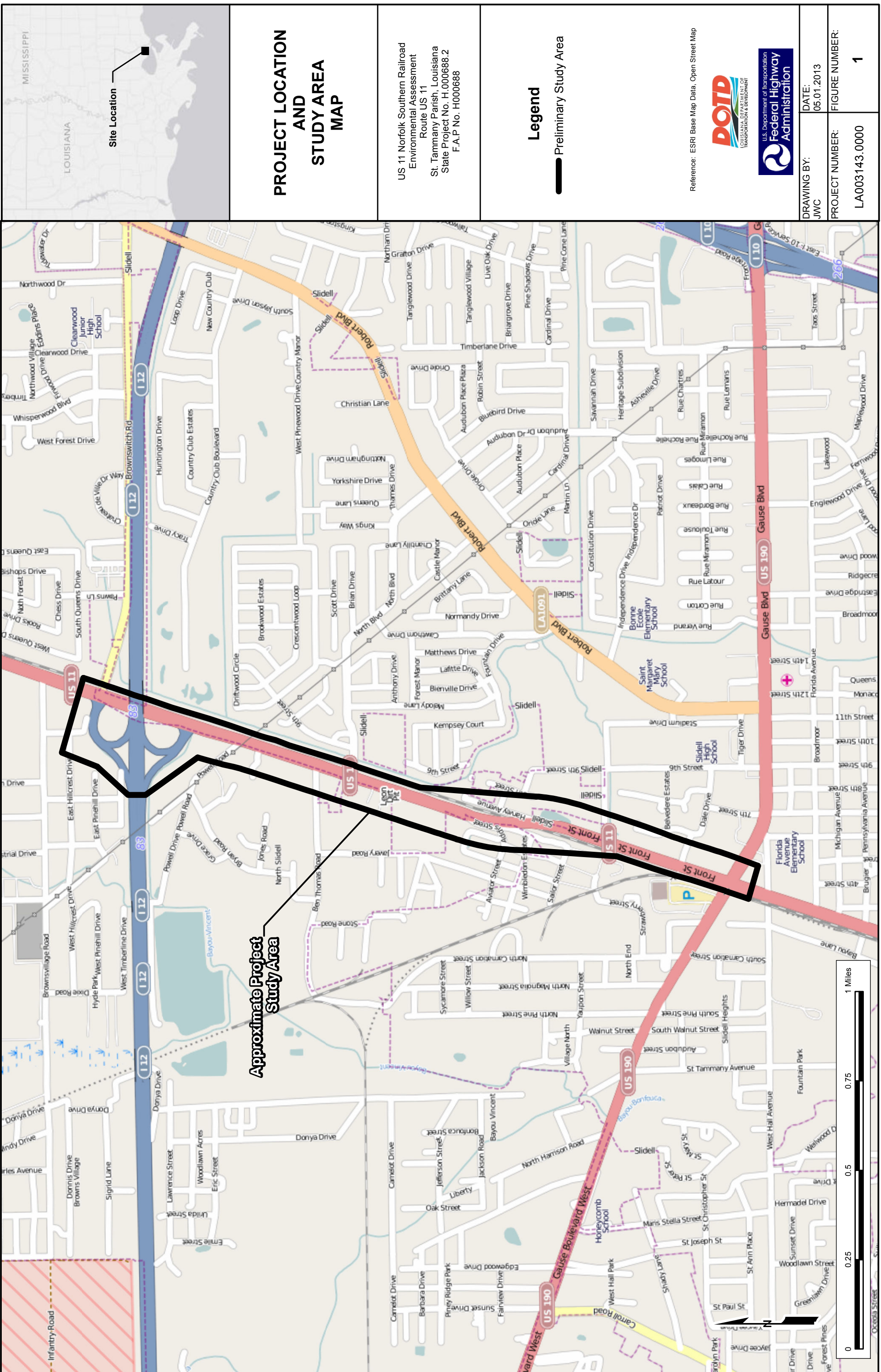
ARCADIS U.S., Inc.

A handwritten signature in black ink that reads "Elizabeth Beam".

Elizabeth Beam, AICP  
Senior Planner/Scientist

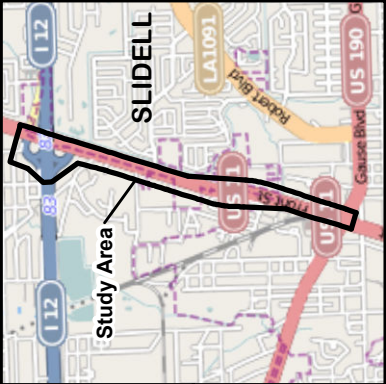
Attachments





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US 11 Norfolk Southern Railroad  
Environmental Assessment  
State Project No. H-000688.2  
E.A.P No. H000688  
Route US 11  
St. Tammany Parish, Louisiana

CORRIDOR PLATES

Legend

--- Preliminary Study Area



Reference: ESRI Base Maps & Imagery



Date: 04/23/2013  
Project Number: LA003413.0000

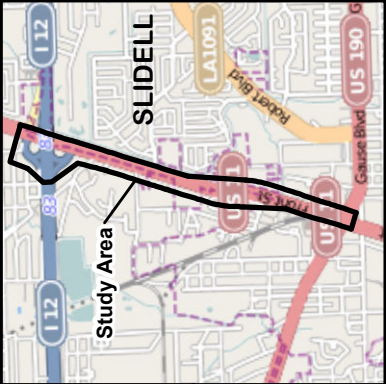
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US 11 Norfolk Southern Railroad  
Environmental Assessment  
State Project No. H-000688.2  
E.A.P No. H000688  
Route US 11  
St. Tammany Parish, Louisiana

CORRIDOR PLATES

Legend

--- Preliminary Study Area



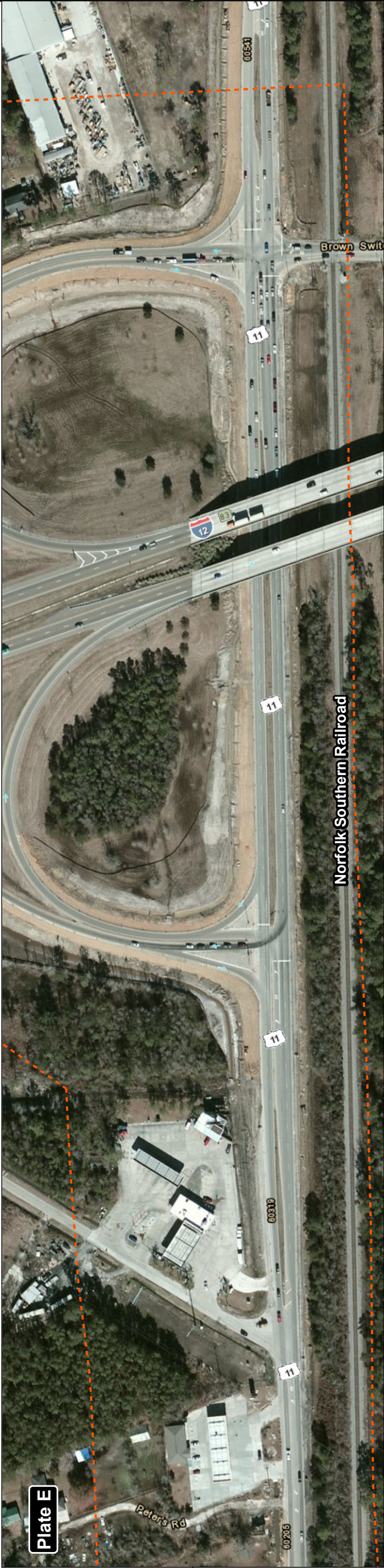
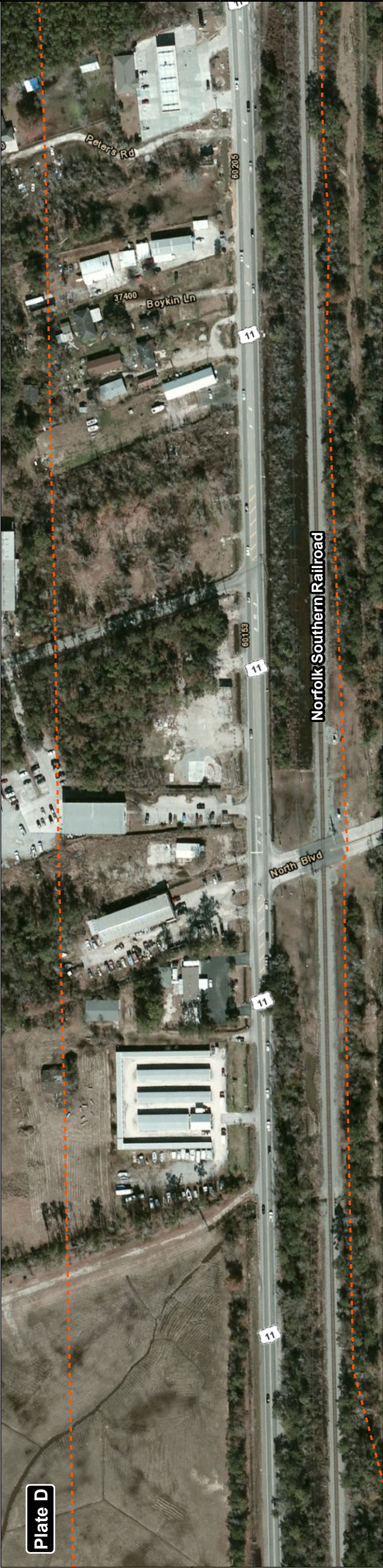
Reference: ESRI Base Maps & Imagery



Date: 04/23/2013  
Project Number: LA003413.0000

Figure No.:

2 - b





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Cat 1	Cat 2	Salutation	Courtesy	First	M	Last Name	Title	Org 1	Org 2	Address	City	State	Zip
State	A	Mr. Adams	Mr.	Elliott		Adams	Director of Business Development	Louisiana Department of Economic Development	Office of Business Development	P.O. Box 94185	Baton Rouge	LA	70804-9185
Federal	E	Representative Alexander	The Honorable	Rodney		Alexander	U.S. Congressman	U.S. House of Representatives (District 5)		1900 Stubbs Avenue, Suite B	Monroe	LA	71201
Federal	A	Mr. Allred	Mr.	Charles	R.	Allred	Supervisory Env Specialist, Section Chief	U.S. Army Corps of Engineers	Vicksburg District	4155 Clay Street	Vicksburg	MS	39183-3435
Parish	E	Mr. Atigue	Mr.	Richard		Atigue		St. Tammany Parish Council, District 13		53057 Hwy 433	Slidell	LA	70461
Federal	A	Mr. Bechtold	Mr.	Michael		Bechtold		U.S. Environmental Protection Agency	Source Water Protection (6WQ-S)	1445 Ross Ave, Suite 1200	Dallas	TX	75202-2733
Parish	E	Mr. Bellisario	Mr.	E.L.		Bellisario		St. Tammany Parish Council, District 9		3090 Gause Blvd, #535	Slidell	LA	70461
Other	T	Mr. Bennett	Mr.	Gliner		Bennett		Apalachee Talmali Band of Louisiana		P.O. Box 84	Libuse	LA	71348
Parish	E	Mr. Binder	Mr.	Jerry		Binder	Chairman	St. Tammany Parish Council, District 12		470 Hickory Drive	Slidell	LA	70458
Federal	E	Representative Boustany	The Honorable	Charles		Boustany Jr., MD	U.S. Congressman	U.S. House of Representatives (District 3)		800 Lafayette Street, Suite 1400	Lafayette	LA	70501
State	A	Ms. Breaux	Ms.	Pam		Breaux	Attn: Mike Varnado, Division of Historic Preservation	Louisiana Department of Culture, Recreation & Tourism		P.O. Box 44247	Baton Rouge	LA	70804-4247
Parish	E	Ms. Brister	Ms.	Pat		Brister	President	St. Tammany Parish		P.O. Box 628	Covington	LA	70434
State	E	Representative Burns	The Honorable	Timothy G.	"Tim"	Burns		Louisiana House of Representatives (District 89)		1 Sanctuary Blvd., Suite 306	Mandeville	LA	70471
Parish	E	Mr. Canulette	Mr.	Chris		Canulette		St. Tammany Parish Council, District 8		109 Stratford Drive	Slidell	LA	70458
Federal	E	Representative Cassidy	The Honorable	Bill		Cassidy, MD	U.S. Congressman	U.S. House of Representatives (District 4)		5555 Hilton Avenue, Suite 100	Baton Rouge	LA	70808
State	A	Mr. Causey	Mr.	Jake		Causey	Chief Engineering	Louisiana Department of Health & Hospitals	Center for Environmental Health	P.O. Box 4489	Baton Rouge	LA	70821
Federal	E	Representative Cedric	The Honorable	Richard		Cedric	U.S. Congressman	U.S. House of Representatives (District 2)		2021 Lakeshore Drive, Suite 309	New Orleans	LA	70122
Other	T	Chief Chretien, Jr.	Chief	Edward		Chretien, Jr.		Atakapa-Ishak Nation		P.O. Box 1532	Lake Charles	LA	70602
State	E	Representative Cromer	The Honorable	Gregory		Cromer		Louisiana House of Representatives (District 90)		P.O. Box 2088	Slidell	LA	70459
State	T	Principal Chief Dardar, Jr.	Principal Chief	Thomas		Dardar, Jr.		United Houma Nation		20986 Highway 1	Golden Meadow	LA	70357
Local		Ms. Davis	Ms.	Shannon		Davis	Director	St. Tammany Parish	Public Works	620 N. Tyler Street	Covington	LA	70433
State	T	Chairman Davis	Chairman	Rufus		Davis, Jr.		Adai Caddo Indians of Louisiana		Route 2, Box 246	Robeline	LA	71469
State	E	Senator Donahue	The Honorable	Jack		Donahue		The State Senate (District 11)		P.O. Box 896	Mandeville	LA	70470
City	E	Mayor Drennan	Mayor	Freddy		Drennan		Mayor, City of Slidell		2045 Second Street	Slidell	LA	70458
State	A	Mr. Dubea	Mr.	Wade		Dubea	Assistant Commissioner	Louisiana Department of Agriculture & Forestry	Office of Forestry	P.O. Box 1628	Baton Rouge	LA	70821
Local		Ms. Edwards	Ms.	Amy		Edwards	Chairman of the Board	St. Tammany West Chamber of Commerce		610 Hollycrest Blvd	Covington	LA	70433
Federal	E	Representative Fleming	The Honorable	John		Fleming, MD	U.S. Congressman	U.S. House of Representatives (District 4)		6425 Youree Drive, Suite 350	Shreveport	LA	71105
Local		Ms. Fontenot	Ms.	Sidney		Fontenot	Director	St. Tammany Parish	Dept of Development/Planning	21454 Koop Drive	Mandeville	LA	70471
State	A	Mr. Ford	Mr.	Mark		Ford	Director	Office of Indian Affairs		P.O. Box 94004	Baton Rouge	LA	70804-9004
State	T	Chairwoman Foret	Chairwoman	Marlene		Foret		Grand Caillou/Dulac Band of the Biloxi Chitimacha		114 Retreat Drive	Bourg	LA	70343
Other	T	Chief Gill	Chief	James	Greywolf	Gill		Louisiana Choctaw Turtle Tribe		379 Sharon Lane	Lake Charles	LA	70611
Other	T	Chief Gillum	Chief	Elwin	Warhors	Gillum		Chahla Tribe		61357 Dixie Ranch Road	Slidell	LA	70460
Federal	A	Ms. Gilmore	Ms.	Cathy		Gilmore	Director of Outdoor Recreation	U.S. Environmental Protection Agency	Federal Activities Branch (6E-F)	1445 Ross Ave, Suite 1200	Dallas	TX	75202-2733
State	A	Mr. Hardeman	Mr.	Cleve		Hardeman		Louisiana Department of Culture, Recreation & Tourism	Office of State Parks	P.O. Box 44426	Baton Rouge	LA	70804
Parish	E	Ms. Heintz	Ms.	Elizabeth	B.	Heintz	President	St. Tammany Parish Public Schools		321 N. Theard Street	Covington	LA	70433-2835
Local		Ms. Herring	Ms.	Ashlee		Herring	Deputy Planning Section Chief	City of New Orleans	Office of Homeland Security & Emergency Preparedness	1300 Perdido Street	New Orleans	LA	70112
Local		Ms. Ingram-Hunter	Ms.	Tara		Ingram-Hunter	Director	City of Slidell Planning Department		250 Bouscaren Street, Suite 203	Slidell	LA	70458
State	G	Ms. Jones	Ms.	Billie	M.	Jones	Project Developer	Office of Cultural Development	Department of Culture, Recreation, and Tourism	P.O. Box 44247	Baton Rouge	LA	70802
Federal	A	Mr. Kamien	Mr.	Douglas	J.	Kamien, PE	Deputy for Programs and Project Management	U.S. Army Corps of Engineers	Vicksburg District	4155 Clay Street	Vicksburg	MS	39183-3435
Federal	E	Senator Landrieu	Senator	Mary		Landrieu		United States Senate	Hale Boggs Federal Building	500 Poydras Street, Rm 1005	New Orleans	LA	70130
State	A	Mr. Leblanc	Mr.	John		Leblanc	Chief, Regulatory Branch	Louisiana Department of Public Safety and Corrections	Highway Safety Commission	P.O. Box 66336	Baton Rouge	LA	70896
Federal	A	Mr. Mayer	Mr.	Martin		Mayer	Manager	U.S. Army Corps of Engineers	New Orleans District	P.O. Box 60267	New Orleans	LA	70160-0267
State	G	Ms. Michon	Ms.	Carolyn		Michon		Louisiana Department of Wildlife and Fisheries	Louisiana Natural Heritage Program	2000 Quail Drive, Room 432	Baton Rouge	LA	70808-9038
State	T	Chief Naquin	Chief	Albert		Naquin		Isle de Jean Charles Band		100 Dennis Street	Montegut	LA	70377
State	T	Chairman Neal	Chairman	Tom		Neal		Clifton Choctaw Tribe of Louisiana		1312 Clifton Road	Clifton	LA	71447
State	E	Senator Nevers	The Honorable	Ben		Nevers		The State Senate (District 12)		724 Avenue F	Bogalusa	LA	70427
Federal	A	Mr. Norton	Mr.	Kevin		Norton	State Conservationist	U.S. Department of Agriculture	Natural Resources Conservation Service	3737 Government Street	Alexandria	LA	71302
Federal	A	Ms. Oberlies	Ms.	Karen		Oberlies	Technical Support	U.S. Department of the Army		P.O. Box 60267	New Orleans	LA	70538
State	E	Senator Pearson	The Honorable	J. Kevin		Pearson		Louisiana House of Representatives (District 76)		620 Oak Harbor Blvd., Suite 203	Slidell	LA	70458
Local		Ms. Pelegrin	Mr.	Alan		Pelegrin	Flood Plain Administrator	St. Tammany Parish		21454 Koop Drive	Mandeville	LA	70471
Other	S	Mr. Perret	Mr.	Ken		Perret		President	Louisiana Good Roads & Transportation Association	P.O. Box 3713	Baton Rouge	LA	70821
State	A	Mr. Philippe	Mr.	Jamie		Philippe		Louisiana Department of Environmental Quality	Office of Environmental Services	P.O. Box 4313	Baton Rouge	LA	70821-4313
State	T	Chairman Procell	Chairman	John		Procell		Choctaw-Apache Tribe of Ebarb		P.O. Box 1428	Zwolle	LA	71486
Federal	G	Mr. Raborn	Mr.	Larry	E.	Raborn	Operations Project Manager	U.S. Army Corps of Engineers	Project Resources Management Branch, Flood Control	4155 Clay Street	Vicksburg	MS	39183-3435
State	T	Mr. Rainwater	Mr.	James		Rainwater		The Louisiana Choctaw Tribe		15212 Hubb Road	Pride	LA	70770
City	E	Mr. Reeves	Mr.	Thomas	P.	Reeves	Council Administrator	City of Slidell		2045 Second Street	Slidell	LA	70458
State	A	Mr. Rieck	Mr.	Brad		Rieck	Deputy Supervisor	U.S. Fish & Wildlife Service	Lafayette Field Office	646 Cajundome Boulevard, Suite 400	Lafayette	LA	70506
State	E	Representative Ritchie	The Honorable	Harold	L.	Ritchie		Louisiana House of Representatives (District 75)		302 Louisiana Avenue	Bogalusa	LA	70427
Federal	E	Representative Scalise	The Honorable	Steve		Scalise	U.S. Congressman	U.S. House of Representatives (District 1)		110 Veterans Blvd., Suite 500	Metairie	LA	70005
State	E	Representative Schroder, Sr	The Honorable	John	M.	Schroder, Sr.	CEO	Louisiana House of Representatives (District 77)		222 N. Vermont Street, Ste K&M	Covington	LA	70433
Local		Ms. Brackett	Ms.	Sawn		Brackett		East St. Tammany Chamber of Commerce		118 West Hall Avenue	Slidell	LA	70460
State	A	Mr. Sibley	Mr.	Tenney		Sibley	Chief Sanitarian	Louisiana Department of Health & Hospitals	Sanitarian Services	P.O. Box 4489	Baton Rouge	LA	70821
State	E	Representative Simon	The Honorable	Scott	M.	Simon		Louisiana House of Representatives (District 74)		P.O. Box 2088	Slidell	LA	70459
Parish	E	Mr. Smith	Mr.	Thomas	J.	Smith		St. Tammany Parish Council, District 14		62390 John Smith Road	Pearl River	LA	70452
State	A	Mr. Solvey	Mr.	Greg		Solvey	Attn: Myra G. Diaz, Natural Hazards Program Specialist	FEMA Region VI		800 North Loop 288	Denton	TX	76201
State	A	Mr. Spicer	Mr.	Brad		Spicer	Assistant Commissioner	Louisiana Department of Agriculture & Forestry	Office of Soil/Water Conservation	P.O. Box 3554	Baton Rouge	LA	70821-3554
State	A	Mr. St. Romain	Mr.	Charles		St. Romain		Division of Administration	Louisiana State Land Office	P.O. Box 44124	Baton Rouge	LA	70804
Parish	E	Mr. Stefancik	Mr.	Steve		Stefancik		St. Tammany Parish Council, District 11		107 Royal Drive	Slidell	LA	70460
Other	S	Mr. Vandersteen	Mr.	Buck		Vandersteen		Executive Director	Louisiana Forestry Association	P.O. Drawer 5067	Alexandria	LA	71301



State	A	Mr. Adams	Mr.	Elliott	Adams	Director of Business Development	Louisiana Department of Economic Development	Office of Business Development	P.O. Box 94185	Baton Rouge	LA	70804-9185
State	A	Ms. Veillon	Ms.	Susan	Veillon, CFM	Floodplain Management Program Coordinator	Louisiana Department of Transportation and Development		1201 Capitol Access Road, 5th Floor	Baton Rouge	LA	70802
State	T	Chairman Verdin	Chairman	Charles	Verdin		Point au Chien Tribe		793 Aragon Road	Montegut	LA	70377
State	T	Chairman Verdun	Chairman	Randy	Verdun		Bayou Lafourche Band of the Biloxi Chitimacha		122 Oakwood Lane	Denham Springs	LA	70726
Federal	E	Senator Vitter	Senator	David	Vitter		United States Senate		2800 Veterans Memorial Blvd., Suite 2	Metairie	LA	70002
Federal	A	Mr. Washburn	Mr.	Eric	Washburn	CG Bridge Administrator, Western Rivers	Commander, 8th USCG District	Bridge Branch	1222 Spruce Street, Room 2.102d	St. Louis	MO	63103
State	A	Mr. Welsh	Mr.	James	H.	Commissioner	Louisiana Department of Natural Resources	Office of Conservation	P.O. Box 94275	Baton Rouge	LA	70804-9275
State	S	Mr. Wilkins	Mr.	James	G.		Louisiana State University	Sea Grant Legal Advisory Service	227B Sea Grant Building	Baton Rouge	LA	70803
State	T	Chief Womack	Chief	Jackie	Womack		Four-Winds Cherokee Tribe		P.O. Box 118	Merryville	LA	70653
State	A	Captain Clark	Captain	Paul	Clark	Commander	Louisiana State Police, Troop L		2600 North Causeway	Mandeville	LA	70471
State							Bogue Chitto Pearl River Soil & Water Conservation District of Louisiana		1111 Washington Street	Franklinton	LA	70438
Federal							National Marine Fisheries Service	Habitat Conservation Division	LSU Center for Wetland RES	Baton Rouge	LA	70803-7535
State	A						Division of Administration	State Planning Office	PO Box 94095	Baton Rouge	LA	70804
Federal	A	Geological Survey					U.S. Department of Interior	U.S. Geological Survey	3535 South Sherwood Forest, Suite 120	Baton Rouge	LA	70816
State	A						U.S. Department of Interior	National Park Service, NPS/Atlanta Federal Center	100 Alabama Street, SW	Atlanta	GA	30303
State	A	Department of Interior					Louisiana Department of Natural Resources	Office of Mineral Resources	P.O. Box 2827	Baton Rouge	LA	70821
Local		Mr. McManus	Mr.		McManus				2045 Second Street, Suite 320	Slidell	LA	70458
Local							St. Tammany Historical Society Inc.		P.O. Box 1001	Mandeville	LA	70470-1001
Local							St. Tammany Parish Sheriff		P.O. Box 1120	Covington	LA	70433
State	E	Senator Crowe	The Honorable	A.G.	Crowe		The State Senate (District 1)		646 Carnation Street	Slidell	LA	70460
State	E	Ms. Ganucheau	Ms.	Jeanne-Marie	Ganucheau			Hale Boggs Federal Building	500 Poydras Street, Rm 1005	New Orleans	LA	70130
Local	A	Mr. Roessel	Mr.	Jeff	Roessel	Deputy Director	Regional Planning Commission	Regional Transportation Management Center	10 Veterans Memorial Blvd	New Orleans	LA	70124-1162
Local	A	Ms. Lala	Ms.	Rebecca	Lala	Traffic Engineer	St. Tammany Parish		21410 Koop Drive	Mandeville	LA	70471
State	A	Ms Altazan-Dixon	Ms.	Beth	Altazan-Dixon	EPS III, Performance Mangement	LDEQ/Office of the Secretary	Business and Community Outreach and Incentives Divis	P.O. Box 4301	Baton Rouge	LA	70821-4301
State	G	Ms. Charrier	Ms.	Christine	Charrier	Program Manager	LA Department of Natural Resources	Office of Coastal Management, Permits Section	617 North Third Street, Suite 1078	Baton Rouge	LA	70804



5/13/2013

## Louisiana Ecological Services Office

### ESA Technical Assistance Form

#### General Information

**Name:** Louisiana Department of Transportation and Development

**Point of Contact:** ARCADIS-US (Scott Hoffeld)

**Address:** 10352 Plaza Americana Dr

**City:** Baton Rouge

**State:** Louisiana

**Zip Code:** 70816

**Phone Number 1:** 2252921004 **Phone Number 2:** 2252921004

**Email Address:** greg.badon@arcadis-us.com

#### Proposed Project Information

**Project Reference ID:** 1024

**Project Latitude:** 30.299803 **Project Longitude:** -89.774668

**Project Parish(es):** Saint Tammany

**Project Description:** The US 11 Norfolk Southern Railroad project includes replacement of the bridge over the Norfolk Southern Railroad and lane widening from two lanes to four lanes for the segment of US Highway 11 (US 11) from Interstate 12 (I-12) south to US Highway 190 (US 190; Gause Boulevard) in Slidell, St. Tammany Parish, Louisiana. The logical termini for the proposed project are US 11 at I-12 and US 11 at US 190. The project consists of providing all necessary services required to prepare an Environmental Assessment (EA) in accordance with the National Environmental Policy Act, as amended, and the Federal Highway Administration's regulations and guidelines.

The purpose of the proposed project is to replace a structurally deficient bridge, while increasing capacity and alleviating congestion.

The project area is located to the east of Airport Road and west of Robert Boulevard (LA 1091) in Slidell, Louisiana. The project study area extends south along US 11 from I-12 to the intersection of US 11 at US 190. The EA will investigate the potential for effects to cultural resources, threatened and endangered species, natural resources, and the

**Louisiana Ecological Services Office****ESA Technical Assistance Form**

human environment within the study area. The proposed project would improve existing roadway infrastructure and will require additional right-of-way.

Based on the information provided, the proposed project is not an activity that would affect a federally listed threatened or endangered species; nor is there proposed or designated critical habitat present within this Parish.

Therefore, a "no effect" conclusion is appropriate. No further ESA coordination with the Service is necessary for the proposed action, unless there are changes in the scope or location of the proposed project or the project has not been initiated one year from the date of this letter.

If the proposed project has not been initiated within one year, follow-up coordination should be accomplished with the Service prior to making expenditures because our threatened and endangered species information is updated annually. If the scope or location of the proposed project is changed, coordination should occur as soon as such changes are made.

This finding completes project review by the Service for effects to Federal trust resources under our jurisdiction and currently protected by the ESA.

Please keep a copy of this pre-development coordination for your records. Additionally, if you would like a copy of this activity kept on file by the Service, please submit a copy to the Louisiana Ecological Services office.

Mailing Address: 646 Cajundome Blvd., Suite 400, Lafayette, LA 70506 Attn: Biological Science Technician  
Email: [Lafayette@fws.gov](mailto:Lafayette@fws.gov)  
Fax: 337/291-3139

If you have additional questions, please contact Louisiana ES Office Biological Science Technician at 337/291-3100 for further assistance.



5/13/2013

## Louisiana Ecological Services Office

### ESA Technical Assistance Form

**Project Type: Other**

Does the project propose to obtain, remodel, refurbish, or rehabilitate existing structures in such a way that does not significantly alter the present capacity or use, and does not alter surrounding land areas that were previously undisturbed? **No**

Does the project propose to reconstruct, resurface, or enhance infrastructure and/or cityscape (e.g. streets, sewers, sidewalks, etc.) within the current footprint of the infrastructure and in a manner that does not disturb previously undisturbed ground? **No**

Is the construction project located entirely within the footprint of an established urban/suburban area (incorporated villages, towns, or cities)? **Yes**



## Beam, Elizabeth

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**From:** Trahan, Amy <amy\_trahan@fws.gov>  
**Sent:** Monday, May 13, 2013 8:56 AM  
**To:** Beam, Elizabeth  
**Subject:** Threatened and Endangered Species Reviews

Elizabeth,

I am writing to let you know about our self-screening website ([www.fws.gov/lafayette](http://www.fws.gov/lafayette)) for threatened and endangered species reviews. The website is easy to use and it is a faster turnaround on response to projects. If you receive a "no effect" determination from this website, then you do not have to send that project to us at all; just print out the page and keep it for your records. If you get a page that tells you your project may have an issue and that you need to send it in for further review, just fill out the page with your project's information, print that out and send it to us with any other information that would help us review the project (i.e. maps). Our office would like everyone to use this website, due to staff shortages and, as I said earlier, a faster turnaround rate for consultants. If you could let your colleagues know about this websites as well, it would be appreciated. If you have any questions or problems with the website, please do not hesitate to contact me at the below phone number or email address.

Thank you for your time.

--

Amy Trahan  
U.S. Fish and Wildlife Service  
646 Cajundome Blvd., Suite 400  
Lafayette, LA 70506  
(337) 291-3126  
[amy\\_trahan@fws.gov](mailto:amy_trahan@fws.gov)



Office of the Secretary  
PO Box 94245 | Baton Rouge, LA 70804-9245  
ph: 225-379-3005 | fx: 225-379-3002

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

June 4, 2013

STATE PROJECT NO.: H.000688.2  
F.A.P. NO.: H000688  
ROUTE: LA 11  
NAME: US 11 NORFOLK SOUTHERN RAILROAD  
PARISH: ST. TAMMANY

Elizabeth Beam, AICP  
Senior Planner/ Scientist  
Arcadis  
10352 Plaza Americana Drive  
Baton Rouge, LA 70816

Subject: Solicitation of Views

Dear Ms. Beam:

Enclosed is a copy of the City of Slidell's Flood Insurance Rate Maps (FIRM's) indicating the proposed project.

During the construction, there must be allowance for the adequate flow of water and assurance that there will be no back up of water. There must be no instance of the creation of flooding where there was no flooding prior to construction. At this time, consideration must be given to the responsibility for cleaning debris and keeping the surrounding area clear so as not to interfere with its function.

In order to assure compliance with the City of Slidell requirements for the National Flood Insurance Program (NFIP), and ensure that appropriate permits are obtained, please contact the floodplain administrator for the City of Slidell. The contact person is: Donna O'Dell, P.E., P.O. Box 828, Slidell, LA, 70459, and telephone no. 985-646-4270.

We thank you for the opportunity to comment on this project. If you need additional information, please contact our office, (225) 379-3005.

Sincerely,

Susan Veillon, CFM  
Floodplain Management Program Coordinator

pc: Donna O'Dell, P.E.

## Beam, Elizabeth

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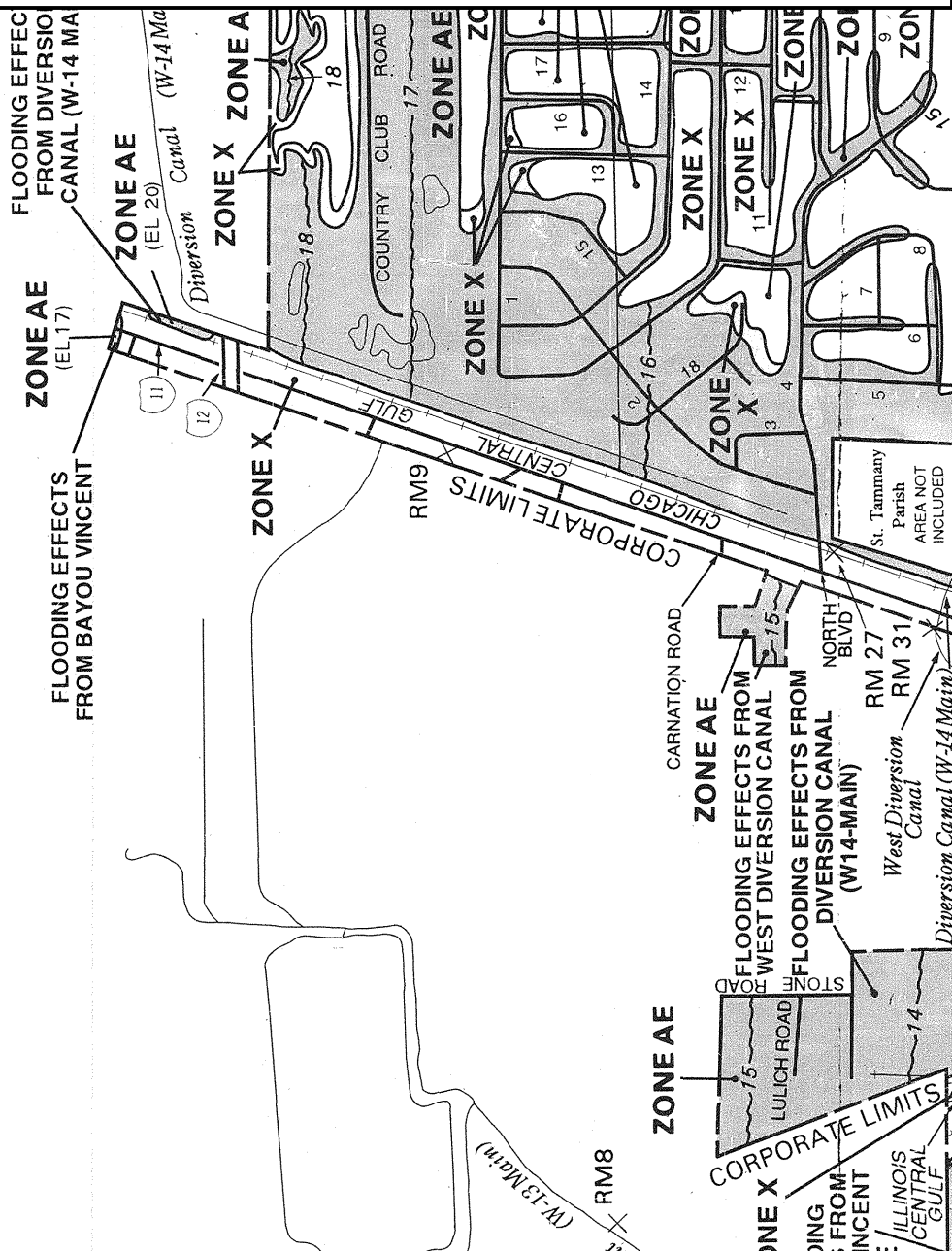
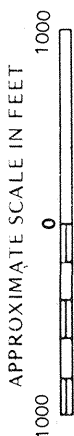
**From:** Susan Veillon (DOTD) <Susan.Veillon@LA.GOV>  
**Sent:** Tuesday, June 04, 2013 1:41 PM  
**To:** Beam, Elizabeth  
**Subject:** SOV, LA 11, SLIDELL, LA  
**Attachments:** LA 11, FROM GAUSE TO I12, SLIDELL, ST TAMMANY PH, MAP 1 OF 2.pdf; LA 11, FROM GAUSE TO I12, SLIDELL, ST TAMMANY PH, MAP 2 OF 2.pdf; LA 11, SLIDELL, ST TAMMANY PARISH.doc

Beth,

I will mail the original. Thanks.

Susan

Susan Veillon, CFM  
Floodplain Management Program Coordinator  
1201 Capitol Access Road, 5th Floor  
Baton Rouge, LA 70802  
Phone 225-379-3005  
Fax 225-379-3002



# NATIONAL FLOOD INSURANCE PROGRAM

# FIRM FLOOD INSURANCE RATE MAP

CITY OF  
SLIDELL,  
LOUISIANA  
ST. TAMMANY PARISH

(SEE MAP INDEX FOR PANELS NOT PRINTED)

**COMMUNITY-PANEL NUMBER**  
**220204 0005 C**

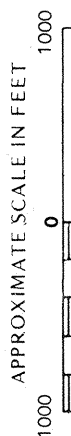
**MAP REVISED:  
APRIL 21, 1999**



**Federal Emergency Management Agency**

This is an official copy of a portion of the above referenced flood map. It was extracted using F-MIT On-Line. This map does not reflect changes or amendments which may have been made subsequent to the date on the title block. For the latest product information about National Flood Insurance Program flood maps check the FEMA Flood Map Store at [www.msc.fema.gov](http://www.msc.fema.gov)





# NATIONAL FLOOD INSURANCE PROGRAM

## FIRM FLOOD INSURANCE RATE MAP

CITY OF  
SLIDELL,  
LOUISIANA  
ST. TAMMANY PARISH

(SEE MAP INDEX FOR PANELS NOT PRINTED)

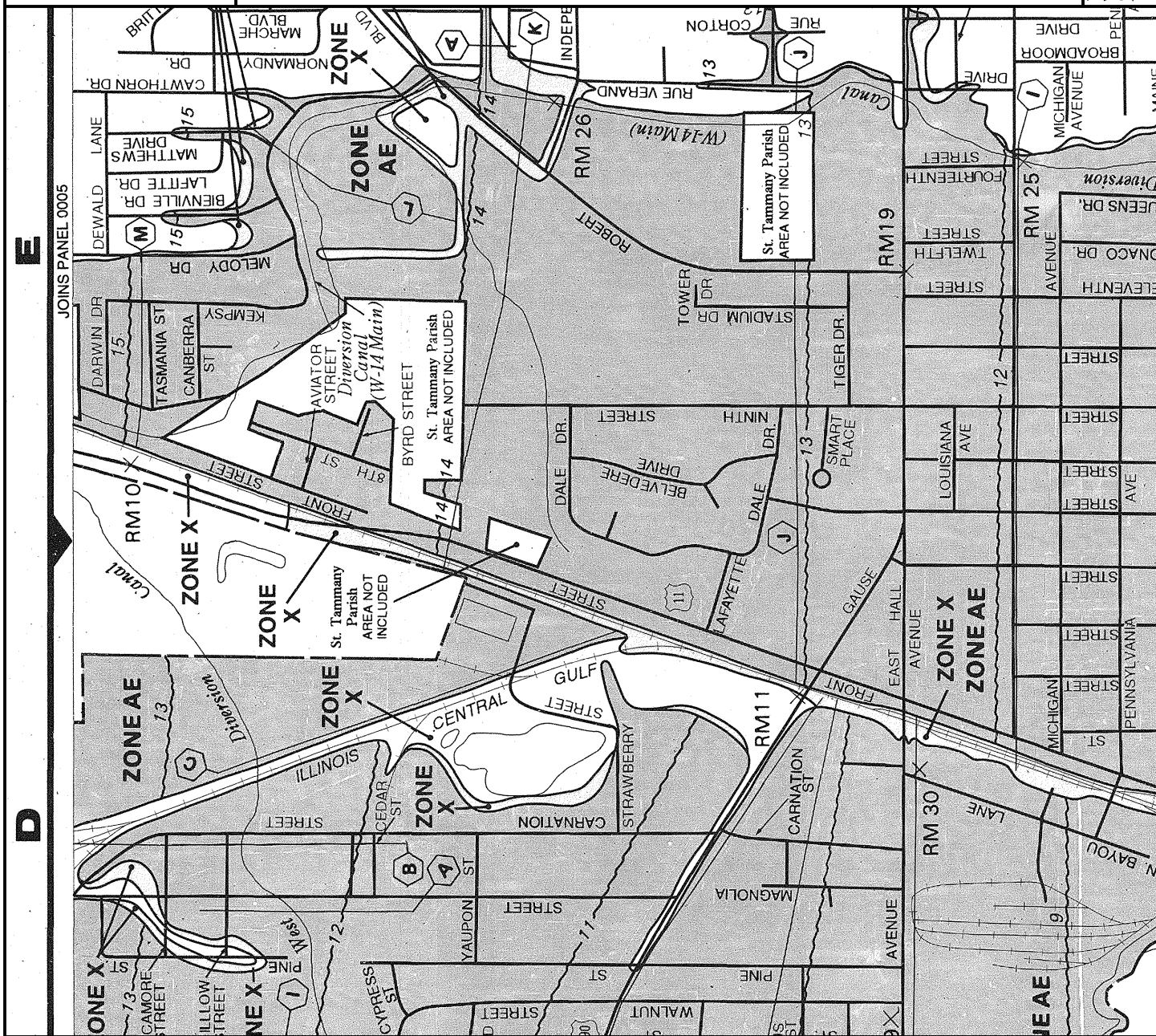
COMMUNITY-PANEL NUMBER  
220204 0010 C

**MAP REVISED:  
APRIL 21, 1999**



**Federal Emergency Management Agency**

This is an official copy of a portion of the above referenced flood map. It was extracted using F-MIT On-Line. This map does not reflect changes or amendments which may have been made subsequent to the date on the title block. For the latest product information about National Flood Insurance Program flood maps check the FEMA Flood Map Store at [www.msc.fema.gov](http://www.msc.fema.gov)



## Beam, Elizabeth

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**From:** Robert Lott <Robert.Lott@LA.GOV>  
**Sent:** Tuesday, July 09, 2013 3:48 PM  
**To:** Beam, Elizabeth  
**Subject:** FW: US 11 Norfolk Southern Railroad

FYI

Thanks,  
Bobby Lott  
DOTD - Environmental Section  
225-242-4504

---

**From:** Charles E. Williams [<mailto:eddie@stpgov.org>]  
**Sent:** Tuesday, July 09, 2013 10:55 AM  
**To:** Robert Lott  
**Subject:** US 11 Norfolk Southern Railroad

Robert,

Sorry for the late response. The letter was lost in transmittal to me. If possible, St Tammany Parish would like to participate in the environmental assessment of the US 11 project.  
If you need to contact me, my cell is 985-630-5087.

Thanks  
Eddie Williams, PE  
Director of Engineering  
St Tammany Parish

## Beam, Elizabeth

---

**From:** Steve Stefancik <steves@stpgov.org>  
**Sent:** Friday, May 17, 2013 3:50 PM  
**To:** Beam, Elizabeth  
**Subject:** Comments on State Project No. H.000688.2

**Follow Up Flag:** FollowUp  
**Flag Status:** Flagged

This is a very critical project for the improvement of traffic and transportation in this area. I do not know of any detrimental environmental impacts to the area.

I do not believe that the "no build" alternative is a viable concept and should not be considered.

Steve Stefancik  
St. Tammany Parish Council  
District 11



JAY DARDENNE  
LIEUTENANT GOVERNOR

**State of Louisiana**  
OFFICE OF THE LIEUTENANT GOVERNOR  
DEPARTMENT OF CULTURE, RECREATION & TOURISM  
OFFICE OF CULTURAL DEVELOPMENT

CHARLES R. DAVIS  
DEPUTY SECRETARY

PAM BREAU  
ASSISTANT SECRETARY

May 20, 2013

Elizabeth Beam  
Arcadis U.S., Inc.  
10352 Plaza Americana Drive  
Baton Rouge, LA 70816

Re: Section 106 Request for Additional Information  
State Project No. H.000688.2  
US 11 Norfolk Southern Railroad  
St. Tammany Parish, LA

Dear Ms. Beam:

Thank you for your letter of May 3, 2013, concerning the above-referenced undertaking. We are unable to complete the Section 106 review at this time due to the submittal of insufficient documentation. We will need the following information to complete our review for the aforementioned project:

- ☐ Name of federal agency, agency involvement (Funding, license/permit, etc. and description of the undertaking (Detailed description of project).
- ☐ Applicant contact information (Name, address, phone number and email address).
- ☐ Agency contact information (Name, address, phone number and email address).
- ☒ Description of the Area of Potential Effects (APE). The APE can be direct or indirect. It is defined as "the geographic area or areas within which an undertaking may cause changes in the character or use of historic properties, if any such properties exist." (Include the latitude/longitude of the undertaking location and APE)
- ☒ Description of all historic properties within and adjacent to the APE. The historic standing structure is any structure fifty years of age and older. Under Section 106, it is the responsibility of the federal agency or its designee to identify all structures listed or eligible for listing in the National Register of Historic Places.
- ☒ Detailed project scope of work including design plans.
- ☒ Map and site plan showing APE and exact location of project undertaking.



Elizabeth Beam  
May 20, 2013  
Page 2

☒ Photographs of the entire APE and project location. Photographs of all historic (fifty years of age and older) within the APE. Buildings should be documented showing diagonal views of front and side and rear and opposite side of the building. All photos should be keyed to a site map and project plans if applicable.

If you have any questions, please contact Mike Varnado in the Division of Historic Preservation at (225) 219-4596 or [mvarnado@crt.la.gov](mailto:mvarnado@crt.la.gov).

Sincerely,



Pam Breaux  
State Historic Preservation Officer

PB:MV:s

## Beam, Elizabeth

---

**From:** Paul Carroll <pcarroll@stpgov.org>  
**Sent:** Thursday, May 23, 2013 1:29 PM  
**To:** Beam, Elizabeth  
**Cc:** Alan Pelegrin; Elizabeth D. Smythe; Daniel Bond; Donna ODell; Charles E. Williams; Rebecca Lala  
**Subject:** US 11 Norfolk Southern Railroad Environmental Assessment Comments

I have reviewed the proposed widening of HWY 11 between I-12 and Gause Boulevard.

One concern would be that the inlet capacity and storage capacity of the large on-line retention pond adjacent to HWY 11 not be reduced. St. Tammany Parish has a flood model of this pond which was made by the New Orleans District USACE for a future flood control project which utilizes this pond. We request that the consultant works with St. Tammany Parish, the City of Slidell, and the USACE to ensure compatibility with the W-14 SELA project.

Where practical, it would be helpful if widening could be aligned to minimize impacts to existing businesses.

Thanks

Paul Carroll, P.E.  
Drainage Engineer  
St. Tammany Parish Government, Dept. of Engineering  
898-2750

Any e-mail may be construed as a public document, and may be subject to a public records request.

United States Department of Agriculture



Natural Resources Conservation Service  
3737 Government Street  
Alexandria, LA 71302

(318) 473-7751  
Fax: (318) 473-7626

May 6, 2013

Beth Beam  
Arcadis U.S., Inc.  
10352 Plaza Americana Drive  
Baton Rouge, LA 70816

RE: US 11 Norfolk Southern Railroad - State Project No.: H.000688.2

Dear Ms. Beam:

I have reviewed the above referenced project for potential requirements of the Farmland Protection Policy Act (FPPA) and potential impact to Natural Resource Conservation Service projects in the immediate vicinity.

Projects are subject to FPPA requirements if they may irreversibly convert farmland (directly or indirectly) to nonagricultural use and are completed by a federal agency or with assistance from a federal agency. For the purpose of FPPA, farmland includes prime farmland, unique farmland, and land of statewide or local importance. Farmland subject to FPPA requirements can be forest land, pastureland, cropland, or other land, but not water or urban built-up land.

The project map submitted with your request indicates that the proposed construction areas will not impact prime farmland and therefore is exempt from the rules and regulations of the Farmland Protection Policy Act (FPPA) - Subtitle I of Title XV, Section 1539-1549. Furthermore, we do not predict impacts to NRCS work in the vicinity.

For specific information about the soils found in the project area, please visit our Web Soil Survey at the following location: <http://websoilsurvey.nrcs.usda.gov/>

Please direct all future correspondence to me at the address shown above.

Respectfully,

A handwritten signature in blue ink that reads "Sarah Haymaker".

Sarah Haymaker  
Acting State Conservationist



BOBBY JINDAL  
GOVERNOR

**State of Louisiana**  
DEPARTMENT OF WILDLIFE AND FISHERIES  
OFFICE OF WILDLIFE


ROBERT J. BARHAM  
SECRETARY  
JIMMY L. ANTHONY  
ASSISTANT SECRETARY

**Date** May 16, 2013  
**Name** Elizabeth Beam  
**Company** ARCADIS  
**Street Address** 10352 Plaza Americana Drive  
**City, State, Zip** Baton Rouge, LA 70816  
**Project** US 11 Norfolk Southern Railroad  
State Project No. H.000688.2  
**Project ID** 1592013  
**Invoice Number** 13051610

Personnel of the Habitat Section of the Coastal & Nongame Resources Division have reviewed the preliminary data for the captioned project. After careful review of our database, no impacts to rare, threatened, or endangered species or critical habitats are anticipated for the proposed project. No state or federal parks, wildlife refuges, scenic streams, or wildlife management areas are known at the specified site within Louisiana's boundaries.

The Louisiana Natural Heritage Program (LNHP) has compiled data on rare, endangered, or otherwise significant plant and animal species, plant communities, and other natural features throughout the state of Louisiana. Heritage reports summarize the existing information known at the time of the request regarding the location in question. The quantity and quality of data collected by the LNHP are dependent on the research and observations of many individuals. In most cases, this information is not the result of comprehensive or site-specific field surveys; many natural areas in Louisiana have not been surveyed. This report does not address the occurrence of wetlands at the site in question. Heritage reports should not be considered final statements on the biological elements or areas being considered, nor should they be substituted for on-site surveys required for environmental assessments. LNHP requires that this office be acknowledged in all reports as the source of all data provided here. If at any time Heritage tracked species are encountered within the project area, please contact the LNHP Data Manager at 225-765-2643. If you have any questions, or need additional information, please call 225-765-2357.

Sincerely,

  
Amity Bass, Coordinator  
Natural Heritage Program





**FEMA**

FEDERAL EMERGENCY MANAGEMENT AGENCY  
REGION VI  
MITIGATION DIVISION

## NOTICE REVIEW/ENVIRONMENTAL CONSULTATION

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☐ We have no comments to offer.      ☒ We offer the following comments:

**WE WOULD REQUEST THAT THE PARISH FLOODPLAIN ADMINISTRATOR BE CONTACTED FOR THE REVIEW AND POSSIBLE PERMIT REQUIREMENTS FOR THIS PROJECT. IF FEDERALLY FUNDED, WE WOULD REQUEST PROJECT TO BE IN COMPLIANCE WITH EO11988 & EO 11990.**

Alan Pelegrin  
FPA  
St. Tammany Parish  
21490 Koop Drive  
Mandeville, LA 70471  
985-898-2574

---

REVIEWER:

*Mayra G. Diaz*  
Floodplain Management and Insurance Branch  
Mitigation Division  
(940) 898-5541

DATE: May 7, 2013

RECEIVED FIC MAIL ROOM  
FEMA REGION VI

2013 APR 30 P 1:29

Mr. Greg Solvey  
Attn: Myra G. Diaz,  
Natural Hazards Program Specialist  
FEMA Region VI  
800 North Loop 288  
Denton, TX 76201

ARCADIS U.S., Inc.  
10352 Plaza Americana Drive  
Baton Rouge  
Louisiana 70816  
Tel 225 292 1004  
Fax 225 218 9677  
[www.arcadis-us.com](http://www.arcadis-us.com)

Subject:  
Solicitation of Views and  
Initiation of Section 106 Consultation  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana  
State Project No. H.000688.2  
F.A.P No. H000688

INFRASTRUCTURE

Date:  
3 May 2013

Dear Mr. Solvey:

Contact:  
Beth Beam

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns and reach agreeable decisions while taking into account the interests of all parties.

Extension:  
215

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

Email:  
[elizabeth.beam@arcadis-us.com](mailto:elizabeth.beam@arcadis-us.com)

A Stage 0 Feasibility Study for this project was completed in 2008. A project summary and figures are attached for your review. If you would like to review the Stage 0 Study in its entirety, please request a copy from Beth Beam by e-mail at [elizabeth.beam@arcadis-us.com](mailto:elizabeth.beam@arcadis-us.com) or by U.S. mail to Ms. Beth Beam, ARCADIS, 10352 Plaza Americana Drive, Baton Rouge, Louisiana 70816. A project overview, project study area map, and figures are attached for your review.

Our ref:  
LA003143.0000  
LDOTD/3143/C/1/kp

We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

Imagine the result



Mr. Greg Solvey  
3 May 2013

**Closing**

On behalf of LADOTD, ARCADIS U.S., Inc. (ARCADIS) requests that you review the attached information and furnish us with your views and comments by **Monday, June 3, 2013**. Replies should be sent to Beth Beam by e-mail or by U.S. mail at the addresses provided. Please reference State Project No. H.000688.2 in your reply.

Sincerely,

ARCADIS U.S., Inc.

A handwritten signature in black ink that reads 'Elizabeth Beam'.

Elizabeth Beam, AICP  
Senior Planner/Scientist

Attachments



**UNITED STATES ENVIRONMENTAL PROTECTION AGENCY**  
REGION 6  
1445 ROSS AVENUE, SUITE 1200  
DALLAS, TX 75202-2733

May 8, 2013

Ms. Elizabeth Beam, AICP  
Senior Planner/Scientist  
Arcadis U.S., Inc.  
10352 Plaza Americana Dr.  
Baton Rouge, LA 70816

Dear Ms. Beam:

We have received your May 3, 2013, letter via email requesting our evaluation of the potential environmental impacts which might result from the following project:

**Bridge Replacement & Lane Widening  
US 11 over Norfolk Southern Railroad  
from I-12 south to US Hwy 190  
SP No. H.000688.2  
F.A.P. No. H.000688.2  
St. Tammany Parish, Louisiana**

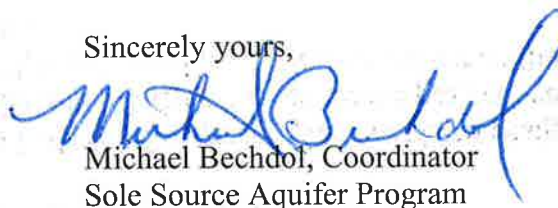
The project, is located on the Southern Hills aquifer system which has been designated a sole source aquifer by the EPA. Based on the information provided for the project, we have determined that the project, as proposed, should not have an adverse effect on the quality of the ground water underlying the project site.

This approval of the proposed project does not relieve the applicant from adhering to other State and Federal requirements, which may apply. This approval is based solely upon the potential impact to the quality of ground water as it relates to the EPA's authority pursuant to Section 1424(e) of the Safe Drinking Water Act.

If you did not include the Parish; a legal description; project location and the latitude and longitude if available, please do so in future Sole Source Aquifer correspondence.

If you have any questions on this letter or the sole source aquifer program please contact me at (214) 665-7133.

Sincerely yours,

  
Michael Bechdol, Coordinator  
Sole Source Aquifer Program  
Ground Water/UIC Section

cc: Jesse Means, LDEQ  
Noel Ardoin, LDOTD





RECEIVED  
JUN 10 2013

**BOBBY JINDAL**  
GOVERNOR

**State of Louisiana**  
**DEPARTMENT OF NATURAL RESOURCES**  
**OFFICE OF CONSERVATION**

**BY:** .....

**STEPHEN CHUSTZ**  
INTERIM SECRETARY

**JAMES H. WELSH**  
COMMISSIONER OF CONSERVATION

June 5, 2013

**TO:** Ms. Elizabeth Beam  
Senior Planner/Scientist  
Arcadis U.S. Inc.  
10352 Plaza Americana Drive  
Baton Rouge, Louisiana 70816

**RE:** Solicitation of Views  
State Project No. H.000688.2  
US 11 Norfolk Southern Railroad  
St. Tammany Parish

Dear Ms. Beam:

In response to your letter dated May 3, 2013, concerning the referenced matter, please be advised that the Office of Conservation collects and maintains many types of information regarding oil and gas exploration, production, distribution, and other data relative to the petroleum industry as well as related and non-related injection well information, surface mining and ground water information and other natural resource related data. Most information concerning oil, gas and injection wells for any given area of the state, including the subject area of your letter can be obtained through records search via the SONRIS data access application available at:

<http://www.dnr.louisiana.gov>


A review of our computer records for the referenced project area indicates that there is, or was, present in the project area a well that was drilled in the search for oil and gas. The DNR water well database indicates that there are registered water wells in the vicinity of the project area. Also, unregistered water wells may be located in the area.

The Office of Conservation maintains records of all activities within its jurisdiction in paper, microfilm or electronic format. These records may be accessed during normal business hours, Monday through Friday, except on State holidays or emergencies that require the Office to be closed. Please call 225-342-5540 for specific contact information or for directions to the Office of Conservation, located in the LaSalle Building, 617 North Third Street, Baton Rouge, Louisiana. For pipelines and other underground hazards, please contact Louisiana One Call at 1-800-272-3020 prior to commencing operations. Should you need to direct your inquiry to any of our Divisions, you may use the following contact information:

<u>Division</u>	<u>Contact</u>	<u>Phone No.</u>	<u>E-mail Address</u>
Engineering	Jeff Wells	225-342-5638	jeff.wells@la.gov
Pipeline	Steven Giambrone	225-342-2989	steven.giambrone@la.gov
Injection & Mining	Laurence Bland	225-342-5515	laurence.bland@la.gov
Geological	Mike Kline	225-342-3335	mike.kline@la.gov
Environmental	Gary Snellgrove	225-342-7222	gary.snellgrove@la.gov

If you have difficulty in accessing the data via the referenced website because of computer related issues, you may obtain assistance from our technical support section by selecting Help on the SONRIS tool bar and submitting an email describing your problems and including a telephone number where you may be reached.

Sincerely,

  
James H. Welsh  
Commissioner of Conservation

JHW:MSK

## Beam, Elizabeth

---

**From:** Office of Coastal Management <bpel.mail@la.gov>  
**Sent:** Monday, August 12, 2013 2:16 PM  
**To:** LEAANN.BAKER@LA.GOV; Beam, Elizabeth  
**Subject:** P20130672- comment received



---

PLEASE DO NOT REPLY TO THIS EMAIL. THIS ACCOUNT IS NOT MONITORED FOR INCOMING MAIL. If you would like to talk to someone about this application please call 225-342-7591 or 1-800-267-4019.

### Coastal Use Permit Application Information

**Applicant:** LA DOTD  
**Project:** US 11 Norfolk Southern Railroad Environmental Assessment Route US 11  
**Project Parish(es):** SAINT TAMMANY

**OCM Analyst:** Sharon Mccarthy  
**Preliminary Determination:** SOV Application Required  
**Application Modification:** N/A

Comment from Other has been received for the referenced Coastal Use Permit Application. Please be advised that you may be required to address any issues raised by these comments.

[View Comments](#)

## Beam, Elizabeth

---

**From:** Office of Coastal Management <bpel.mail@la.gov>  
**Sent:** Tuesday, July 09, 2013 2:45 PM  
**To:** LEAANN.BAKER@LA.GOV; SHARON.TRAHAN@LA.GOV; Beam, Elizabeth  
**Subject:** P20130672- comment received



---

PLEASE DO NOT REPLY TO THIS EMAIL. THIS ACCOUNT IS NOT MONITORED FOR INCOMING MAIL. If you would like to talk to someone about this application please call 225-342-7591 or 1-800-267-4019.

### Coastal Use Permit Application Information

**Applicant:** LA DOTD  
**Project:** US 11 Norfolk Southern Railroad Environmental Assessment Route US 11  
**Project Parish(es):** SAINT TAMMANY

**OCM Analyst:** Sharon Mccarthy  
**Preliminary Determination:** SOV Application Required  
**Application Modification:** N/A

Comment from Other has been received for the referenced Coastal Use Permit Application. Please be advised that you may be required to address any issues raised by these comments.

[View Comments](#)



## Beam, Elizabeth

---

**From:** Office of Coastal Management <bpel.mail@la.gov>  
**Sent:** Tuesday, May 28, 2013 1:12 PM  
**To:** LEAANN.BAKER@LA.GOV; SHARON.TRAHAN@LA.GOV; Beam, Elizabeth  
**Subject:** P20130672- comment received

**Follow Up Flag:** Flag for follow up  
**Flag Status:** Flagged



---

PLEASE DO NOT REPLY TO THIS EMAIL. THIS ACCOUNT IS NOT MONITORED FOR INCOMING MAIL. If you would like to talk to someone about this application please call 225-342-7591 or 1-800-267-4019.

### Coastal Use Permit Application Information

**Applicant:** LA DOTD  
**Project:** US 11 Norfolk Southern Railroad Environmental Assessment Route US 11  
**Project Parish(es):** SAINT TAMMANY

**OCM Analyst:** Sharon Mccarthy  
**Preliminary Determination:** SOV Application Required  
**Application Modification:** N/A

Comment from Other has been received for the referenced Coastal Use Permit Application. Please be advised that you may be required to address any issues raised by these comments.

[View Comments](#)

## Beam, Elizabeth

---

**From:** Office of Coastal Management <bpel.mail@la.gov>  
**Sent:** Monday, May 20, 2013 4:17 PM  
**To:** LEAANN.BAKER@LA.GOV; SHARON.TRAHAN@LA.GOV; TIM.KILLEEN@LA.GOV;  
DBOND@STPGOV.ORG; DBUTLER@WLF.LA.GOV; Beam, Elizabeth;  
UCM\_MAIL@LA.GOV  
**Subject:** P20130672 - Processing Complete



---

PLEASE DO NOT REPLY TO THIS EMAIL. THIS ACCOUNT IS NOT MONITORED FOR INCOMING MAIL. If you would like to talk to someone about this application please call 225-342-7591 or 1-800-267-4019.

### Coastal Use Permit Application Information

**Applicant:** LA DOTD  
**Project:** US 11 Norfolk Southern Railroad Environmental Assessment Route US 11  
**Project Parish(es):** SAINT TAMMANY

**OCM Analyst:** Sharon Mccarthy  
**Final Determination:** SOV Application Required  
**Application Modification:** N/A

Processing of the above application has been completed. Click the link below to view the final determination:

[Authorization](#)

P20130672



Infrastructure · Water · Environment · Buildings

Ms. Christine Charrier  
Program Manager  
Office of Coastal Management, Permits Section  
LA Department of Natural Resources  
617 North Third Street, Suite 1078  
Baton Rouge, Louisiana 70804

ARCADIS U.S., Inc.  
10352 Plaza Americana Drive  
Baton Rouge  
Louisiana 70816  
Tel 225.292.1004  
Fax 225.218.9677  
www.arcadis-us.com

Subject:  
Environmental Assessment  
US 11 Norfolk Southern Railroad  
Route US 11  
St. Tammany Parish, Louisiana  
State Project No. H.000688.2  
F.A.P. No. H000688

INFRASTRUCTURE

30 July 2013

Dear Ms. Charrier:

On behalf of the Federal Highway Administration (FHWA) in cooperation with the Louisiana Department of Transportation and Development (LADOTD), we invite you to an officials meeting for the above-referenced project. The meeting will be held at the Slidell City Auditorium, 2056 2<sup>nd</sup> Street, Slidell, Louisiana 70458, from **3:00 p.m. to 4:00 p.m. on Thursday, August 22, 2013**. The consultant team will present a project overview and representatives from LADOTD, FHWA, and the consultant team will facilitate a discussion on issues material to the environmental review process. The officials meeting will be followed by a brief **key stakeholders meeting from 4:00 p.m. to 4:30 p.m.**

Contact:  
Beth Beam

Extension:  
215

Email:  
elizabeth.beam@arcadis-us.com

Our ref:  
LA003143.0001.00003  
LADOTD/3143/C/8/jk

We would also like to remind you that a **public meeting will be held at the same location on the same day from 5:00 p.m. to 7:00 p.m.** The public has been invited to this meeting to learn more about the project, discuss issues, and ask questions. Comments will be accepted at the public meeting and by mail postmarked no later than **Monday, September 2, 2013**.

If you would like to receive invitations for meetings and other notices via email, please reply to me at [elizabeth.beam@arcadis-us.com](mailto:elizabeth.beam@arcadis-us.com) to be added to the project database.

Sincerely,

ARCADIS U.S., Inc.

A handwritten signature in cursive script that reads 'Elizabeth Beam'.

Elizabeth Beam, AICP  
Associate Project Manager

Imagine the result

2013 AUG -1 PM 1:39  
RECEIVED  
OFFICE OF COASTAL MANAGEMENT



REPLY TO  
ATTENTION OF

Operations Division  
Operations Manager,  
Completed Works

Elizabeth Beam, AICP  
Senior Planner/ Scientist  
ARCADIS U.S., Inc.  
10352 Plaza Americana Drive  
Baton Rouge, LA 70816

Dear Ms. Beam:

This is in response to your Solicitation of Views request dated May 3, 2013, concerning the Route US-11 widening and bridge replacement at Slidell in St. Tammany Parish, Louisiana.

We have reviewed your request for potential Department of the Army regulatory requirements and impacts on any Department of the Army projects.

We do not anticipate any adverse impacts to any Corps of Engineers projects.

Based on the limited information provided, we have determined that information and signatures obtained from recent maps, aerial photography, and local soil surveys concerning the proposed project are indicative of the occurrence of waters of the United States, including wetlands. Department of the Army (DA) permits are required prior to the deposition and/or redistribution of dredged or fill material into jurisdictional waters and wetlands.

This preliminary determination is advisory in nature. If an approved delineation is needed, please furnish us with the detailed field data concerning vegetations, soils, and hydrology that we require for all jurisdictional decisions. The fact that a field wetland delineation/determination has not been completed does not alleviate your responsibility to obtain the proper DA permits prior to working in jurisdictional wetlands or waters occurring on this property.

Please be advised that this property is in the Louisiana Coastal Zone. For additional information regarding coastal use permit requirements, contact Ms. Christine Charrier, Office of Coastal Management, Louisiana Department of Natural Resources at (225) 342 7953.

Off-site locations of activities such as borrow, disposals, haul-and detour-roads and work mobilization site developments may be subject to Department of the Army regulatory

DEPARTMENT OF THE ARMY  
NEW ORLEANS DISTRICT, CORPS OF ENGINEERS  
P. O. BOX 60267  
NEW ORLEANS, LOUISIANA 70160-0267

OFFICE OF COASTAL MANAGEMENT

2013 JUL -9 PM 2:09

RECEIVED

P20130672



requirements and may have an impact on a Department of the Army project.

You should apply for said permit well in advance of the work to be performed. The application should include sufficiently detailed maps, drawings, photographs, and descriptive text for accurate evaluation of the proposal.

Please contact Mr. Robert Heffner, of our Regulatory Branch by telephone at (504) 862-1288, or by e-mail at Robert.A.Heffner@usace.army.mil for questions concerning wetlands determinations or need for on-site evaluations. Questions concerning regulatory permit requirements may be addressed to Mr. Michael Farabee by telephone at (504) 862-2292 or by email at Michael.V.Farabee@usace.army.mil.

Future correspondence concerning this matter should reference our account number MVN-2013-01421-SA. This will allow us to more easily locate records of previous correspondence, and thus provide a quicker response.

We apologize for missing the target date of June 3, 2013 listed in your request. Thank you for your patience in this matter.

Sincerely,

Karen L. Clement  
Solicitation of Views Manager

Copy Furnished:

Ms. Christine Charrier  
Coastal Zone Management  
Department of Natural Resources  
Post Office Box 44487  
Baton Rouge, Louisiana 70804-4487





**State of Louisiana**  
**DEPARTMENT OF NATURAL RESOURCES**  
**OFFICE OF COASTAL MANAGEMENT**

05/20/2013

ARCADIS U.S., INC.  
10352 PLAZA AMERICANA DRIVE  
BATON ROUGE, LA 70816  
Attn: Elizabeth Beam

**RE: P20130672, Solicitation of Views  
LA DOTD**

**Description:** Proposed US 11 Norfolk Southern Railroad Project (State Project No. H.000688.2) includes replacement of the bridge over the Norfolk Southern Railroad and lane widening from two lanes to four lanes from US Hwy. 11 to US Hwy. 190.

**Location:** Lat 30° 18' 30"N / Long 89° 46' 20"W; US Hwy 11 from I-12 south to US Hwy 190 in Slidell.

**Saint Tammany Parish, LA**

Dear Elizabeth Beam:

We have received your Solicitation of Views for the above referenced project, which has been found to be inside the Louisiana Coastal Zone. In order for us to properly review and evaluate this project, we require that a complete Coastal Use Permit Application packet (Joint Application Form, locality maps, project illustration plats with plan and cross section views, etc.) along with the appropriate application fee be submitted to our office. Using your complete application, we can provide you with an official determination, and begin the processing of any Coastal Use Permit that may be required for your project. You may obtain a free application packet by calling our office at (225) 342-7591 or (800)-267-4019, or by visiting our website at <http://www.dnr.state.la.us/crm/coastmgt/cup/cup.asp>.

We recommend that, during your planning process, you make every effort to minimize impacts to vegetated wetlands. As our legislative mandate puts great emphasis on avoiding damages to these habitats, in many cases the negotiations involved in reducing such disturbances and developing the required mitigation to offset the lost habitat values delay permit approval longer than any other factor. Additionally, the following sensitive features may require additional processing time by the appropriate resource agencies: Map ID: 001.HP.13 Slidell Ring Levee; Coastal Protection and Restoration Authority (CPRA); contact Mandy Green @ 225-342-1357 or [mandy.green@la.gov](mailto:mandy.green@la.gov) for more information.

Should you desire additional consultation with our office prior to submitting a formal application, we recommend that you call and schedule a pre-application meeting with our Permit Section staff. Such a preliminary meeting may be helpful, especially if a permit application that is as complete as possible

is presented for evaluation at the pre-application meeting.

If you have any questions, would like to request an application packet or would like to schedule a pre-application meeting, please contact Sharon Mccarthy at (225) 342-6140 or Sharon.McCarthy@la.gov.

Sincerely,

A handwritten signature in black ink that reads "Karl L. Morgan". The signature is written in a cursive style with a long, sweeping horizontal line extending from the end of the name.

Karl L. Morgan  
Administrator

**Karl L. Morgan/sm**

Attachments



**Final Plats:**

1) [P20130672](#)      [Final Plats](#)      [05/07/2013](#)

cc: Jessica Diez, OCM w/plats  
Tim Killeen, CMD/FI w/plats  
Saint Tammany Parish w/plats  
LA DOTD w/plats

**BOBBY JINDAL**  
GOVERNOR



**STEPHEN CHUSTZ**  
INTERIM SECRETARY

**State of Louisiana**  
**DEPARTMENT OF NATURAL RESOURCES**  
**OFFICE OF COASTAL MANAGEMENT**

05/09/2013

ARCADIS U.S., INC.  
10352 PLAZA AMERICANA DRIVE  
BATON ROUGE, LA 70816  
Attn: Elizabeth Beam

**RE: P20130672, Solicitation of Views  
LA DOTD**

**Description:** US 11 Norfolk Southern Railroad Project (State Project No. H.000688.2) includes replacement of the bridge over the Norfolk Southern Railroad and lane widening from two lanes to four lanes

**Location:** Lat 30° 18' 30"N / Long 89° 46' 20"W; US Hwy 11 from I-12 south to US Hwy 190 in Slidell.

**Saint Tammany Parish, LA**

Dear Elizabeth Beam:

You are hereby advised that your application for a Coastal Use Permit (CUP) has been determined to be complete and review by the State for compliance with the Louisiana Coastal Resource Program (LCRP) and consistency with the federal Coastal Zone Management Act (CZMA) has begun. Additionally, it has been determined that your proposed activity is a use of state concern in accordance with Louisiana Revised Statute 49:214.5.

All correspondence and calls regarding this application should reference the Coastal Use Permit Number (P#) indicated above. Please note that all information concerning your application is in our database and is updated throughout the day as changes to the status of the application occur.

Your application can be found on our [Webpage](#).

Should you have any questions, please check the online database or contact the assigned permit analyst: Sharon McCarthy at (225) 342-6140 or Sharon.McCarthy@la.gov. Be sure to reference the above Coastal Use Permit Number.

A handwritten signature in black ink, appearing to read "Chris Meltz", with a long horizontal flourish extending to the right.

Permit Coordinator

CM

cc: Martin Mayer, COE

LA DOTD

## Beam, Elizabeth

---

**From:** Office of Coastal Management <bpel.mail@la.gov>  
**Sent:** Tuesday, May 07, 2013 3:49 PM  
**To:** LEAANN.BAKER@LA.GOV; Beam, Elizabeth  
**Subject:** P20130672 - Joint Permit Application Received



---

PLEASE DO NOT REPLY TO THIS EMAIL. THIS ACCOUNT IS NOT MONITORED FOR INCOMING MAIL. If you would like to talk to someone about this application please call 225-342-7591 or 1-800-267-4019.

### Coastal Use Permit Application Information

Applicant: LA DOTD  
Project: US 11 Norfolk Southern Railroad Environmental Assessment Route US 11  
Project Parish(es): SAINT TAMMANY

OCM Analyst:  
Preliminary Determination:  
Application Modification: N/A

Thank you for using Office of Coastal Management's on-line application process. Your application has been received and has been assigned the following number: **P20130672**

You will be contacted within 5 business days regarding the status of your application. You may also follow the progress of your application on-line at:

[Item Tracking \(text\)](#)  
[Application](#)  
[Make Comments](#)  
[Application Invoice](#)



## Beam, Elizabeth

---

**From:** Christine Charrier <Christine.Charrier@LA.GOV>  
**Sent:** Tuesday, May 07, 2013 3:12 PM  
**To:** Beam, Elizabeth  
**Subject:** RE: US 11 NS RR Bridge EA (H000688): Solicitation of Views

Thank you Ms. Beam for including us early in the process. We will review the information and provide a response by the deadline.

*Christine Charrier*  
OCM/PMD – Permits Program Manager  
614 N. Third Street, Suite 1078  
Baton Rouge, LA 70804  
Ph: 225-342-7953  
Fax: 225-342-6760  
Email: [christine.charrier@la.gov](mailto:christine.charrier@la.gov)  
Webpage: [www.dnr.louisiana.gov](http://www.dnr.louisiana.gov)

---

**From:** Beam, Elizabeth [<mailto:Elizabeth.Beam@arcadis-us.com>]  
**Sent:** Monday, May 06, 2013 11:58 AM  
**To:** Christine Charrier  
**Subject:** US 11 NS RR Bridge EA (H000688): Solicitation of Views

Ms. Charrier:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns. In addition to identifying any concerns or issues as mentioned, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested.

A project overview, project study area map, and figures are attached for your review.

On behalf of LADOTD, ARCADIS U.S., Inc. (ARCADIS) requests that you review the attached information and furnish us with your views and comments by Monday, June 3, 2013. Replies should be sent to Beth Beam by e-mail or by U.S. mail at the addresses provided. Please reference State Project No. H.000688 in your reply.

Thank you.

**Beth Beam MS, AICP** | Senior Planner/Scientist | [elizabeth.beam@arcadis-us.com](mailto:elizabeth.beam@arcadis-us.com)  
ARCADIS U.S., Inc. | 10352 Plaza Americana Drive | Baton Rouge, LA 70816  
T 225.292.1004 | M 225.335.0134 | F 225.218.9677  
[www.arcadis-us.com](http://www.arcadis-us.com)

ARCADIS, Imagine the result

Please consider the environment before printing this email.

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**State of Louisiana**  
Department of Health and Hospitals  
Office of Public Health

May 30, 2013

Elizabeth Beam  
ARCADIS U.S., Inc.  
10352 Plaza Americana Drive  
Baton Rouge, LA 70816

Re: Solicitation of Views and Initiation of Section 106 Consultation US 11 Norfolk Southern  
Railroad Environmental Assessment, Route US 11, St. Tammany Parish, Louisiana, State Project  
No. H000688.2, F.A.P No. H000688.

This office is in receipt of a Solicitation of Views regarding the above referenced project(s).

Based upon the information received from your office we have no objection to the referenced project(s) at this time. The applicant shall be aware of and comply with any and all applicable Louisiana State Sanitary Code regulations (LAC 51, as applicable). Furthermore, should additional project data become available to this office that in any way amend the information upon which this office's response has been based, we reserve the right of additional comments on the referenced project(s).

In the event of any future discovery of evidence of non-compliance with the Louisiana Administrative Code Title 51 (Public Health-Sanitary Code) and the Title 48 (Public Health-General) regulations or any applicable public health laws or statutes which may have escaped our awareness during the course of this cursory review, please be advised that this office's preliminary determination on this Solicitation of View of the project(s) shall not be construed as absolving the applicant of responsibility, if any, with respect to compliance with the Louisiana Administrative Code Title 51 (Public Health-Sanitary Code) and the Title 48 (Public Health-General) regulations or any other applicable public health laws or statutes.

Sincerely,

A handwritten signature in cursive script that reads "Yuanda Zhu".

Yuanda Zhu  
Louisiana Department of Health and Hospitals, Office of Public Health  
Engineering Services  
Telephone: (225) 342-7432  
Electronic mail: [yuanda.zhu@la.gov](mailto:yuanda.zhu@la.gov)

**Beam, Elizabeth**

---

**From:** Tim Mathison <tmathison@cityofslidell.org>  
**Sent:** Tuesday, May 28, 2013 5:51 PM  
**To:** Beam, Elizabeth  
**Cc:** Freddy D. Drennan; Tim Mathison  
**Subject:** LA003143.0000

**Follow Up Flag:** FollowUp  
**Flag Status:** Flagged

**Dear Ms. Beam,**

**On behalf of Slidell Mayor Freddy Drennan I am responding to your letter dated May 3, 2013, relative to the LADOTD State Project No. H.000688.2, F.A.P. No. H000688.**

**Our comments/concerns are as follows:**

- (1) The storage capacity of the large detention pond adjacent to Hwy. 11 should not be reduced by the widening of the highway;**
- (2) The design consultants coordinate with the City of Slidell, St. Tammany Parish and the U.S. Army Corps of Engineers so as to ensure compatibility of the project with various drainage projects in the area;**
- (3) Maintain or improve current drainage capacities along the entire project area;**
- (4) Minimize the impact the widening of the highway would have on adjacent businesses;**
- (5) An at grade railroad crossing should not be considered due to safety concerns;**
- (6) A No Build Alternative would not be acceptable.**

**In the event we think of any other comments or concerns we will present them to you prior to the June 3<sup>rd</sup> deadline.**

**Thank you.**

**Tim Mathison, Esq.**

**Chief Administrative Officer**

**City of Slidell**

**2045 Second Street, Ste. 304**

**Slidell, Louisiana 70458**

**985-646-4330**

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## Beam, Elizabeth

---

**From:** Hauck, Allison <ahauck@ldaf.state.la.us>  
**Sent:** Tuesday, May 21, 2013 3:24 PM  
**To:** Beam, Elizabeth  
**Subject:** Letter from Brad Spicer  
**Attachments:** No comment letter example - Brad.doc

May 21, 2013

Ms. Beth Beam, ARCADIS  
10352 Plaza Americana Drive  
Baton Rouge, Louisiana 70816

RE: Solicitation of Views and  
Initiation of Section 106 Consultation  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana  
State Project No. H. 000688.2  
F.A.P. No. H000688

Dear Ms. Beam:

This office has no comment or objection to this project.

Sincerely,

Bradley E. Spicer  
Assistant Commissioner  
Louisiana Depart of Ag & Forestry  
Office of Soil & Water Conservation

BES:ah



Mr. Brad Rieck  
Deputy Supervisor  
U.S. Fish & Wildlife Service  
Lafayette Field Office  
646 Cajundome Boulevard, Suite 400  
Lafayette, LA 70506

ARCADIS U.S., Inc.  
10352 Plaza Americana Drive  
Baton Rouge  
Louisiana 70816  
Tel 225 292 1004  
Fax 225 218 9677  
www.arcadis-us.com

Subject:

Solicitation of Views and  
Initiation of Section 106 Consultation  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana  
State Project No. H.000688.2  
F.A.P No. H000688

This project has been reviewed for effects to Federal trust resources  
under our jurisdiction and currently protected by the Endangered  
Species Act of 1973 (Act). The project, as proposed,  
( ) Will have no effect on those resources  
( ) is not likely to adversely affect those resources. RCW  
This finding fulfills the requirements under Section 7(a)(2) of the Act.

INFRASTRUCTURE

*Karin Boitell*  
Acting Supervisor  
Louisiana Field Office  
U.S. Fish and Wildlife Service

*6/13/13*  
Date

Date:  
3 May 2013

Dear Mr. Rieck:

Contact:  
Beth Beam

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns and reach agreeable decisions while taking into account the interests of all parties.

Extension:  
215

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

Email:  
elizabeth.beam@  
arcadis-us.com

A Stage 0 Feasibility Study for this project was completed in 2008. A project summary and figures are attached for your review. If you would like to review the Stage 0 Study in its entirety, please request a copy from Beth Beam by e-mail at [elizabeth.beam@arcadis-us.com](mailto:elizabeth.beam@arcadis-us.com) or by U.S. mail to Ms. Beth Beam, ARCADIS, 10352 Plaza Americana Drive, Baton Rouge, Louisiana 70816. A project overview, project study area map, and figures are attached for your review.

Our ref:  
LA003143.0000  
LDOTD/3143/C/1/kp

We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

Imagine the result



Mr. Brad Rieck  
3 May 2013

**Closing**

On behalf of LADOTD, ARCADIS U.S., Inc. (ARCADIS) requests that you review the attached information and furnish us with your views and comments by **Monday, June 3, 2013**. Replies should be sent to Beth Beam by e-mail or by U.S. mail at the addresses provided. Please reference State Project No. H.000688.2 in your reply.

Sincerely,

ARCADIS U.S., Inc.

A handwritten signature in black ink that reads "Elizabeth Beam".

Elizabeth Beam, AICP  
Senior Planner/Scientist

Attachments



**DEPARTMENT OF THE ARMY**  
NEW ORLEANS DISTRICT, CORPS OF ENGINEERS  
P. O. BOX 60267  
NEW ORLEANS, LOUISIANA 70160-0267

**RECEIVED**  
JUL 05 2013

BY: .....

**JUL 03 2013**

REPLY TO  
ATTENTION OF

Operations Division  
Operations Manager,  
Completed Works

Elizabeth Beam, AICP  
Senior Planner/ Scientist  
ARCADIS U.S., Inc.  
10352 Plaza Americana Drive  
Baton Rouge, LA 70816

Dear Ms. Beam:

This is in response to your Solicitation of Views request dated May 3, 2013, concerning the Route US-11 widening and bridge replacement at Slidell in St. Tammany Parish, Louisiana.

We have reviewed your request for potential Department of the Army regulatory requirements and impacts on any Department of the Army projects.

We do not anticipate any adverse impacts to any Corps of Engineers projects.

Based on the limited information provided, we have determined that information and signatures obtained from recent maps, aerial photography, and local soil surveys concerning the proposed project are indicative of the occurrence of waters of the United States, including wetlands. Department of the Army (DA) permits are required prior to the deposition and/or redistribution of dredged or fill material into jurisdictional waters and wetlands.

This preliminary determination is advisory in nature. If an approved delineation is needed, please furnish us with the detailed field data concerning vegetations, soils, and hydrology that we require for all jurisdictional decisions. The fact that a field wetland delineation/determination has not been completed does not alleviate your responsibility to obtain the proper DA permits prior to working in jurisdictional wetlands or waters occurring on this property.

Please be advised that this property is in the Louisiana Coastal Zone. For additional information regarding coastal use permit requirements, contact Ms. Christine Charrier, Office of Coastal Management, Louisiana Department of Natural Resources at (225) 342 7953.

Off-site locations of activities such as borrow, disposals, haul-and detour-roads and work mobilization site developments may be subject to Department of the Army regulatory



requirements and may have an impact on a Department of the Army project.

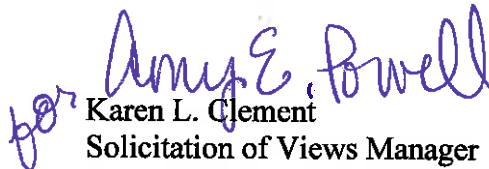
You should apply for said permit well in advance of the work to be performed. The application should include sufficiently detailed maps, drawings, photographs, and descriptive text for accurate evaluation of the proposal.

Please contact Mr. Robert Heffner, of our Regulatory Branch by telephone at (504) 862-1288, or by e-mail at [Robert.A.Heffner@usace.army.mil](mailto:Robert.A.Heffner@usace.army.mil) for questions concerning wetlands determinations or need for on-site evaluations. Questions concerning regulatory permit requirements may be addressed to Mr. Michael Farabee by telephone at (504) 862-2292 or by email at [Michael.V.Farabee@usace.army.mil](mailto:Michael.V.Farabee@usace.army.mil).

Future correspondence concerning this matter should reference our account number MVN-2013-01421-SA. This will allow us to more easily locate records of previous correspondence, and thus provide a quicker response.

We apologize for missing the target date of June 3, 2013 listed in your request. Thank you for your patience in this matter.

Sincerely,

  
for Karen L. Clement  
Solicitation of Views Manager

Copy Furnished:

Ms. Christine Charrier  
Coastal Zone Management  
Department of Natural Resources  
Post Office Box 44487  
Baton Rouge, Louisiana 70804-4487

## Beam, Elizabeth

---

**From:** Beth Altazan-Dixon <Beth.Dixon@LA.GOV>  
**Sent:** Friday, May 10, 2013 10:23 AM  
**To:** Beam, Elizabeth  
**Subject:** DEQ SOV 130503/0800US 11 Norfolk Southern Railroad EA-Route US 11

May 10, 2013

Elizabeth Beam, Senior Planner/Scientist  
ARCADIS U.S., Inc.  
10352 Plaza Americana Drive  
Baton Rouge, LA 70816  
[Elizabeth.Beam@arcadis-us.com](mailto:Elizabeth.Beam@arcadis-us.com)

RE: 130503/0800 US 11 Norfolk Southern Railroad EA-Route US 11  
H.000688.2 LADOTD Funding  
St. Tammany Parish

Dear Ms. Beam:

The Department of Environmental Quality (LDEQ), Business and Community Outreach Division has received your request for comments on the above referenced project.

After reviewing your request, the Department has no objections based on the information provided in your submittal. However, for your information, the following general comments have been included. Please be advised that if you should encounter a problem during the implementation of this project, you should immediately notify LDEQ's Single-Point-of-contact (SPOC) at (225) 219-3640.

- Please take any necessary steps to obtain and/or update all necessary approvals and environmental permits regarding this proposed project.
- If your project results in a discharge to waters of the state, submittal of a Louisiana Pollutant Discharge Elimination System (LPDES) application may be necessary.
- If the project results in a discharge of wastewater to an existing wastewater treatment system, that wastewater treatment system may need to modify its LPDES permit before accepting the additional wastewater.
- All precautions should be observed to control nonpoint source pollution from construction activities. LDEQ has stormwater general permits for construction areas equal to or greater than one acre. It is recommended that you contact the LDEQ Water Permits Division at (225) 219-9371 to determine if your proposed project requires a permit.
- If your project will include a sanitary wastewater treatment facility, a Sewage Sludge and Biosolids Use or Disposal Permit application or Notice of Intent must be submitted no later than January 1, 2013. Additional information may be obtained on the LDEQ website at <http://www.deq.louisiana.gov/portal/tabid/2296/Default.aspx> or by contacting the LDEQ Water Permits Division at (225) 219- 9371.
- If any of the proposed work is located in wetlands or other areas subject to the jurisdiction of the U.S. Army Corps of Engineers, you should contact the Corps directly regarding permitting issues. If a Corps permit is required, part of the application process may involve a water quality certification from LDEQ.
- All precautions should be observed to protect the groundwater of the region.
- Please be advised that water softeners generate wastewaters that may require special limitations depending on local water quality considerations. Therefore if your water system improvements include water softeners, you are advised to contact the LDEQ Water Permits to determine if special water quality-based limitations will be necessary.
- Any renovation or remodeling must comply with LAC 33:III.Chapter 28, Lead-Based Paint Activities; LAC 33:III.Chapter 27, Asbestos-Containing Materials in Schools and State Buildings (includes all training and accreditation); and LAC 33:III.5151, Emission Standard for Asbestos for any renovations or demolitions.
- If any solid or hazardous wastes, or soils and/or groundwater contaminated with hazardous constituents are encountered during the project, notification to LDEQ's Single-Point-of-Contact (SPOC) at (225) 219-3640 is required. Additionally, precautions should be taken to protect workers from these hazardous constituents.

**Currently, St. Tammany Parish is classified as attainment with the National Ambient Air Quality Standards and has no general conformity determination obligations.**

Please send all future requests to my attention. If you have any questions, please feel free to contact me at (225) 219-3958 or by email at [beth.dixon@la.gov](mailto:beth.dixon@la.gov).

Sincerely,

A handwritten signature in blue ink that reads "Beth".

Beth Altazan-Dixon, EPS III  
Performance Management  
LDEQ/Office of the Secretary  
Business and Community Outreach and Incentives Division  
P.O. Box 4301 (602 N. 5th Street)  
Baton Rouge, LA 70821-4301  
Phone: 225-219-3955  
Fax: 225-325-8148  
Email: [beth.dixon@la.gov](mailto:beth.dixon@la.gov)



# APPENDIX C-2

## Solicitation of Views Tribes

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**Environmental Section**  
P.O. Box 94245 | Baton Rouge, LA 70804-9245  
phone: 225-242-4502 | fax: 225-242-4500

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

May 8, 2013

Mr. Bryant J. Celestine  
Historic Preservation Officer  
Alabama Coushatta Tribe of Texas  
571 State Park Road 56  
Livingston, TX 77351

Subject:  
State Project No. H.000688.2  
F.A.P No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Mr. Celestine:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Attachments  
cc: Scott Nelson (FHWA)



**Environmental Section**  
P.O. Box 94245 | Baton Rouge, LA 70804-9245  
phone: 225-242-4502 | fax: 225-242-4500

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

May 8, 2013

Mr. Earl J. Barbry, Jr.  
THPO  
Tunica-Biloxi Indians of Louisiana  
Attn: Museum Division Offices  
P.O. Box 1589  
Marksville, LA 71351

Subject:  
State Project No. H.000688.2  
F.A.P No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Mr. Barbry:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Attachments  
cc: Scott Nelson (FHWA)



**Environmental Section**  
P.O. Box 94245 | Baton Rouge, LA 70804-9245  
phone: 225-242-4502 | fax: 225-242-4500

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

May 8, 2013

Ms. Natalie Harjo  
Historic Preservation Officer  
Seminole Nation of Oklahoma  
P.O. Box 1498  
Wewoka, OK 74884

Subject:  
State Project No. H.000688.2  
F.A.P No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Ms. Harjo:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Attachments  
cc: Scott Nelson (FHWA)



**Environmental Section**  
P.O. Box 94245 | Baton Rouge, LA 70804-9245  
phone: 225-242-4502 | fax: 225-242-4500

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

May 8, 2013

Ms. Dana Masters  
THPO  
Jena Band of Choctaws  
P.O. Box 14  
Jena, LA 71324

Subject:  
State Project No. H.000688.2  
F.A.P No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Ms. Masters:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Attachments  
cc: Scott Nelson (FHWA)



**Environmental Section**  
P.O. Box 94245 | Baton Rouge, LA 70804-9245  
phone: 225-242-4502 | fax: 225-242-4500

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

May 8, 2013

Dr. Ian Thompson  
THPO  
Choctaw Nation of Oklahoma  
P.O. Box 1210  
Durant, OK 74702

Subject:  
State Project No. H.000688.2  
F.A.P No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Dr. Thompson:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

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On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Attachments  
cc: Scott Nelson (FHWA)





**Environmental Section**  
P.O. Box 94245 | Baton Rouge, LA 70804-9245  
phone: 225-242-4502 | fax: 225-242-4500

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

May 8, 2013

Ms. Jean Ann Lambert  
THPO  
Quapaw Tribe of Oklahoma  
5681 S. 630 Road  
Quapaw, OK 74363

Subject:  
State Project No. H.000688.2  
F.A.P No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Ms. Lambert:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Attachments  
cc: Scott Nelson (FHWA)



**Environmental Section**  
P.O. Box 94245 | Baton Rouge, LA 70804-9245  
phone: 225-242-4502 | fax: 225-242-4500

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

May 8, 2013

Mr. Kenneth Carleton  
Archaeologist & THPO  
Mississippi Band of Choctaw Indians  
P.O. Box 6257  
Philadelphia, MS 39350

Subject:  
State Project No. H.000688.2  
F.A.P No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Mr. Carleton:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

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A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Attachments  
cc: Scott Nelson (FHWA)



**Environmental Section**  
P.O. Box 94245 | Baton Rouge, LA 70804-9245  
phone: 225-242-4502 | fax: 225-242-4500

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

May 8, 2013

Ms. Kimberly Walden  
Cultural Director  
Chitimacha Tribe of Louisiana  
P.O. Box 661  
Clarenton, LA 70523

Subject:  
State Project No. H.000688.2  
F.A.P No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Ms. Walden:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

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On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Attachments  
cc: Scott Nelson (FHWA)



**Environmental Section**  
P.O. Box 94245 | Baton Rouge, LA 70804-9245  
phone: 225-242-4502 | fax: 225-242-4500

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

May 8, 2013

Dr. Linda Langley  
Cultural Preservation Officer  
Coushatta Tribe of Louisiana  
P.O. Box 818  
Elton, LA 70532

Subject:  
State Project No. H.000688.2  
F.A.P No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Dr. Langley:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

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On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Attachments  
cc: Scott Nelson (FHWA)



**Environmental Section**  
P.O. Box 94245 | Baton Rouge, LA 70804-9245  
phone: 225-242-4502 | fax: 225-242-4500

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

May 8, 2013

Mr. Robert Cast  
THPO  
Caddo Nation  
P.O. Box 487  
Binger, OK 73009

Subject:  
State Project No. H.000688.2  
F.A.P No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Mr. Cast:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

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On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Attachments  
cc: Scott Nelson (FHWA)





**Environmental Section**  
P.O. Box 94245 | Baton Rouge, LA 70804-9245  
phone: 225-242-4502 | fax: 225-242-4500

**Bobby Jindal**, Governor  
**Sherri H. LeBas**, P.E., Secretary

May 8, 2013

Mr. Paul Bachouse  
THPO  
Seminole Tribe of Florida  
30290 Josie Billie Highway, PMB 1004  
Clewiston, FL 33440

Subject:  
State Project No. H.000688.2  
F.A.P No. H000688  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana

Re: Solicitation of Views and Initiation of Section 106 Consultation

Dear Mr. Bachouse:

Early in the planning process for a transportation facility, views from federal, state, and local agencies, organizations, and individuals are solicited. The special expertise of these groups can assist the Louisiana Department of Transportation and Development (LADOTD) with the identification of possible adverse economic, social, or environmental effects from the project or other related concerns.

In addition to identifying any concerns or issues as mentioned above, your consultation to address cultural and historic resource issues pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800) is also requested. Earth Search, Inc., will be conducting the cultural resources survey for the proposed project. If you know of another interested party or potential Section 106 concerns, please advise us to that information as well.

A project overview, project study area map, and figures are attached for your review. We would also like to advise you that a stakeholders/elected officials meeting will be held in Slidell, Louisiana, in Summer 2013 followed by a public meeting on the same day. Specific information regarding these meetings will be provided soon. We ask that your agency or organization provide comments regarding this preliminary information.

On behalf of the Federal Highway Administration (FHWA), we respectfully request that you review the attached information and furnish us with your views and comments by **Friday, June 10, 2013**. If you have any questions please contact Robert Lott at 225-242-4504.

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Attachments  
cc: Scott Nelson (FHWA)



**Solicitation of Views  
US 11 Norfolk Southern Railroad  
Environmental Assessment  
Route US 11  
St. Tammany Parish, Louisiana  
State Project No. H.000688.2  
F.A.P No. H000688**

**PROJECT OVERVIEW**

**Description of the Project.** The US 11 Norfolk Southern Railroad project includes replacement of the bridge over the Norfolk Southern Railroad and lane widening from two lanes to four lanes for the segment of US Highway 11 (US 11) from Interstate 12 (I-12) south to US Highway 190 (US 190; Gause Boulevard) in Slidell, St. Tammany Parish, Louisiana. The logical termini for the proposed project are US 11 at I-12 and US 11 at US 190. The project consists of providing all necessary services required to prepare an Environmental Assessment (EA) in accordance with the National Environmental Policy Act, as amended, and the Federal Highway Administration's regulations and guidelines.

**Purpose and Need for the Project.** The purpose of the proposed project is to replace a structurally deficient bridge, while increasing capacity and alleviating congestion.

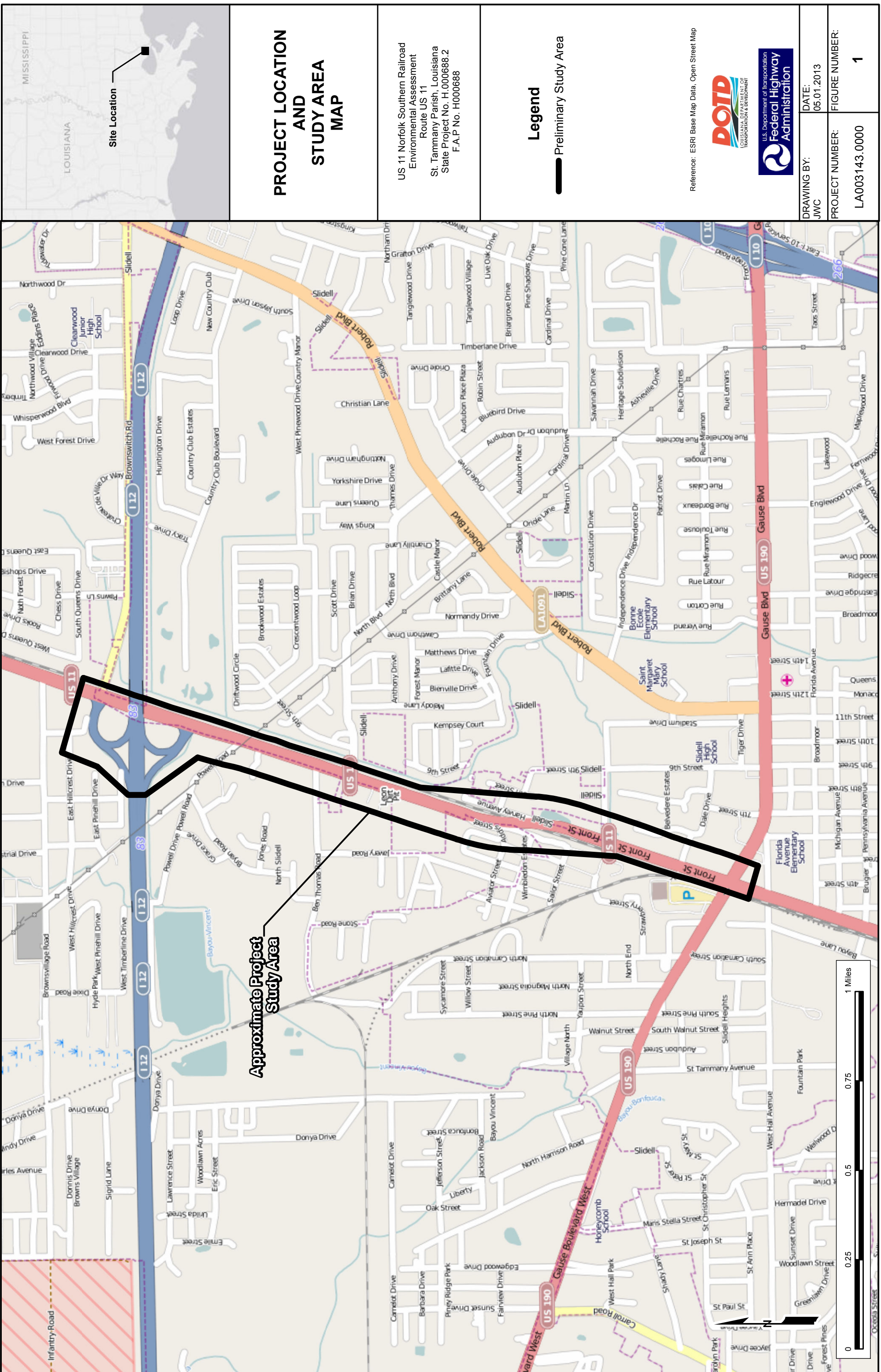
**Project Study Area.** The project area is located to the east of Airport Road and west of Robert Boulevard (LA 1091) in Slidell, Louisiana. The project study area extends south along US 11 from I-12 to the intersection of US 11 at US 190. The EA will investigate the potential for effects to cultural resources, threatened and endangered species, natural resources, and the human environment within the study area. The proposed project would improve existing roadway infrastructure and will require additional right-of-way. A location map that illustrates the project study area is attached (Figure 1).

**Build and No Build Alternatives.** The bridge over the Norfolk Southern Railroad, constructed in 1937, is a two-lane concrete/steel bridge with no shoulders. The bridge spans 828 feet and is 28 feet in width. The most recent LADOTD structure inventory and appraisal (March 2010) and bridge inspection report (May 2009) identified the bridge as structurally deficient noting poor condition of the substructure and superstructure; severe collision damage to the rails at multiple locations; spalling; and corrosion with section loss.

Currently, one build alternative will be considered for evaluation in the EA. This alternative includes roadway widening from the two lanes to four lanes with median and shoulders. This widening will require the replacement of the existing bridge over the railroad (Figures 2-a, 2-b). Up to two additional alternatives will be developed for evaluation in the EA. The No Build Alternative, which assumes that this project will not be built, will also be considered.

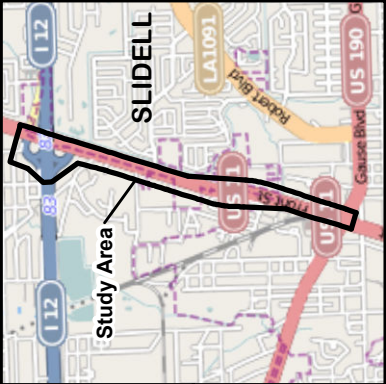
**Recent US 11 Corridor Improvements.** Several improvements have been recently completed within the US 11 corridor. Within the project study area, these improvements include the addition of left-turn lanes on US 11 north and southbound at the east and westbound approaches to I-12; additional turn lanes on the eastbound and westbound I-12 ramps; addition of a right-turn lane along northbound US 11 to Brown Switch Road; and the addition of a right-turn lane from I-12 south along US 11 to Powell Drive.





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US 11 Norfolk Southern Railroad  
Environmental Assessment  
State Project No. H-000688.2  
E.A.P No. H000688  
Route US 11  
St. Tammany Parish, Louisiana

CORRIDOR PLATES

Legend

--- Preliminary Study Area



Reference: ESRI Base Maps & Imagery



Date: 04/23/2013  
Project Number: LA003413.0000

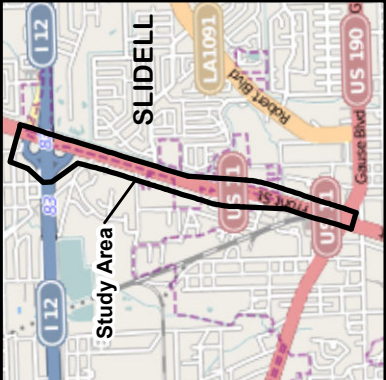
Figure No.:





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US 11 Norfolk Southern Railroad  
Environmental Assessment  
State Project No. H-000688.2  
E.A.P No. H000688  
Route US 11  
St. Tammany Parish, Louisiana

CORRIDOR PLATES

Legend

--- Preliminary Study Area



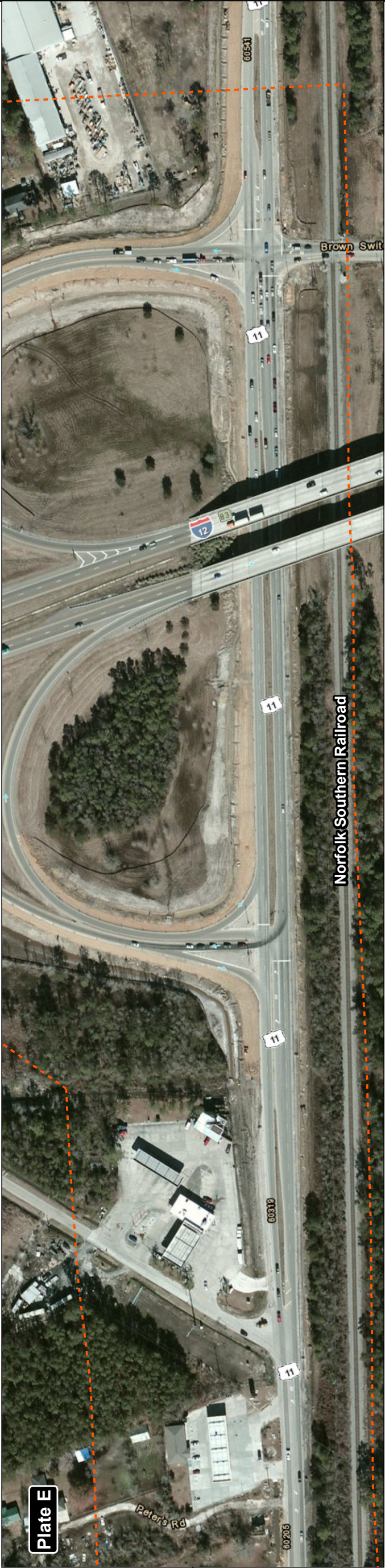
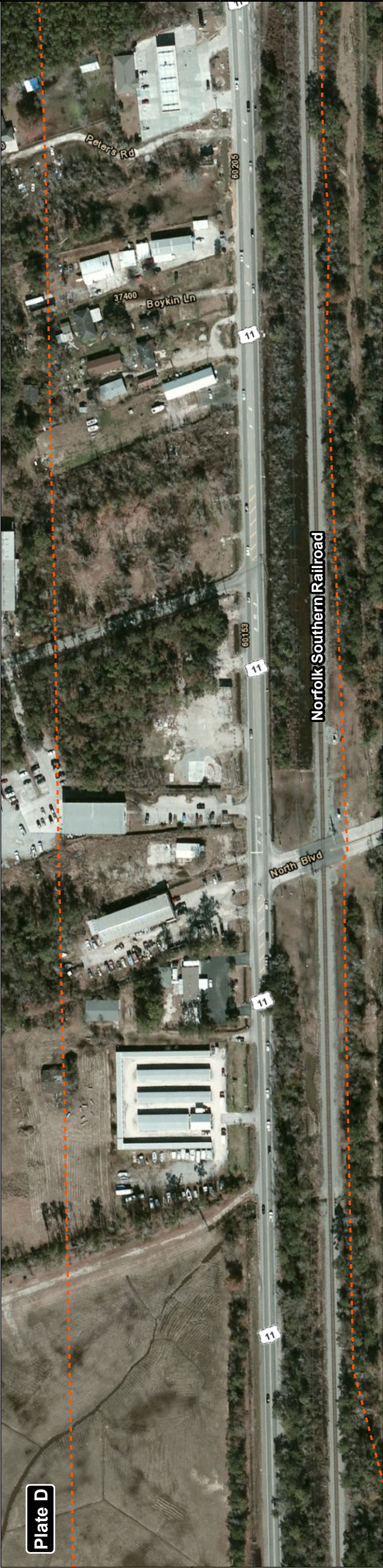
Reference: ESRI Base Maps & Imagery



Date: 04/23/2013  
Project Number: LA003413.0000

Figure No.:

2 - b





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US 11 Norfolk Southern Railroad  
SOV Mailing List  
Environmental Assessment  
H.000688.2

Cat 1	Cat 2	Salutation	Courtesy	First	M	Last Name	Title	Org 1	Org 2	Address	City	State	Zip
Federal	T	Mr. Celestine	Mr.	Bryant	J.	Celestine	Historic Preservation Officer	Alabama Coushatta Tribe of Texas		571 State Park Road 56	Livingston	TX	77351
Federal	T	Mr. Barbry	Mr.	Earl	J.	Barbry, Jr.	THPO	Tunica-Biloxi Indians of Louisiana	Attn: Museum Division Offices	P.O. Box 1589	Marksville	LA	71351
Federal	T	Ms. Harjo	Ms.	Natalie		Harjo	Historic Preservation Officer	Seminole Nation of Oklahoma		P.O. Box 1498	Wewoka	OK	74884
Federal	T	Ms. Masters	Ms.	Dana		Masters	THPO	Jena Band of Choctaws		P.O. Box 14	Jena	LA	71324
Federal	T	Dr. Thompson	Dr.	Ian		Thompson	THPO	Choctaw Nation of Oklahoma		P.O. Box 1210	Durant	OK	74702
Federal	T	Ms. Lambert	Ms.	Jean Ann		Lambert	THPO	Quapaw Tribe of Oklahoma		5681 S. 630 Road	Quapaw	OK	74363
Federal	T	Mr. Carleton	Mr.	Kenneth		Carleton	Archaeologist & THPO	Mississippi Band of Choctaw Indians		P.O. Box 6257	Philadelphia	MS	39350
Federal	T	Ms. Walden	Ms.	Kimberly		Walden	Cultural Director	Chitimacha Tribe of Louisiana		P.O. Box 661	Clarenton	LA	70523
Federal	T	Dr. Langley	Dr.	Linda		Langley	Cultural Preservation Officer	Coushatta Tribe of Louisiana		P.O. Box 818	Elton	LA	70532
Federal	T	Mr. Cast	Mr.	Robert		Cast	THPO	Caddo Nation		P.O. Box 487	Binger	OK	73009
Federal	T	Mr. Bachouse	Mr.	Paul		Bachouse	THPO	Seminole Tribe of Florida		30290 Josie Billie Highway, PMB 1004	Clewiston	FL	33440

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## Beam, Elizabeth

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**From:** Robert Lott <Robert.Lott@LA.GOV>  
**Sent:** Thursday, November 20, 2014 1:33 PM  
**To:** Beam, Elizabeth  
**Subject:** FW: H.000688 US 11 Norfolk Southern Railroad, St. Tammany Parish

FYI

Thanks,  
Bobby Lott  
LADOTD – Environmental Section  
225.242.4504

---

**From:** Michelle Hanks  
**Sent:** Thursday, November 20, 2014 9:14 AM  
**To:** Robert Lott  
**Subject:** FW: H.000688 US 11 Norfolk Southern Railroad, St. Tammany Parish

Bobby,

This should be added to your records and documentation.

Michelle

---

**From:** Lindsey Bilyeu [<mailto:lbilyeu@choctawnation.com>]  
**Sent:** Monday, October 06, 2014 1:55 PM  
**To:** Michelle Hanks  
**Subject:** RE: H.000688 US 11 Norfolk Southern Railroad, St. Tammany Parish

Ms. Hanks,

The Choctaw Nation of Oklahoma thanks the Louisiana DOTD for the correspondence regarding the above referenced project. St. Tammany Parish, LA lies in the Choctaw Nation of Oklahoma's area of historic interest. The Choctaw Nation is unaware of any cultural or sacred sites located in the immediate project area. Due to the presence of historic sites located in the area and due to the SHPO's concerns, the Choctaw Nation Historic Preservation Department respectfully defers to the Louisiana SHPO. However, as the project lies in an area of historic interest to the Tribe, we ask that work be stopped and our office contacted immediately in the event that Native American cultural material or human remains are encountered. If you have any questions, please contact our office at 580-924-8280 ext. 2631.

Thank You,

Lindsey D. Bilyeu  
Senior Section 106 Reviewer  
Historic Preservation Department  
Choctaw Nation of Oklahoma  
P.O. Box 1210  
Durant, OK 74701  
580-924-8280 ext. 2631

---

**From:** Michelle Hanks [<mailto:Michelle.Hanks@LA.GOV>]  
**Sent:** Monday, September 15, 2014 2:06 PM  
**To:** Lindsey Bilyeu  
**Subject:** FW: H.000688 US 11 Norfolk Southern Railroad, St. Tammany Parish

Lindsey,

My apologies, I intended to include you in the original email this morning. Please let me know if you have any questions.

Thank you,  
Michelle

---

**From:** Michelle Hanks  
**Sent:** Monday, September 15, 2014 11:29 AM  
**To:** 'ithompson@choctawnation.com'  
**Cc:** 'Beam, Elizabeth'; Robert Lott; [robert.mahoney@dot.gov](mailto:robert.mahoney@dot.gov)  
**Subject:** H.000688 US 11 Norfolk Southern Railroad, St. Tammany Parish

Good morning Ian,

Attached are the requested documents and a memo for your records regarding the above captioned project. If you have any questions or concerns, please let me know.

Thank you,

Michelle

Michelle Whipp Hanks, B.A., M.A.  
Environmental Impact Specialist  
La DOTD-Environmental  
(225) 242-4514

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This message is intended only for the use of the individual or entity to which it is addressed and may contain information that is privileged, confidential and exempt from disclosure. If you have received this message in error, you are hereby notified that we do not consent to any reading, dissemination, distribution or copying of this message. If you have received this communication in error, please notify the sender immediately and destroy the transmitted information. Please note that any view or opinions presented in this email are solely those of the author and do not necessarily represent those of the Choctaw Nation.

## Beam, Elizabeth

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**From:** Robert Lott <Robert.Lott@LA.GOV>  
**Sent:** Monday, July 01, 2013 12:58 PM  
**To:** Beam, Elizabeth  
**Subject:** FW: LADOT, H.000688.2, US 11 Norfolk Southern Railroad, St. Tammany Parish, LA

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

FYI

Thanks,  
Bobby Lott  
DOTD - Environmental Section  
225-242-4504

---

**From:** Noel Ardoin  
**Sent:** Monday, July 01, 2013 12:01 PM  
**To:** Robert Lott  
**Subject:** FW: LADOT, H.000688.2, US 11 Norfolk Southern Railroad, St. Tammany Parish, LA

---

**From:** Johnnie L. Jacobs [<mailto:jjacobs@choctawnation.com>]  
**Sent:** Wednesday, June 26, 2013 10:41 AM  
**To:** Noel Ardoin  
**Subject:** LADOT, H.000688.2, US 11 Norfolk Southern Railroad, St. Tammany Parish, LA

Dear Noel,

Thank you for the correspondence regarding the above referenced project. St. Tammany Parish is within the historic area of interest to the Choctaw Nation of Oklahoma. Please forward our office a copy of the SHPO comments once received. If you have any further questions, please let us know.

Thank you,

Ms. Johnnie Jacobs  
NHPA Section 106 Coordinator  
Choctaw Nation of Oklahoma  
Historic Preservation Department  
P.O. Box 1210  
Durant, OK 74701  
[jjacobs@choctawnation.com](mailto:jjacobs@choctawnation.com)

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# APPENDIX C-3

## Section 106

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JAY DARDENNE  
LIEUTENANT GOVERNOR

**State of Louisiana**  
OFFICE OF THE LIEUTENANT GOVERNOR  
DEPARTMENT OF CULTURE, RECREATION & TOURISM  
OFFICE OF CULTURAL DEVELOPMENT

CHARLES R. DAVIS  
DEPUTY SECRETARY

PAM BREAU  
ASSISTANT SECRETARY

9 July 2014

Noel Ardoin  
Environmental Engineer  
Dept of Transportation and Development  
PO Box 94245  
Baton Rouge, LA 70804-9245

Re: Draft Report  
La Division of Archaeology Report No. 22-4693  
*Phase I Cultural Resources Survey for the Environmental Assessment of the US 11 Norfolk Southern Railroad, Route US 11, St. Tammany Parish, Louisiana*

Dear Ms. Ardoin:

We acknowledge receipt your letter dated 5 June 2014 and two copies of the above-referenced report. We have completed our review of this report and offer the following comments.

In reference to historic standing structures, we concur with your evaluation that the US 11 Norfolk Southern Railroad Bridge (52-02393), Old Ice House (52-02387), and 1184 Front Street residential property (52-02398) are eligible for listing in the National Register of Historic Places. However, we are of the opinion that the residential property (52-02398) would be eligible under Criterion C as a Craftsman Style house in the context of the City of Slidell and the Old Ice House (52-02387) would be eligible under Criterion A for its role as the first power station for the City of Slidell. The replacement of the US 11 Norfolk Southern Bridge (52-02393) and the expansion of the required right-of-way upon the Old Ice House (52-02387) and 1184 Front Street residence (52-02398) would constitute an Adverse Effect on historic properties as defined in the Section 106 Regulations (36 CFR 800). As such, we invite you to consult with our office in order to begin the Memorandum of Agreement process.

We concur that the portion of site 16ST255 within the project right-of-way is not eligible for nomination to the National Register. Our office has no further concerns for archaeological resources for this project.

We look forward to receiving two bound copies of the final report along with a pdf of the report. If you have any questions, please contact Chip McGimsey in the Division of Archaeology by email at [cmcgimsey@crt.la.gov](mailto:cmcgimsey@crt.la.gov) or by phone at 225-219-4598.

Sincerely,

Pam Breau  
State Historic Preservation Officer

PB:crm



Environmental Section  
PO Box 94245 | Baton Rouge, LA 70804-9245  
ph: 225-242-4502 | fx: 225-242-4500

Bobby Jindal, Governor  
Sherri H. LeBas, P.E., Secretary

June 5, 2014

STATE PROJECT NO. H.000688  
F.A.P. NO. H000688  
NAME: US 11 NORFOLK SOUTHERN R.R.OVERPASS  
ROUTE: US 11  
STRUCTURE NO.: 62520180400531(Recall No. 059090)  
PARISH: ST. TAMMANY

---

Ms. Pam Breaux  
State Historic Preservation Officer  
Office of Cultural Development  
Louisiana Department of Culture, Recreation and Tourism  
P.O. Box 44247, Capitol Station  
Baton Rouge, LA 70804

SUBJECT: Draft Report: *Phase I Cultural Resources Survey for the Environmental Assessment of the US 11 Norfolk Southern Railroad, Route US 11, St. Tammany Parish, Louisiana*

Dear Ms. Breaux:

Enclosed for your review and comment are two copies of the above-titled draft Cultural Resource Survey report, prepared by Earth Search, Inc., dated April 2014.

The Louisiana Department of Transportation and Development (LADOTD) is proposing a project to replace the existing two-lane bridge over the Norfolk Southern Railroad with a four-lane bridge on US 11 in Slidell, St. Tammany Parish. Structure No. 62520180400531 (Recall No. 059090) is a concrete deck and bents with steel I-beam bridge, built in 1937, and is 828 feet long and 24 feet wide. The project proposes widening the segment of US 11 from I-12, south to US 190 (Gause Boulevard), from two lanes to four lanes. During construction, the traffic will be maintained on the existing bridge. During bridge construction, two lanes of the new bridge will be constructed and traffic will be shifted to the new bridge while the current bridge is removed and the second half of the bridge is constructed. No roadway closures are anticipated along US 11. The project is approximately 1.7 miles in length. Approximately 10.7 acres of additional right-of-way (ROW) will be required.

The Area of Potential Effects (APE) for this proposed project is the limits of the required ROW, 10.7 acres. The study identified all structures greater than 50 years of age within a .25 mile (400 m) buffer on either side of the existing highway centerline. On June 18, 2013 and November 13,

2013, staff from Earth Search, Inc. (ESI) conducted a cultural resources survey to identify historic properties. One archaeological site (16ST255) was identified within the direct APE (see Table 1). It is recommended ineligible for the National Register of Historic Places (NRHP) and no additional archaeological investigations of the site within the direct APE are recommended.

Five standing structures, greater than fifty years of age were identified within the direct APE: 52-02387, 52-02398, 52-02380, 52-02366, and 52-02393 (see Table 1). Two of these structures are recommended eligible, the bridge has been previously determined eligible for listing on the NRHP and the remaining structures are recommended not eligible. Although there were a number of structures greater than 50 years of age identified within the .25 mile buffer the proposed project will have no impact on these properties and therefore no further consultation is requested.

**Table 1: Standing Structures and Archaeological Sites identified within the direct APE.**

<b>Standing Structure No./ Archaeological Site No.</b>	<b>Description</b>	<b>NRHP Eligibility</b>	<b>Effect Determination</b>	<b>Recommendations</b>
52-02387	1398 Front St.: Old Ice House (currently used as restaurant)	Eligible	Adverse Effect	Sec 106 Consultation to seek ways to avoid, minimize, or mitigate Adverse Effect
52-02398	1184 Front St: Abandoned Residence with detached garage	Eligible	Adverse Effect	Sec 106 Consultation to seek ways to avoid, minimize, or mitigate Adverse Effect
52-02380	1104 Front St.: Business Building Slidell Vet Clinic	Not Eligible	N/A	No further work recommendation
52-02366	Highway 11 and Boykin Lane (no address): Abandoned building unknown function	Not Eligible	N/A	No further work recommendation
52-02393	US 11 Norfolk Southern Railroad Bridge Str. No. 62520180400531	Previously Determined Eligible	Adverse Effect	Sec 106 Consultation to seek ways to avoid, minimize, or mitigate Adverse Effect

STATE PROJECT NO: H.000688  
NAME: PHASE I CULTURAL RESOURCES SURVEY US 11  
ST. TAMMANY  
LETTER TO MS BREAUX  
JUNE 5, 2014

Page 3 of 3

16ST255	Remains of Historic Gas Station	Not Eligible	N/A	No further work recommendation
---------	---------------------------------	--------------	-----	--------------------------------

The Old Ice House (52-02387), located at 1398 Front Street, is recommended eligible for the NRHP under Criterion C; the building retains its integrity of form, style, and materials. It is also recommended eligible und Criterion D; it is possible that the building and intact equipment and ice machinery could provide important information regarding early, rural utilities and ice production/storage. This building lies within the direct APE, approximately 16 meters east of the existing pavement edge. It is recommended that the Old Ice House be avoided during construction if possible. If avoidance is not possible, mitigation measures should be taken.

The residential structure (52-02398), located at 1184 Front Street, is recommended eligible for the NRHP under Criterion A as an example of a property that exhibits characteristics of a broad pattern in our history associated with transportation and the shift from horse-drawn carriage to automobile use. The residence has a detached automobile garage and secondary residential apartment above the garage. The structure is also recommended eligible under Criterion C for its integrity of form, style, and material. It is also a locally unique form without substantial modern modifications. The building lies within the direct APE, approximately 10 meters east of the existing pavement edge. It is recommended that the building be avoided during construction if possible. If avoidance is not possible, mitigation measures should be taken.

LADOTD, in conjunction with FHWA, request your concurrence on the eligibility determinations of Structures 52-02387, 52-02398, 52-02380, 52-02366, and site 16ST255 and the proposed recommendations (see Table 1). If you have any questions or comments, contact Michelle Whipp at 225-242-4514 or [michelle.whipp@la.gov](mailto:michelle.whipp@la.gov).

Sincerely,

Noel Ardoin  
Environmental Engineer Administrator

Enclosures  
NA/mw  
cc: SHPO File  
FHWA  
Robert Lott via email: [Robert.lott@la.gov](mailto:Robert.lott@la.gov)





Preserving America's Heritage

September 15, 2014

Mr. Carl Highsmith  
Project Delivery Team Leader  
Federal Highway Administration  
Louisiana Division  
5304 Flanders Drive, Suite A  
Baton Rouge, LA 70808

Ref: *Proposed Replacement of US 11 Bridge over the Norfolk Southern Railroad  
St. Tammany Parish, Louisiana*

Dear Mr. Highsmith:

The Advisory Council on Historic Preservation (ACHP) has received your notification and supporting documentation regarding the adverse effects of the referenced undertaking on a property or properties listed or eligible for listing in the National Register of Historic Places. Based upon the information provided, we have concluded that Appendix A, *Criteria for Council Involvement in Reviewing Individual Section 106 Cases*, of our regulations, "Protection of Historic Properties" (36 CFR Part 800), does not apply to this undertaking. Accordingly, we do not believe that our participation in the consultation to resolve adverse effects is needed. However, if we receive a request for participation from the State Historic Preservation Officer (SHPO), Tribal Historic Preservation Officer (THPO), affected Indian tribe, a consulting party, or other party, we may reconsider this decision. Additionally, should circumstances change, and it is determined that our participation is needed to conclude the consultation process, please notify us.

Pursuant to 36 CFR §800.6(b)(1)(iv), you will need to file the final Memorandum of Agreement (MOA), developed in consultation with the Louisiana State Historic Preservation Office (SHPO), and any other consulting parties, and related documentation with the ACHP at the conclusion of the consultation process. The filing of the MOA, and supporting documentation with the ACHP is required in order to complete the requirements of Section 106 of the National Historic Preservation Act.

Thank you for providing us with the notification of adverse effect. If you have any questions or require further assistance, please contact Mr. Chris Wilson at 202-517- 0229 or via e-mail at [cwilson@achp.gov](mailto:cwilson@achp.gov).

Sincerely,

LaShavio Johnson  
Historic Preservation Technician  
Office of Federal Agency Programs

ADVISORY COUNCIL ON HISTORIC PRESERVATION

401 F Street NW, Suite 308 • Washington, DC 20001-2637  
Phone: 202-517-0200 • Fax: 202-517-6381 • [achp@achp.gov](mailto:achp@achp.gov) • [www.achp.gov](http://www.achp.gov)

## Beam, Elizabeth

---

**From:** Michelle Hanks <Michelle.Hanks@LA.GOV>  
**Sent:** Monday, September 15, 2014 9:27 AM  
**To:** Beam, Elizabeth; Robert Lott  
**Subject:** FW: Proposed Replacement of US 11 Bridge over the Norfolk Southern Railroad, St. Tammany Parish, LA (H000688)  
**Attachments:** la.fhwa.replacement of US 11 bridge over norfolk southern railroad.np.15sept14.pdf

Beth,

I just received this. Another piece completed.

Michelle

---

**From:** [Robert.Mahoney@dot.gov](mailto:Robert.Mahoney@dot.gov) [<mailto:Robert.Mahoney@dot.gov>]  
**Sent:** Monday, September 15, 2014 9:15 AM  
**To:** Michelle Hanks; Michelle Hanks; Noel Ardoin  
**Cc:** [Scott.Nelson@dot.gov](mailto:Scott.Nelson@dot.gov); [Carl.Highsmith@dot.gov](mailto:Carl.Highsmith@dot.gov)  
**Subject:** FW: Proposed Replacement of US 11 Bridge over the Norfolk Southern Railroad, St. Tammany Parish, LA (H000688)

The ACHP will not participate, as of now, in development of the MOA for the subject project.  
A copy of the signed MOA will need to be sent to the ACHP.  
BobM.

---

**From:** OFAP [<mailto:OFAP2@achp.gov>]  
**Sent:** Monday, September 15, 2014 8:23 AM  
**To:** Highsmith, Carl (FHWA)  
**Cc:** Mahoney, Robert (FHWA); Zachary, Denise (FHWA); Naber, MaryAnn (FHWA); Phil Boggan  
**Subject:** Proposed Replacement of US 11 Bridge over the Norfolk Southern Railroad, St. Tammany Parish, LA

From: Office of Federal Agency Programs

Advisory Council on Historic Preservation

Attached is our letter on the subject undertaking (in Adobe Acrobat PDF format)

If you have any questions concerning our letter, please contact:

Chris Wilson  
[cwilson@achp.gov](mailto:cwilson@achp.gov)  
202 517-0229  
Case # 8403

Note: Please do not reply to this email.



# CONTENT ON CD

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CD-1 Traffic Analysis Files

CD-2 Biological Resources and Wetland Findings Report

CD-3 Noise Analysis Technical Report

CD-4 Phase I Environmental Site Assessment Report

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