

AS-DESIGNED RATING		
VEHICLE	RATING FACTOR	NOTES
HL-93 (INV)	1.486	
HL-93 (OPR)	1.927	
LADV-II (INV)	1.143	MAGNIFICATION FACTOR = 1.3

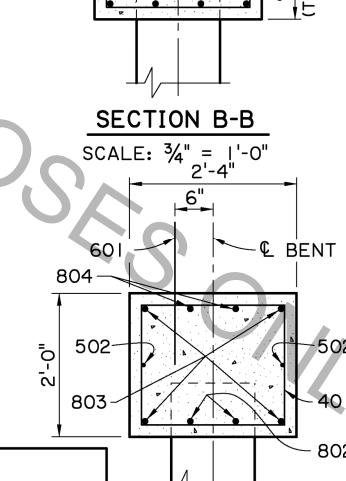
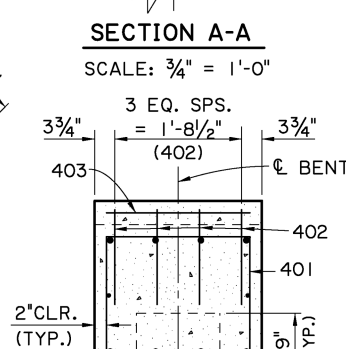
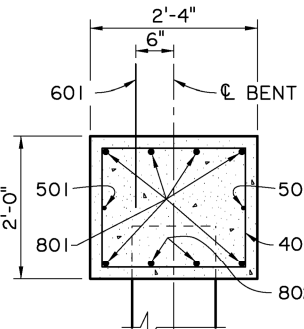
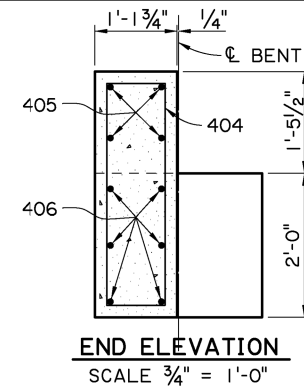
NOTES:

CONSTRUCTION SPECIFICATIONS: LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS 4th EDITION, WITH 2008 & 2009 INTERIMS.

DESIGN LOAD: LIVE LOAD IS HL-93, AND LADV-II (LOUISIANA DESIGN VEHICLE LIVE LOAD 2011).

STRUCTURAL CONCRETE: ALL CONCRETE SHALL BE CLASS A1. EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER UNLESS OTHERWISE NOTED. ALL EXPOSED FACES OF WINGWALLS AND ENDS OF CAPS SHALL RECEIVE A SURFACE FINISH AS PER SUBSECTION 805.08 OF THE STANDARD SPECIFICATIONS, EXCEPT WHEN SPECIFIED ELSEWHERE IN THE PLANS.



ESTIMATED QUANTITIES (ONE INTER. BENT)

BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
801	6	42'-8"	256'-0"	LONGIT. IN CAP
802	8	9'-4"	74'-8"	LONGIT. IN CAP
TOTAL NO. 8 BARS = 330'-8" = 883 LBS.				
601	25	2'-0"	50'-0"	DOWELS
TOTAL NO. 6 BARS = 50'-0" = 75 LBS.				
501	2	42'-8"	85'-4"	LONGIT. IN CAP
TOTAL NO. 5 BARS = 85'-4" = 89 LBS.				
401	56	8'-2"	457'-4"	STIRRUPS IN CAP
402	4	3'-7"	14'-4"	STIRRUPS IN RISER
403	2	2'-9"	5'-6"	LONGIT. IN RISER
TOTAL NO. 4 BARS = 477'-2" = 319 LBS.				
* TOTAL DEFORMED REINFORCING STEEL = 1,366 LBS.				
o CLASS A1 CONCRETE = 7.23 CU. YDS.				
MAX. PILE LOAD: SERVICE DEAD LOAD = 25 TONS				
SERVICE LIVE LOAD = 31 TONS				
FACTORED TOTAL LOAD = 77 TONS				

* ADD 75 LBS. OF REINFORCING STEEL (25-601 DOWELS) WHEN TWO FIXED ENDS OCCUR ON THE SAME BENT.

ESTIMATED QUANTITIES (ONE END BENT)

BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
802	8	9'-4"	74'-8"	LONGIT. IN CAP
803	4	41'-9"	167'-0"	LONGIT. IN CAP
804	2	41'-9"	83'-6"	LONGIT. IN CAP
TOTAL NO. 8 BARS = 325'-2" = 868 LBS.				
601	25	2'-0"	50'-0"	DOWELS
TOTAL NO. 6 BARS = 50'-0" = 75 LBS.				
502	2	41'-9"	83'-6"	LONGIT. IN CAP
TOTAL NO. 5 BARS = 83'-6" = 87 LBS.				
401	56	8'-2"	457'-4"	STIRRUPS IN CAP
402	4	3'-7"	14'-4"	STIRRUPS IN RISER
403	2	2'-9"	5'-6"	LONGIT. IN RISER
404	8	8'-9"	70'-0"	STIRRUPS IN WINGWALL
405	8	2'-10"	22'-8"	LONGIT. IN WINGWALL
406	12	4'-0"	48'-0"	LONGIT. IN WINGWALL
TOTAL NO. 4 BARS = 617'-10" = 413 LBS.				
TOTAL DEFORMED REINFORCING STEEL = 1,443 LBS.				
o CLASS A1 CONCRETE = 7.89 CU. YDS.				
MAX. PILE LOAD: SERVICE DEAD LOAD = 25 TONS				
FACTORED DEAD LOAD = 31 TONS				
FACTORED LIVE LOAD = 77 TONS				

o 16" Ø PPC PILES USED FOR ESTIMATING PURPOSES ONLY. (ADD 0.06 CU. YDS. OF CLASS A1 CONCRETE PER BENT WHEN 14" Ø PPC PILES ARE USED.)



REINFORCING STEEL: ALL REINFORCING SHALL BE GRADE 60. DIMENSIONS RELATING TO FABRICATION ARE OUT TO OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE NOTED. DOWELS (601 BARS) SHALL BE PROVIDED AT ALL FIXED BEARINGS AND APPROACH SLAB BEARINGS (SEE GENERAL PLAN). ALL EXPOSED ENDS OF DOWELS SHALL BE WRAPPED WITH TWO LAYERS OF 15 LB. ASPHALT SATURATED FELT. CLOSE FITTING TUBES OF COMPRESSIBLE MATERIAL NOT LESS THAN 3/16" THICK MAY BE SUBSTITUTED.

PRECAST CONCRETE PILES: FOR DETAILS SEE STANDARD DETAIL BD.2.5.1.0.01 (CS-216). EXTERIOR PILES ARE TO BE BATTERED OUTWARD AT 1/2" ON 12" IN THE LONGITUDINAL DIRECTION OF THE BENT, WHEN NOTED ON THE GENERAL PLAN.

BASIS OF PAYMENT: ALL MATERIALS SHALL BE PAID FOR UNDER "BRIDGE SUPERSTRUCTURE AND SUBSTRUCTURE" ACCORDING TO THE SPECIFICATIONS.

PREFORMED JOINT MATERIAL: PREFORMED JOINT MATERIAL SHALL BE IN ACCORDANCE WITH SECTION 815.04 OF THE STANDARD SPECIFICATIONS.

AS-DESIGNED RATING		
VEHICLE	RATING FACTOR	NOTES
HL-93 (INV)	1.366	—
HL-93 (OPR)	1.771	—
LADV-11 (INV)	1.051	MAGNIFICATION FACTOR = 1.3

SPAN NOTES:

CONSTRUCTION SPECIFICATIONS:
LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS
FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS
AND SPECIAL PROVISIONS.
DESIGN SPECIFICATIONS:
AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION.
WITH 2008 & 2009 INTERIMS.

DESIGN LOADS:
THE BRIDGE DECK IS DESIGNED FOR A FUTURE WEARING
COURSE OF 19 PSF. THE LIVE LOAD IS HL-93, AND LADV-11
(LOUISIANA DESIGN VEHICLE LIVE LOAD 2011).

STRUCTURAL CONCRETE:
ALL CONCRETE SHALL BE CLASS A1. EXPOSED EDGES SHALL HAVE
A 3/4" CHAMFER UNLESS OTHERWISE NOTED. ALL BARRIER RAIL
SURFACES ARE TO RECEIVE A CLASS 3 SPECIAL SURFACE FINISH.

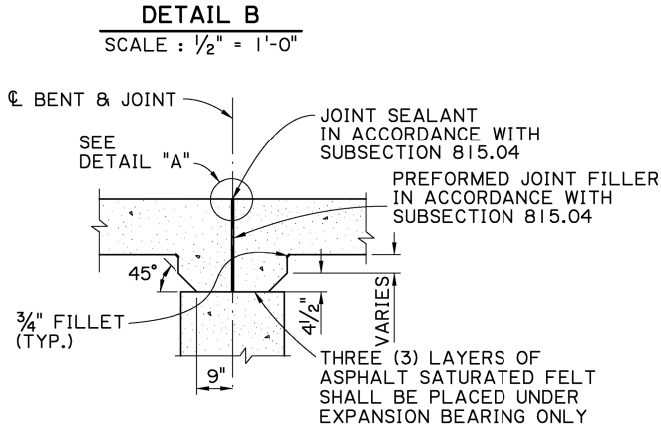
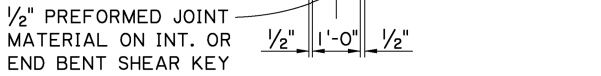
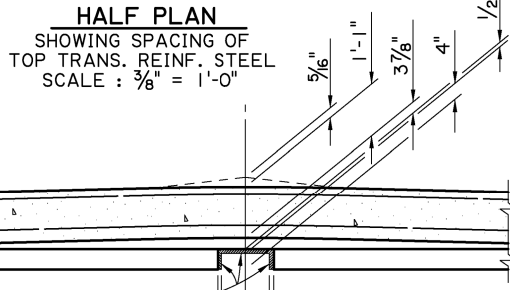
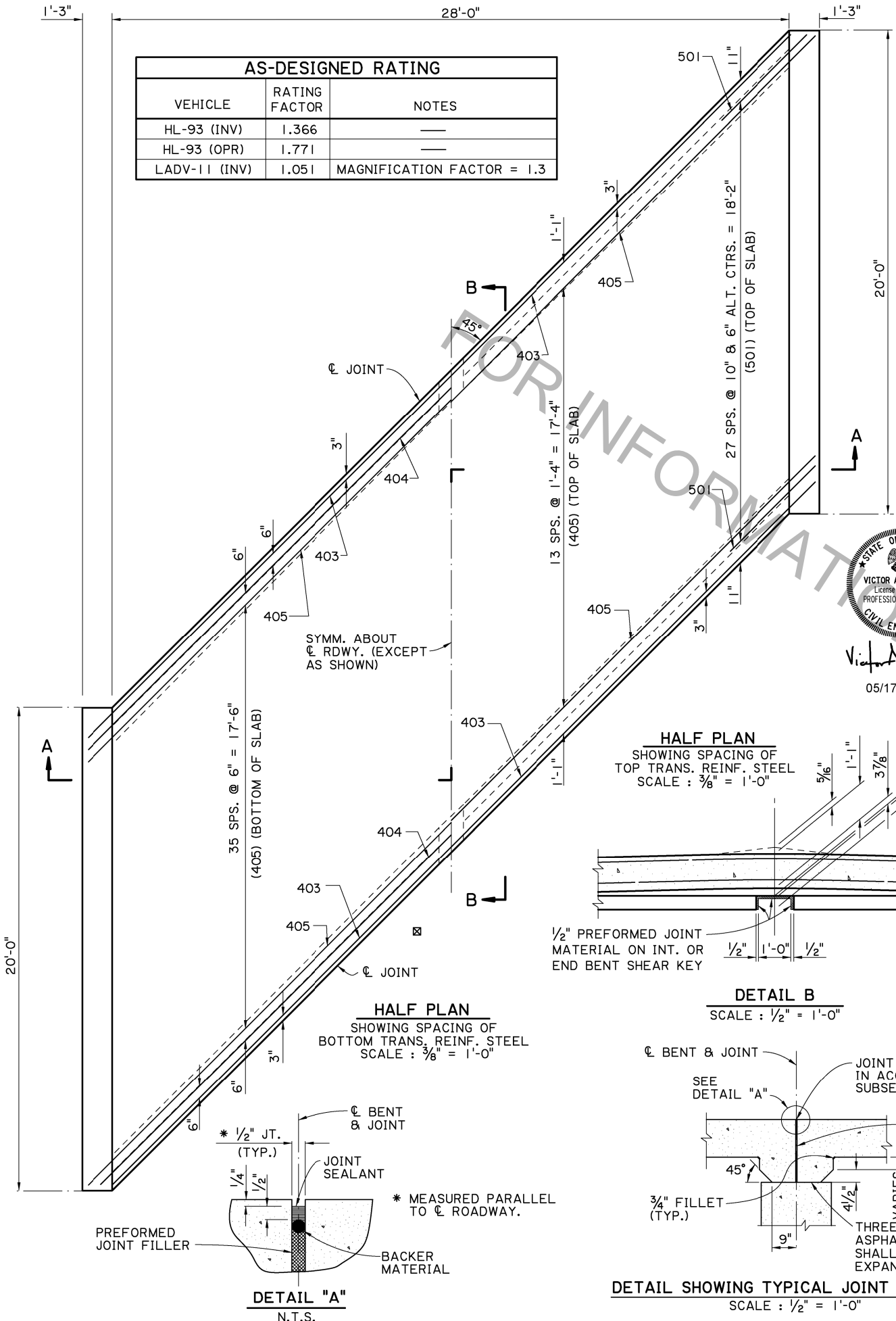
REINFORCING STEEL:
ALL REINFORCING SHALL BE GRADE 60; DIMENSIONS RELATING TO
SPACING ARE TO BAR CENTERS, DIMENSIONS RELATING TO FABRICATION
ARE OUT TO OUT OF BARS, UNLESS OTHERWISE NOTED. ALL
REINFORCING BARS SHALL BE PLACED TO PROVIDE A MINIMUM
COVER OF ONE INCH FROM THE SURFACE OF THE DRAIN HOLES TO
THE FACE OF THE BARS.

GUARD RAIL:
REFER TO THE GENERAL PLAN FOR GUARD RAIL REQUIREMENTS.
PROVIDE HOLES FOR GUARD RAIL CONNECTIONS ACCORDING TO
STANDARD PLAN BD.1.1.1.0.01 (GR-200) ON ALL FOUR BRIDGE RAIL ENDS.

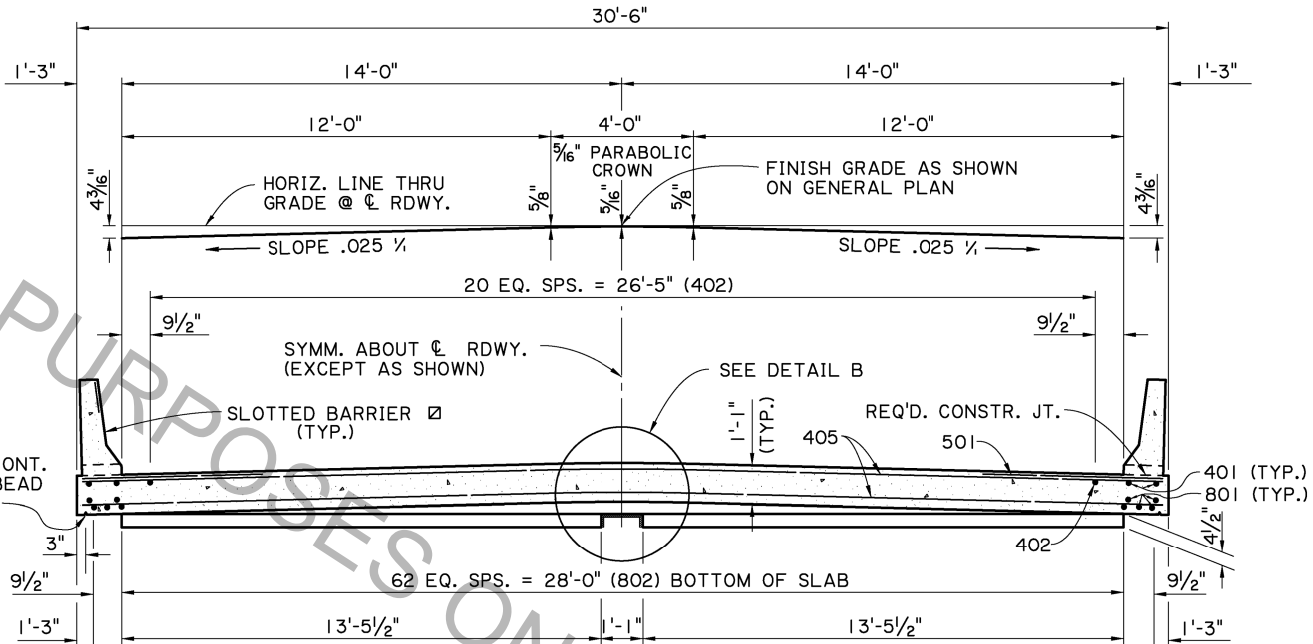
BASIS OF PAYMENT:
ALL MATERIAL SHALL BE PAID FOR UNDER "BRIDGE SUPERSTRUCTURE
AND SUBSTRUCTURE" ACCORDING TO THE SPECIFICATIONS.

ESTIMATED QUANTITIES (ONE SPAN)				
BAR NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION	
801	8	19'-7"	156'-8"	LONGIT. BOT. OF SLAB
802	63	19'-5"	1223'-3"	LONGIT. BOT. OF SLAB
TOTAL NO. 8 BARS = 1379'-11" = 3685 LBS.				
501	56	5'-0"	280'-0"	TRANS. TOP OF SLAB
TOTAL NO. 5 BARS = 280'-0" = 292 LBS.				
401	4	19'-7"	78'-4"	LONGIT. TOP OF SLAB
402	21	19'-5"	407'-9"	LONGIT. TOP OF SLAB
* 403	4	42'-10"	171'-4"	TRANS. TOP & BOT. OF SLAB
* 404	2	43'-7"	87'-2"	TRANS. BOT. OF SLAB
* 405	50	44'-3"	2212'-6"	TRANS. TOP & BOT. OF SLAB
TOTAL NO. 4 BARS = 2957'-1" = 1975 LBS.				
TOTAL DEFORMED REINFORCING STEEL = 5952 LBS.				
CLASS A1 CONCRETE = 25.48 CU. YDS.				
CONCRETE RAILING (BARRIER TYPE) = 40.00 LIN. FT.				

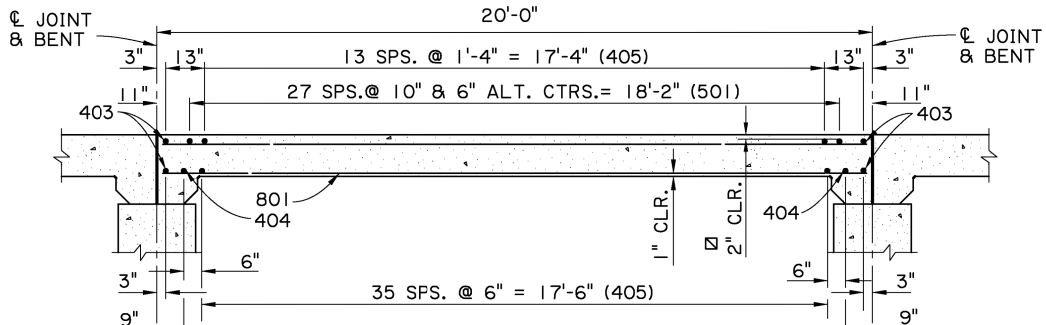
* INCLUDES ONE (1) 1'-8" MINIMUM LAP SPLICE.
ALL LAP SPLICES TO BE STAGGERED.



DETAIL SHOWING TYPICAL JOINT & HAUNCH
SCALE: 1/2" = 1'-0"



SECTION A-A
SCALE: 3/8" = 1'-0"



SECTION B-B
SCALE: 3/8" = 1'-0"

SHEET NUMBER

DESIGNED J. NAKHLEH

CHECKED B. DELATTE

PARISH

CONTROL SECTION

STATE

PROJECT

REVISION OR CHANGE ORDER DESCRIPTION

NO.

DATE

BY

SPAN (1 OF 2)

20'-0" CONCRETE SLAB SPAN

28'-0" CLEAR ROADWAY

45° CROSSING TWO WAY TANGENT

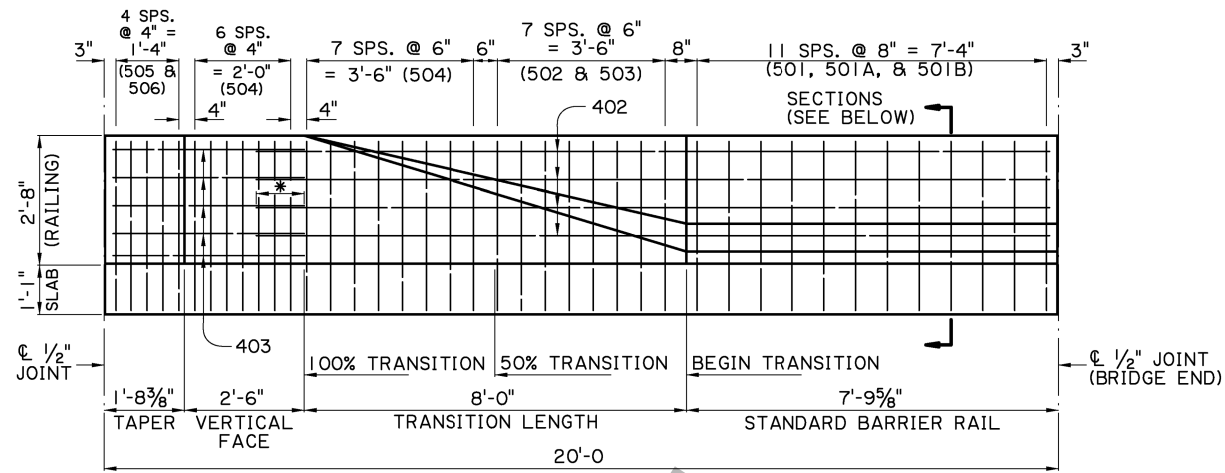
STANDARD DETAIL

PSS-45-28-20SL

DOTD

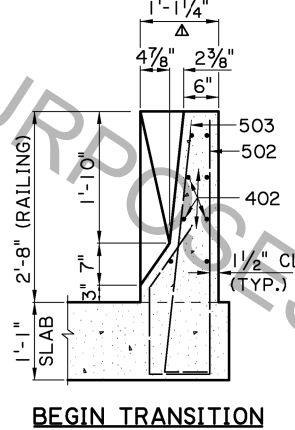
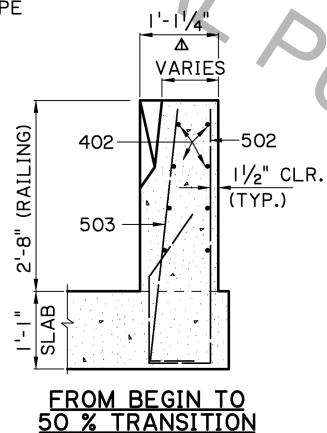
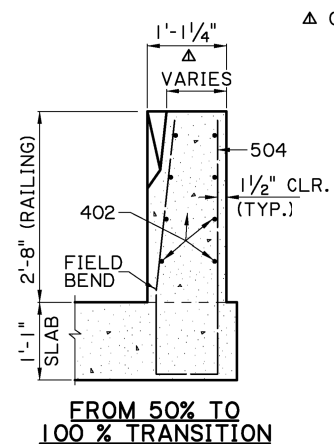
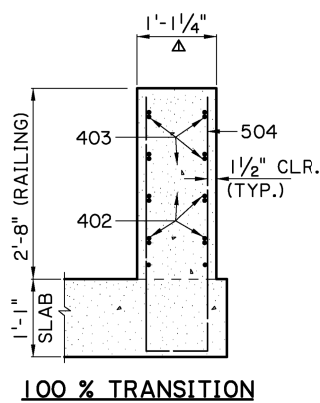
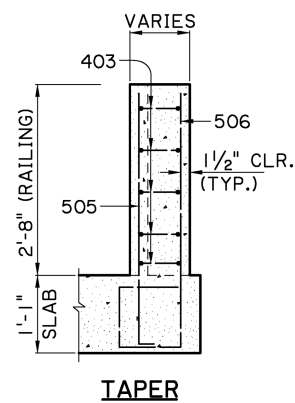
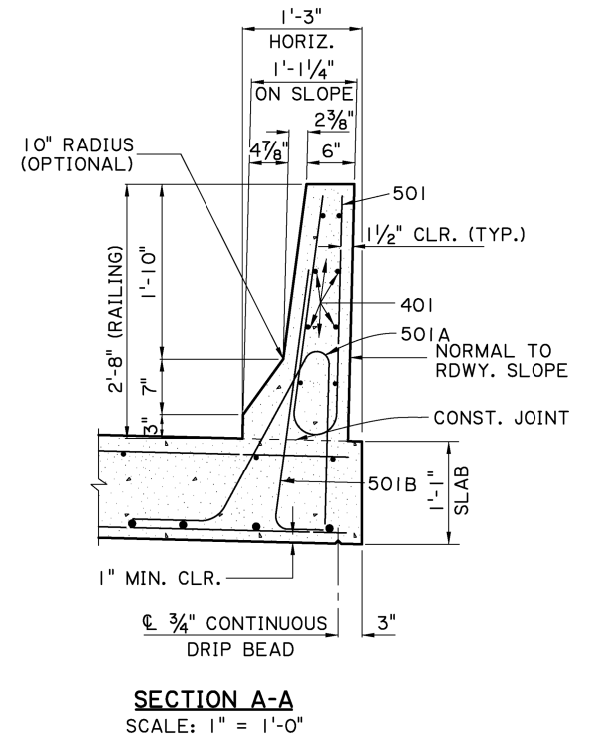
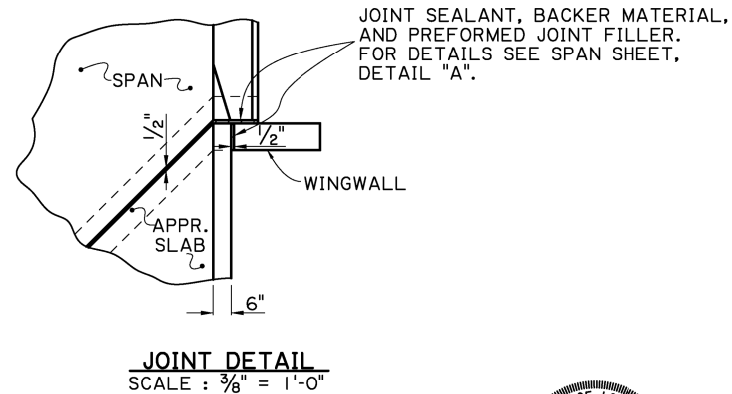
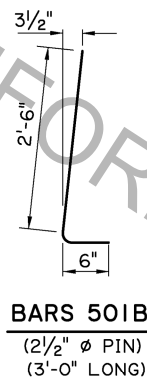
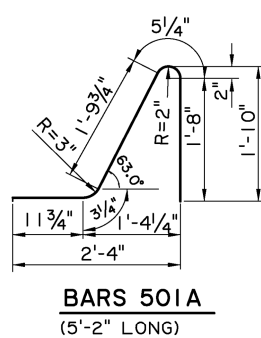
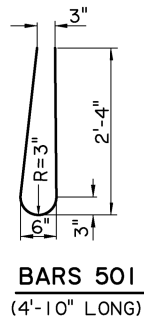
LOUISIANA

DOTD BRIDGE DESIGN

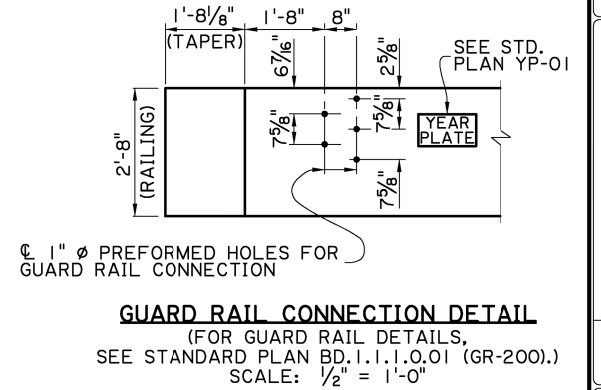
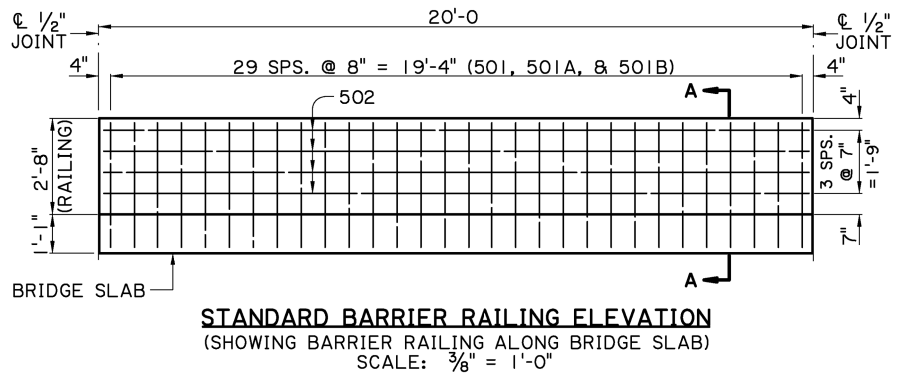
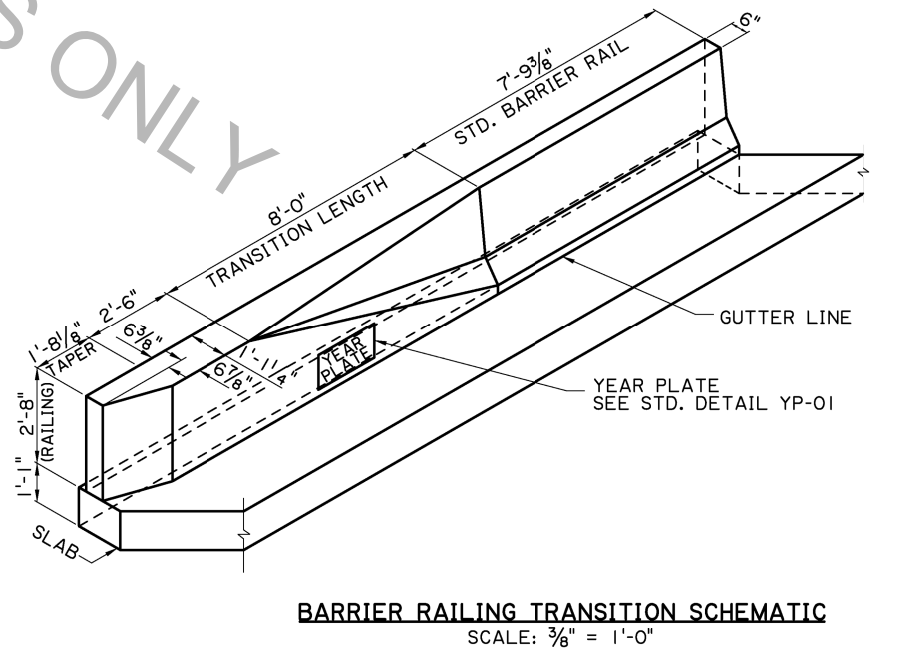
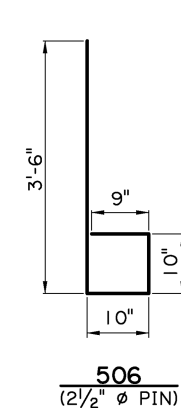
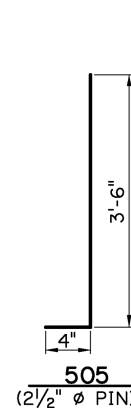
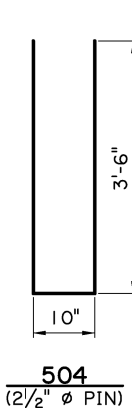
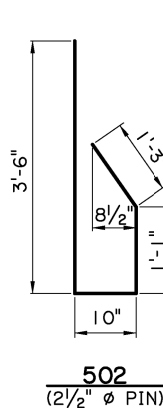
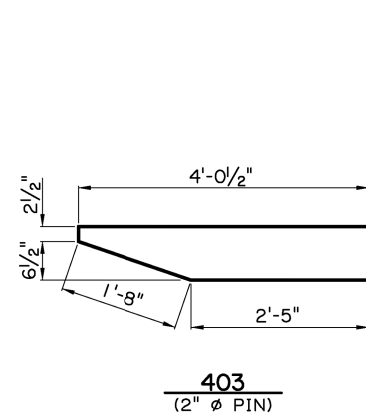


* 1'-0" (MIN.)
SPLICE

BARRIER RAILING TRANSITION ELEVATION
(SHOWING BARRIER RAILING AT END OF BRIDGE)
SCALE: 1/2" = 1'-0"

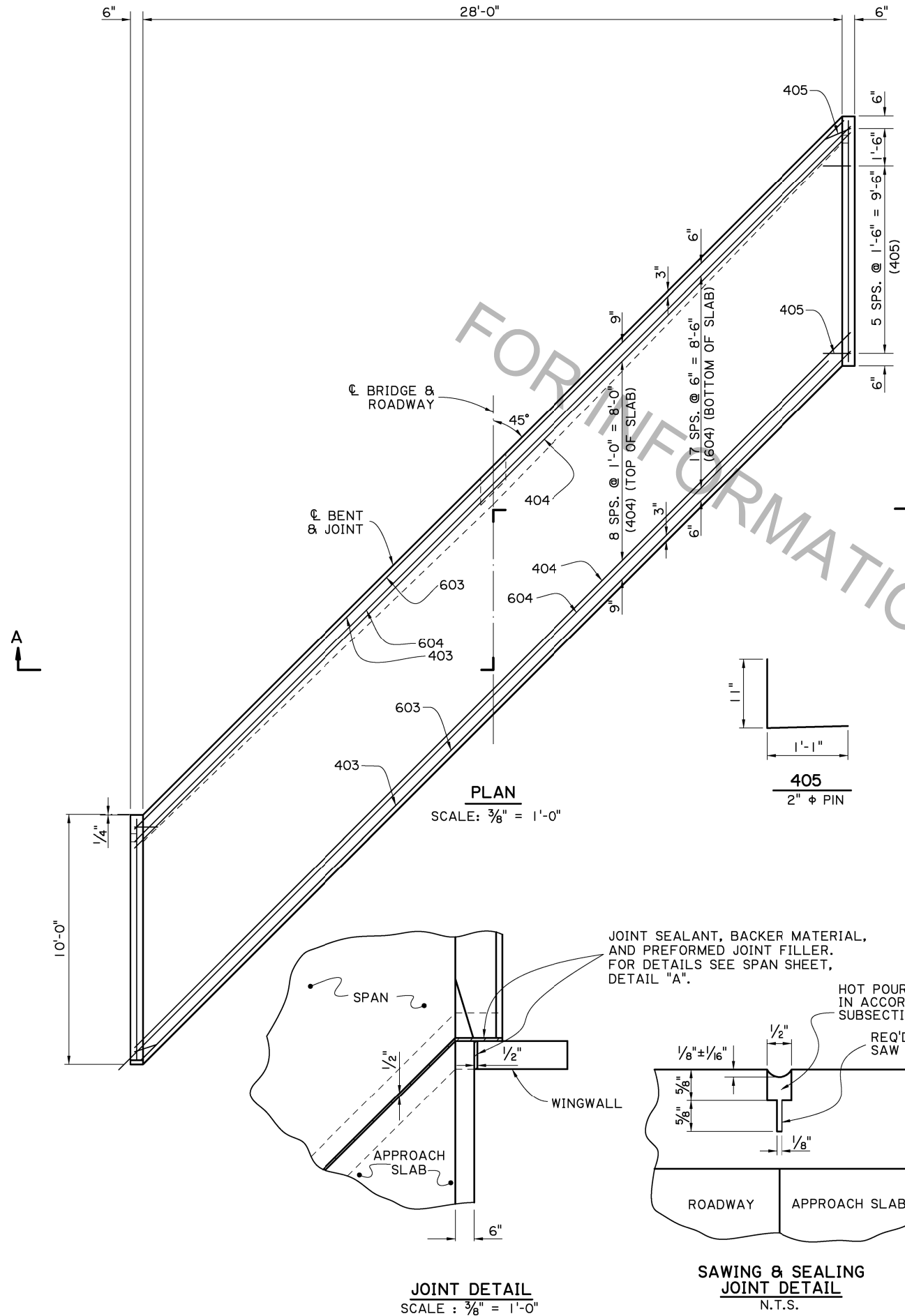


BARRIER RAILING TRANSITION SECTIONS
SCALE: 3/4" = 1'-0"



VICTOR A. SANCHEZ
License No. 33975
PROFESSIONAL ENGINEER
IN
CIVIL ENGINEERING
05/17/17

SHEET NUMBER		PARISH		CONTROL SECTION		STATE PROJECT	
DESIGNED J. NAKHLEH		CHECKED B. DELATTE		REVIEWED J. NAKHLEH		SERIES # 3 OF 13	
DATE		NO.		REVISION OR CHANGE ORDER DESCRIPTION		BY	
SPAN (2 OF 2)		20'-0" CONCRETE BARRIER		28'-0" CLEAR ROADWAY		45° CROSSING TWO WAY TANGENT	
DOTD		DOTD BRIDGE DESIGN		PSS-45-28-20SL			



APPROACH SLAB NOTES:

CONSTRUCTION SPECIFICATIONS: LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION, WITH 2008 & 2009 INTERIMS.

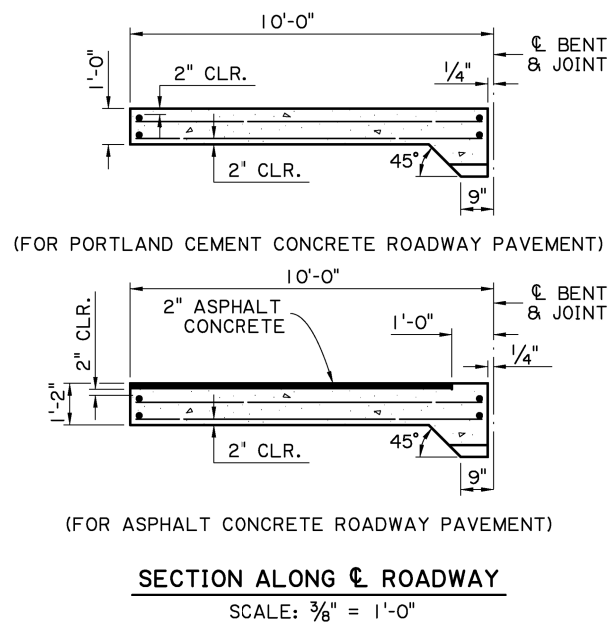
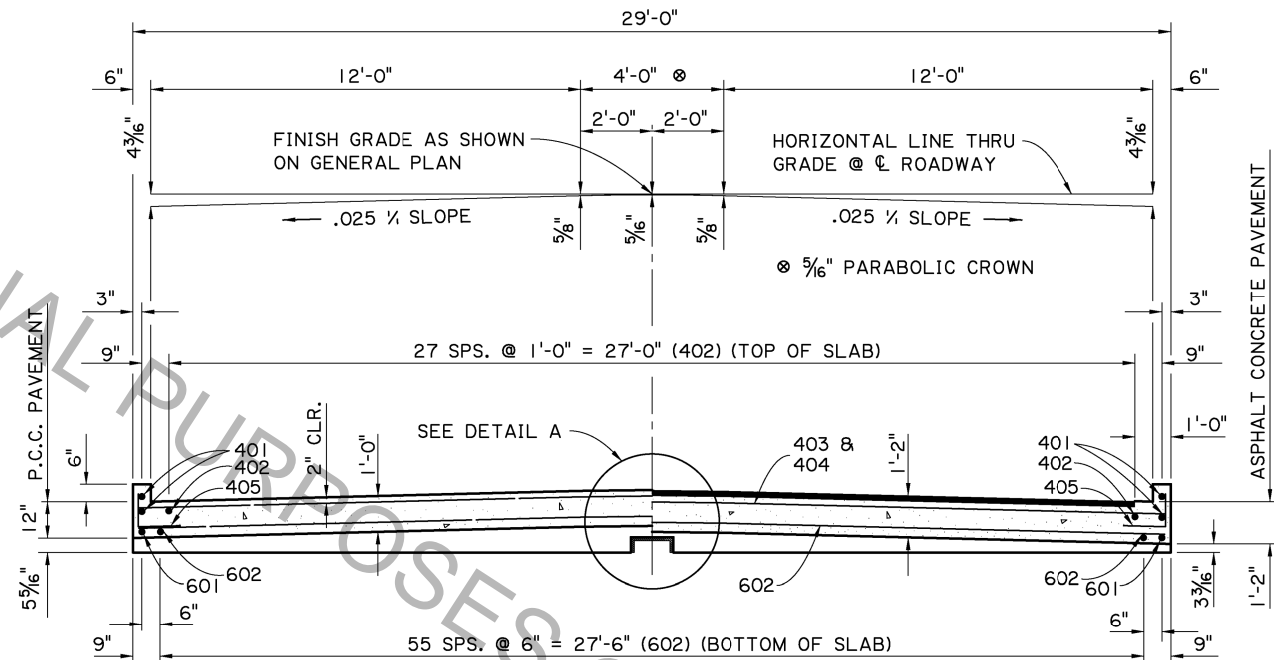
STRUCTURAL CONCRETE: ALL CONCRETE SHALL BE CLASS A1. EXPOSED EDGES SHALL HAVE A $\frac{3}{4}"$ CHAMFER, UNLESS OTHERWISE NOTED.

REINFORCING STEEL: ALL REINFORCING STEEL SHALL BE GRADE 60. DIMENSIONS RELATING TO THE FABRICATION ARE OUT-TO-OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE NOTED.

BEDDING MATERIAL: FOR DETAILS OF BEDDING MATERIAL AND UNDERDRAINS, SEE STANDARD DETAIL BD.2.10.1.0.07.

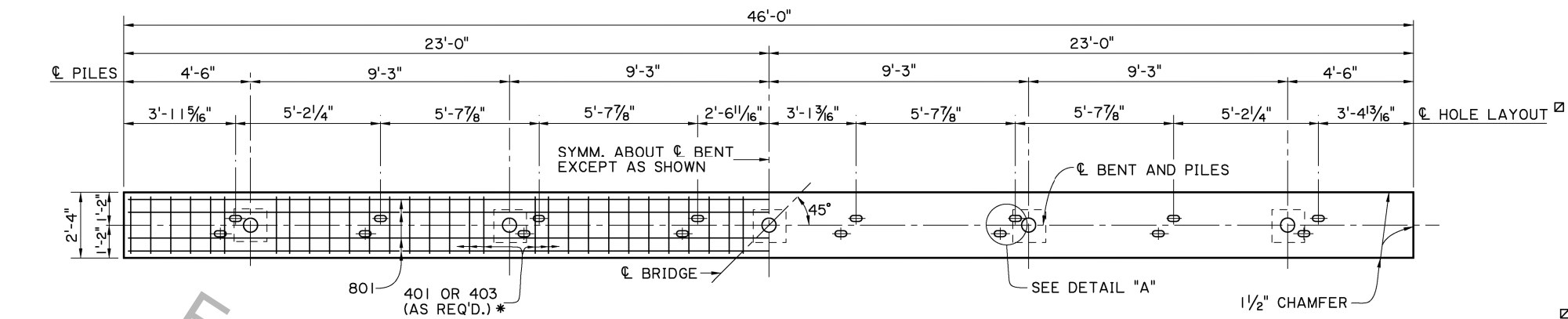
SAWING & SEALING: THE ASPHALT CONCRETE SHALL BE SAW CUT AT THE END OF THE CONCRETE APPROACH SLAB THE ENTIRE ROADWAY WIDTH AND SEALED, COST TO BE INCLUDED WITH CONCRETE APPROACH SLABS.

BASIS OF PAYMENT: ALL MATERIAL SHALL BE PAID FOR UNDER 'CONCRETE APPROACH SLABS' ACCORDING TO THE SPECIFICATIONS.



ESTIMATED QUANTITIES (ONE SLAB)				
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
601	2	9'-7"	19'-2"	LONGIT. BOT. OF SLAB
602	56	9'-6"	532'-0"	LONGIT. BOT. OF SLAB
603	2	40'-1"	80'-2"	TRANSV. BOT. OF SLAB
604	18	40'-6"	729'-0"	TRANSV. BOT. OF SLAB
TOTAL NO. 6 BARS = 1360'-4"				= 2,043 LBS.
401	4	9'-7"	38'-4"	LONGIT. TOP OF SLAB & CURB
402	28	9'-6"	266'-0"	LONGIT. TOP OF SLAB
403	2	41'-9"	83'-6"	TRANSV. TOP OF SLAB
404	9	42'-2"	379'-6"	TRANSV. TOP OF SLAB
405	14	2'-0"	28'-0"	DOWELS IN CURB
TOTAL NO. 4 BARS = 795'-4"				= 531 LBS.
TOTAL DEFORMED REINFORCING STEEL =				2,574 LBS.
CONCRETE APPROACH SLAB =				32.22 SQ. YDS.
ASPHALT CONCRETE =				3.0 TONS
SAW CUT & SEAL =				38 LIN. FT.

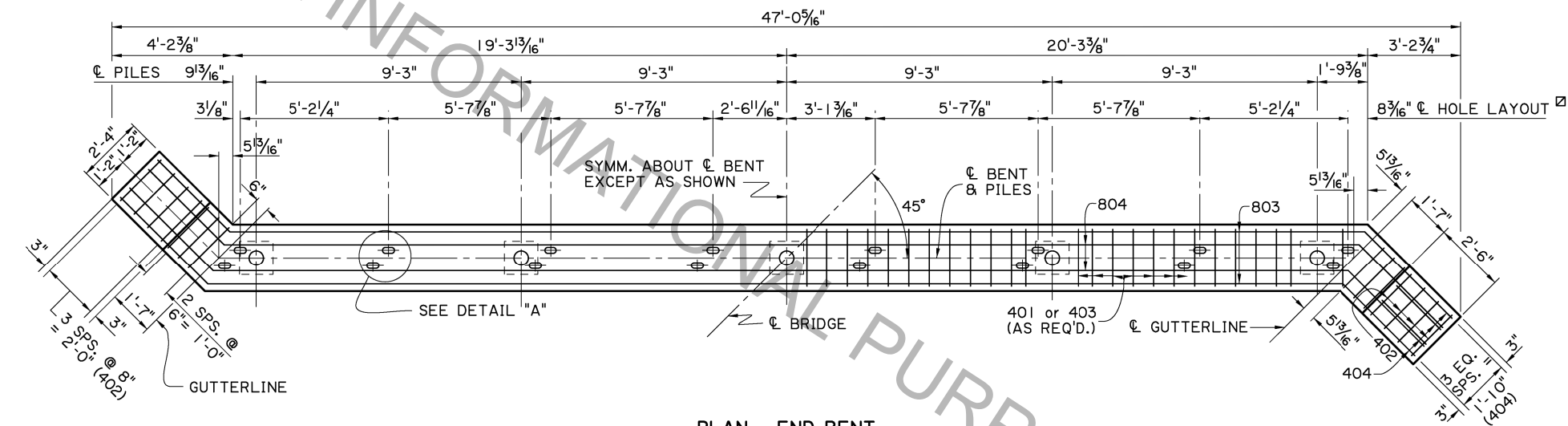
- TO BE PAID FOR UNDER ITEM CONCRETE APPROACH SLABS.
- INCLUDES ONE (1) 1'-8" MINIMUM LAP SPLICE. ALL LAP SPLICES ARE TO BE STAGGERED.
- REQUIRED WHEN APPROACH SLAB IS ADJACENT TO ASPHALT CONCRETE PAVEMENT.



PLAN - INTERMEDIATE BENT

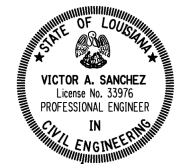
SCALE 3/8" = 1'-0"

HOLES ARE OPTIONAL AT EXPANSION ENDS.

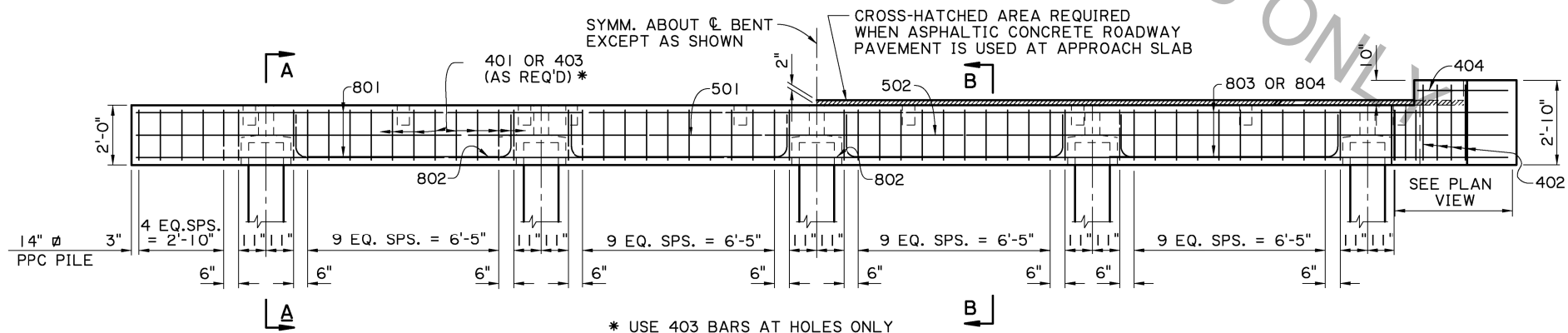


PLAN - END BENT

SCALE 3/8" = 1'-0"



Victor A. Sanchez
05/17/17



HALF ELEVATION - INTERMEDIATE BENT

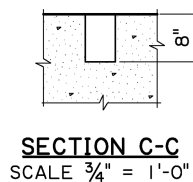
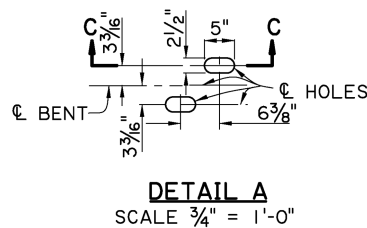
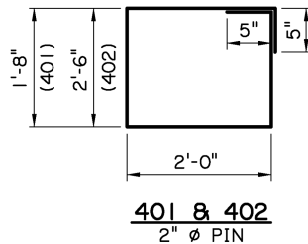
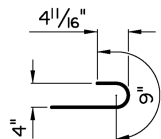
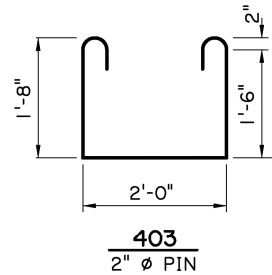
SCALE 3/8" = 1'-0"

HALF ELEVATION - END BENT

SCALE 3/8" = 1'-0"

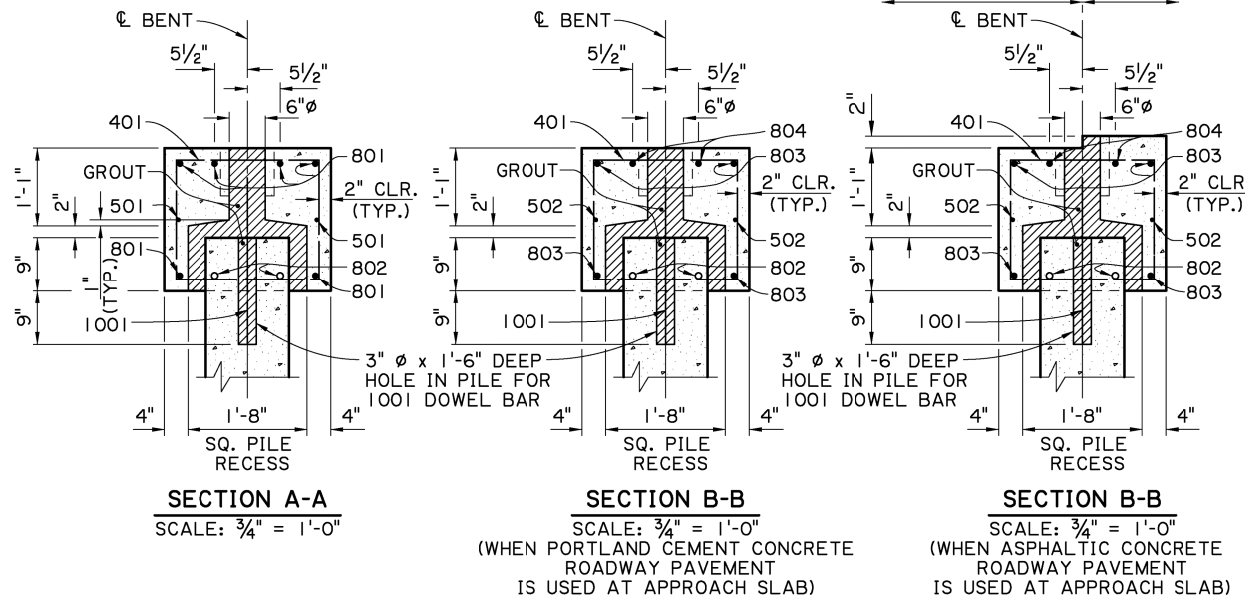
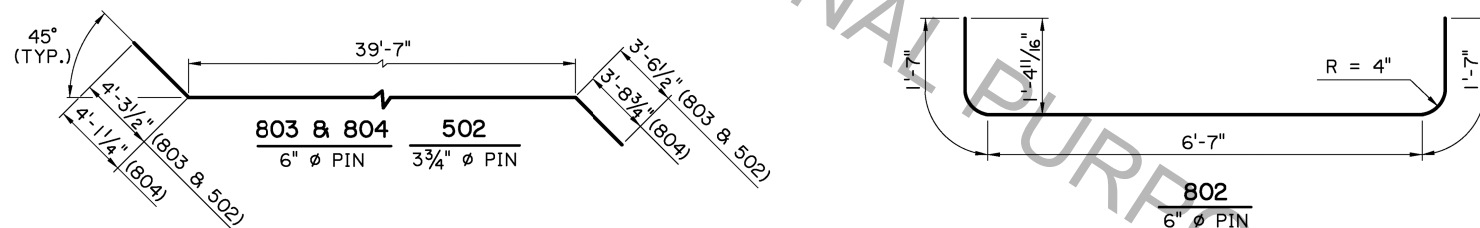
* USE 403 BARS AT HOLES ONLY

SHEET NUMBER		PARISH		CONTROL SECTION		STATE PROJECT	
DESIGNED B. DELATTE		CHECKED J. NAKHLEH		REVIEWED J. NAKHLEH		SERIES # 5 OF 13	
DATE		NO.		REVISION OR CHANGE ORDER DESCRIPTION		BY	
05/17/17		13		ALTERNATE BENTS (1 OF 2) PRECAST CONCRETE BENTS 28'-0" CLEAR ROADWAY 45° CROSSING TWO WAY TANGENT		PSS-45-28-20SL	
DOTD		DOTD BRIDGE DESIGN		STANDARD DETAIL			



ALTERNATE BENT NOTES:

CONSTRUCTION SPECIFICATIONS: LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.
DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION, WITH 2008 & 2009 INTERIMS.
DESIGN LOAD: LIVE LOAD IS HL-93, AND LADV-11 (LOUISIANA DESIGN VEHICLE LIVE LOAD 2011).
STRUCTURAL CONCRETE: ALL CONCRETE SHALL BE CLASS P1. STEEL SIDE FORMS AND STEEL OR CONCRETE BOTTOM FORMS SHALL BE USED FOR PRECAST COMPONENTS. EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER UNLESS OTHERWISE NOTED. ALL SURFACES SHALL RECEIVE A CLASS 1 ORDINARY SURFACE FINISH UPON REMOVAL OF THE FORMS. ALL EXPOSED FACES OF WINGWALLS AND ENDS OF CAPS SHALL RECEIVE A CLASS 3 SPECIAL SURFACE FINISH.
REINFORCING STEEL: ALL REINFORCING SHALL BE GRADE 60. DIMENSIONS RELATING TO FABRICATION ARE OUT TO OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE NOTED.
GROUT: THE GROUT SHALL BE AN APPROVED FLOWABLE NON-SHRINK GROUT LISTED ON AML. THE GROUT SHALL BE TESTED FOR ACCEPTANCE PRIOR TO USAGE. SURFACES SHALL BE THOROUGHLY SATURATED WITH WATER BY FLOODING THE VOID FOR APPROXIMATELY 5 MINUTES IMMEDIATELY BEFORE THE GROUT IS PLACED. ONLY POTABLE WATER SHALL BE USED FOR SATURATION AND MIXING PURPOSES.
PRECAST UNITS: THE PLANS FOR AN ONGOING OPERATION OF FABRICATING FACILITIES SHALL BE APPROVED BY THE DEPARTMENT. EACH UNIT SHALL HAVE THE FABRICATOR'S MARK AND UNIQUE NUMBER, MEETING THE APPROVAL OF THE ENGINEER, STAMPED OR SCRIBED IN THE PLASTIC CONCRETE. ALL UNITS SHALL BE HELD AT THE PLANT FOR A MINIMUM OF 10 DAYS AFTER CASTING. THE CONCRETE SHALL REACH A MINIMUM STRENGTH OF 3,000 PSI BEFORE HANDLING IS PERMITTED. THE LIFTING INSERTS SHALL BE 1" TYPE S INSERTS AS MANUFACTURED BY DAYTON-SUPERIOR CORPORATION OR AN APPROVED EQUAL. EACH INSERT SHALL HAVE A MINIMUM LOAD CAPACITY OF 10,000 POUNDS. FOUR INSERTS WITH 1" Ø x 5" LONG COIL BOLTS SHALL BE PLACED IN THE TOP OF THE UNITS AND LOCATED AT A DISTANCE 21% OF ITS LENGTH (+/- 6") FROM EACH END AND 6" FROM THE EDGES. INSERT HOLES SHALL BE GROUT FILLED AFTER PLACEMENT OF THE UNIT. AT THE CONTRACTOR'S OPTION, A SLING OF SUFFICIENT CAPACITY MAY BE USED FOR LIFTING, PROVIDED THE SAME PICKUP LOCATIONS FROM THE ENDS ARE USED.
PRECAST CONCRETE PILES: PILES SHALL BE FABRICATED ACCORDING TO STANDARD DETAIL BD.2.5.1.0.01 (CS-216). THE CENTROID OF THE PILE AT CUTOFF ELEVATION SHALL NOT VARY FROM THE PLAN LOCATION BY MORE THAN 3" MEASURED EITHER PERPENDICULAR OR PARALLEL TO THE CENTERLINE OF BENT. IF THE CENTROID OF A PILE IS OUTSIDE THESE LIMITS BUT WITHIN THE ACCURACY OF DRIVING REQUIRED BY THE SPECIFICATIONS, A BENT CAP SHALL BE PROVIDED ACCORDING TO THE CAST-IN-PLACE ALTERNATE. EXTERIOR PILES ARE TO BE BATTERED OUTWARD A 1 1/2 ON 12 IN THE LONGITUDINAL DIRECTION OF THE BENT, WHEN NOTED ON THE GENERAL PLAN.
BASIS OF PAYMENT: ALL MATERIALS SHALL BE PAID FOR UNDER "BRIDGE SUPERSTRUCTURE AND SUBSTRUCTURE" ACCORDING TO THE SPECIFICATIONS.

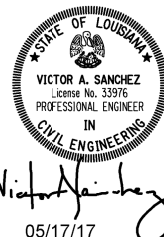


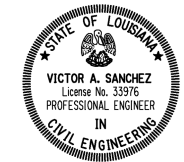
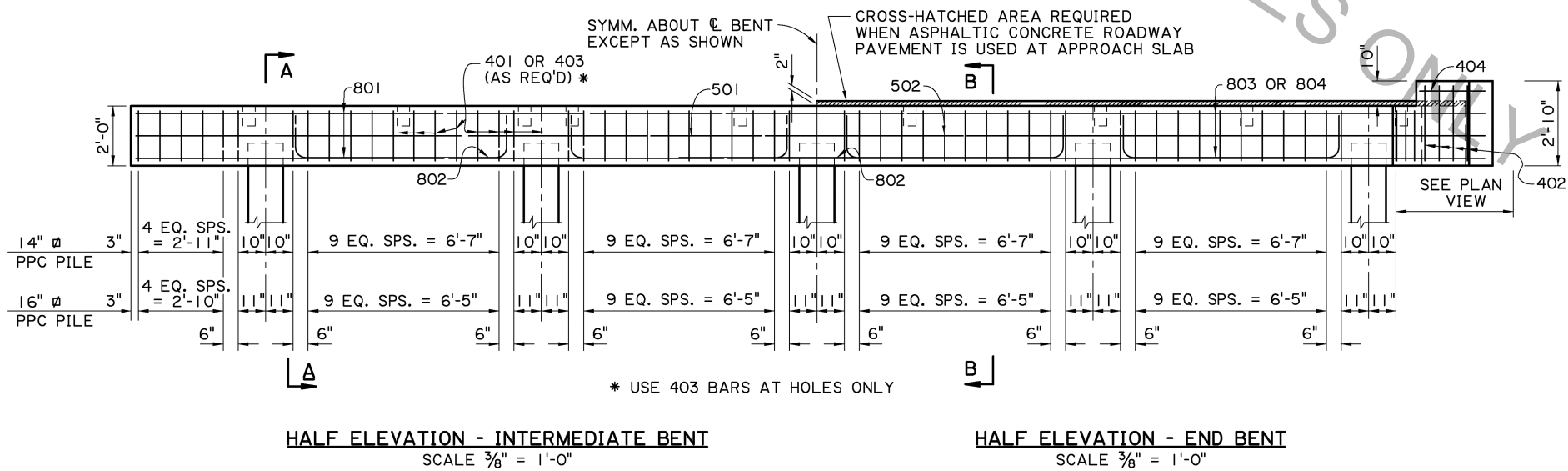
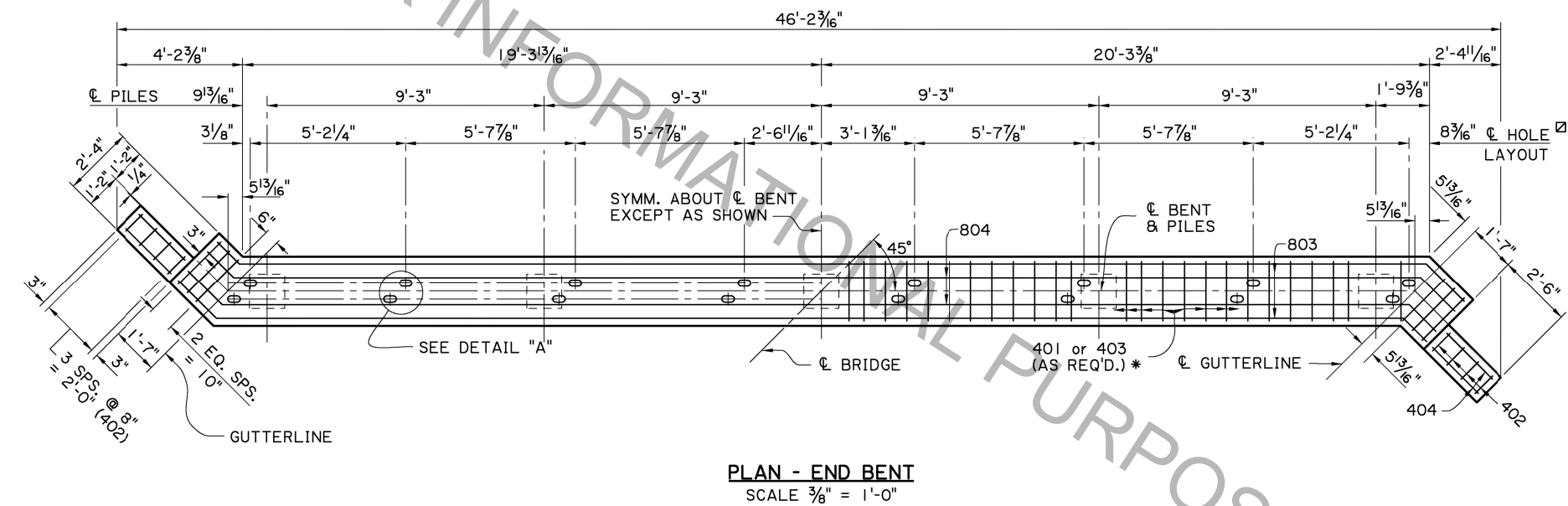
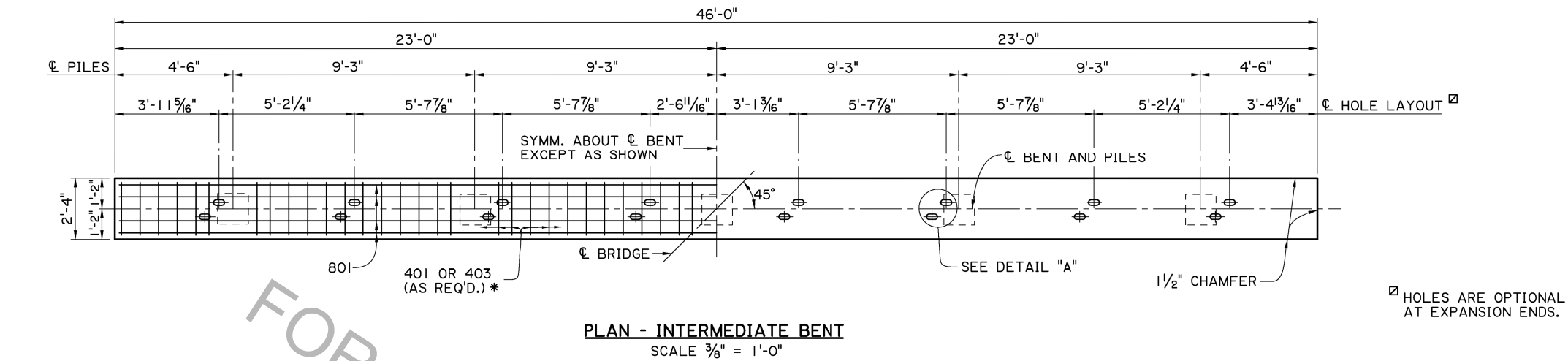
ESTIMATED QUANTITIES (ONE INTER. BENT)				
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
1001	5	2'-4"	11'-8"	DOWELS IN PILES
TOTAL NO. 10 BARS = 11'-8" = 50 LBS.				
801	6	45'-8"	274'-0"	LONGIT. IN CAP
802	8	9'-9"	78'-0"	LONGIT. IN CAP BTW. PILES
TOTAL NO. 8 BARS = 352'-0" = 940 LBS.				
501	2	45'-8"	91'-4"	LONGIT. IN CAP
TOTAL NO. 5 BARS = 91'-4" = 95 LBS.				
401	52	8'-2"	424'-8"	STIRRUPS IN CAP
403	8	6'-6"	52'-0"	STIRRUPS IN CAP
TOTAL NO. 4 BARS = 476'-8" = 318 LBS.				
TOTAL DEFORMED REINFORCING STEEL = 1403 LBS.				
TOTAL CLASS P1 CONCRETE = 7.39 CU. YDS.				
MAX. PILE LOAD: SERVICE DEAD LOAD = 22 TONS				
SERVICE LIVE LOAD = 33 TONS				
FACTORED TOTAL LOAD = 76 TONS				
TOTAL GROUT FOR PILE RECESSES = 0.36 CU. YDS.				

ESTIMATED QUANTITIES (ONE END BENT)				
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
1001	5	2'-4"	11'-8"	DOWELS IN PILES
TOTAL NO. 10 BARS = 11'-8" = 50 LBS.				
802	8	9'-9"	78'-0"	LONGIT. IN CAP BTW. PILES
803	4	47'-5"	189'-8"	LONGIT. IN CAP
804	2	47'-5"	94'-10"	LONGIT. IN CAP
TOTAL NO. 8 BARS = 362'-6" = 968 LBS.				
502	2	47'-7"	94'-10"	LONGIT. IN CAP
TOTAL NO. 5 BARS = 94'-10" = 99 LBS.				
401	48	8'-2"	392'-0"	STIRRUPS IN CAP
402	8	9'-10"	78'-8"	STIRRUPS IN WINGWALL
403	8	6'-6"	52'-0"	STIRRUPS IN CAP
404	8	2'-2"	17'-4"	LONGIT. IN WINGWALL
TOTAL NO. 4 BARS = 540'-0" = 361 LBS.				
TOTAL DEFORMED REINFORCING STEEL = 1,478 LBS.				
* TOTAL CLASS P1 CONCRETE = 8.05 CU. YDS.				
MAX. PILE LOAD: SERVICE DEAD LOAD = 22 TONS				
SERVICE LIVE LOAD = 33 TONS				
FACTORED TOTAL LOAD = 76 TONS				
TOTAL GROUT FOR PILE RECESSES = 0.36 CU. YDS.				

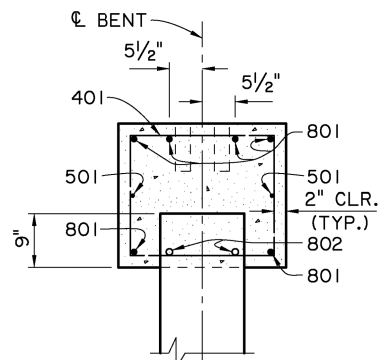
* ADD 0.31 CU. YDS. OF CLASS P1 CONCRETE PER BENT WHEN ASPHALTIC CONCRETE ROADWAY PAVEMENT IS USED AT APPROACH SLAB.

AS-DESIGNED RATING		
VEHICLE	RATING FACTOR	NOTES
HL-93 (INV)	1.605	
HL-93 (OPR)	2.081	
LADV-11 (INV)	1.235	MAGNIFICATION FACTOR = 1.3

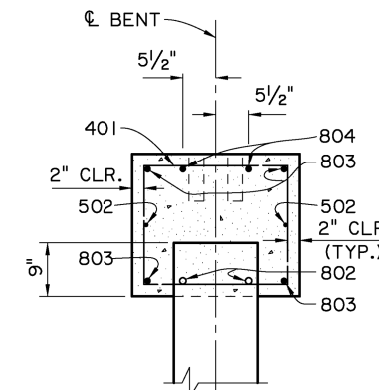




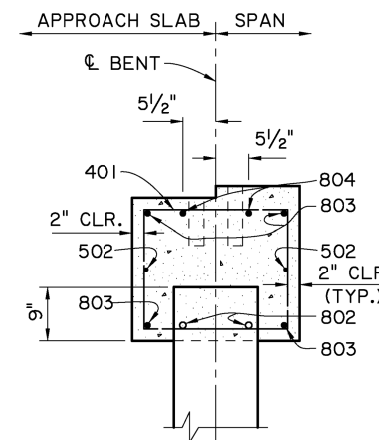
Victor A. Sanchez
05/17/17



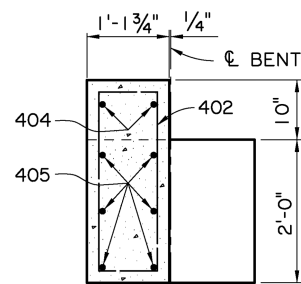
SECTION A-A
SCALE: 3/4" = 1'-0"



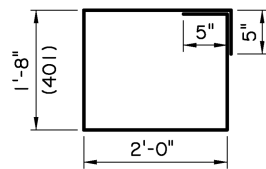
SECTION B-B
SCALE: 3/4" = 1'-0"
(WHEN PORTLAND CONCRETE ROADWAY PAVEMENT IS USED AT APPROACH SLAB)



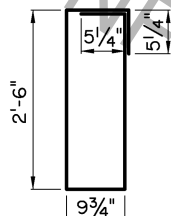
SECTION B-B
SCALE: 3/4" = 1'-0"
(WHEN ASPHALTIC CONCRETE ROADWAY PAVEMENT IS USED AT APPROACH SLAB)



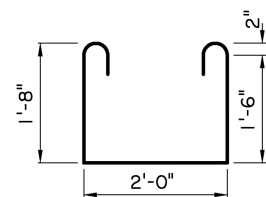
END ELEVATION
SCALE: 3/4" = 1'-0"



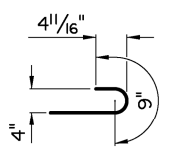
401
2" Ø PIN



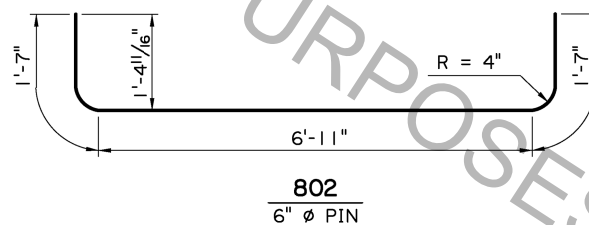
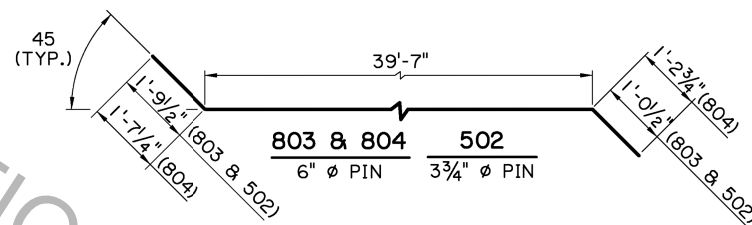
402
2" Ø PIN



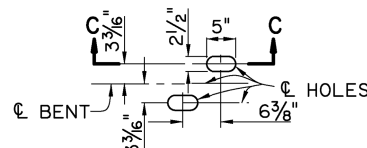
403
2" Ø PIN



180° HOOK FOR
403 BARS
3" Ø PIN



802
6" Ø PIN



DETAIL A
SCALE 3/4" = 1'-0"



SECTION C-C
SCALE 3/4" = 1'-0"

ALTERNATE BENT NOTES:

CONSTRUCTION SPECIFICATIONS: LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION, WITH 2008 & 2009 INTERIMS.

DESIGN LOAD: LIVE LOAD IS HL-93, AND LADV-11 (LOUISIANA DESIGN VEHICLE LIVE LOAD 2011).

STRUCTURAL CONCRETE: ALL CONCRETE SHALL BE CLASS A1. EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER UNLESS OTHERWISE NOTED. ALL EXPOSED FACES OF WINGWALLS AND ENDS OF CAPS SHALL RECEIVE A SURFACE FINISH AS PER SUBSECTION 805.08 OF THE STANDARD SPECIFICATIONS, EXCEPT WHEN SPECIFIED ELSEWHERE IN THE PLANS.

REINFORCING STEEL: ALL REINFORCING SHALL BE GRADE 60. DIMENSIONS RELATING TO FABRICATION ARE OUT TO OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE NOTED.

PRECAST CONCRETE PILES: FOR DETAILS OF PILES SEE STANDARD DETAIL BD.2.5.1.0.01 (CS-216). EXTERIOR PILES ARE TO BATTERED OUTWARD AT 1 1/2 ON 12 IN THE LONGITUDINAL DIRECTION OF THE BENT, WHEN NOTED ON THE GENERAL PLAN.

BASIS OF PAYMENT: ALL MATERIALS SHALL BE PAID FOR UNDER "BRIDGE SUPERSTRUCTURE AND SUBSTRUCTURE" ACCORDING TO THE SPECIFICATIONS.

ESTIMATED QUANTITIES (ONE INTER. BENT)

BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
801	6	45'-8"	274'-0"	LONGIT. IN CAP
802	8	10'-1"	80'-8"	LONGIT. IN CAP BTW. PILES
TOTAL NO. 8 BARS =		354'-8"	=	947 LBS.
501	2	45'-8"	91'-4"	LONGIT. IN CAP
TOTAL NO. 5 BARS =		91'-4"	=	95 LBS.
401	52	8'-2"	424'-8"	STIRRUPS IN CAP
403	8	6'-6"	52'-0"	STIRRUPS IN CAP
TOTAL NO. 4 BARS =		476'-8"	=	318 LBS.
TOTAL DEFORMED REINFORCING STEEL =		1360 LBS.		
TOTAL CLASS A1 CONCRETE =		7.67 CU. YDS.		
MAX. PILE LOAD: SERVICE DEAD LOAD =		22 TONS		
SERVICE LIVE LOAD =		33 TONS		
FACTORED TOTAL LOAD =		76 TONS		

16" Ø PPC PILES USED FOR ESTIMATING PURPOSES ONLY. (ADD 0.06 CU. YDS. OF CLASS A1 CONCRETE PER BENT WHEN 14" Ø PPC PILES ARE USED.)

ESTIMATED QUANTITIES (ONE END BENT)

BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
802	8	10'-1"	80'-8"	LONGIT. IN CAP BTW. PILES
803	4	42'-5"	169'-8"	LONGIT. IN CAP
804	2	42'-5"	84'-10"	LONGIT. IN CAP
TOTAL NO. 8 BARS =		335'-2"	=	895 LBS.
502	2	42'-5"	84'-10"	LONGIT. IN CAP
TOTAL NO. 5 BARS =		84'-10"	=	88 LBS.
401	48	8'-2"	392'-0"	STIRRUPS IN CAP
402	8	7'-6"	60'-0"	STIRRUPS IN WINGWALL
403	8	6'-6"	52'-0"	STIRRUPS IN CAP
404	4	2'-2"	8'-8"	LONGIT. IN WINGWALL
405	12	3'-11"	47'-0"	LONGIT. IN WINGWALL
TOTAL NO. 4 BARS =		559'-8"	=	374 LBS.
TOTAL DEFORMED REINFORCING STEEL =		1,357 LBS.		
TOTAL CLASS A1 CONCRETE =		7.72 CU. YDS.		
MAX. PILE LOAD: SERVICE DEAD LOAD =		22 TONS		
SERVICE LIVE LOAD =		33 TONS		
FACTORED TOTAL LOAD =		76 TONS		

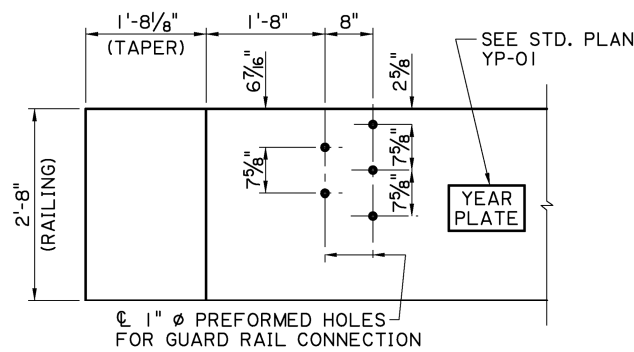
16" Ø PPC PILES USED FOR ESTIMATING PURPOSES ONLY. (ADD 0.06 CU. YDS. OF CLASS A1 CONCRETE PER BENT WHEN 14" Ø PPC PILES ARE USED.) ADD 0.31 CU. YDS. OF CLASS A1 CONCRETE PER BENT WHEN ASPHALTIC CONCRETE ROADWAY PAVEMENT IS USED AT APPROACH SLAB.

AS-DESIGNED RATING

VEHICLE	RATING FACTOR	NOTES
HL-93 (INV)	1.333	
HL-93 (OPR)	1.729	
LADV-11 (INV)	1.026	MAGNIFICATION FACTOR = 1.3

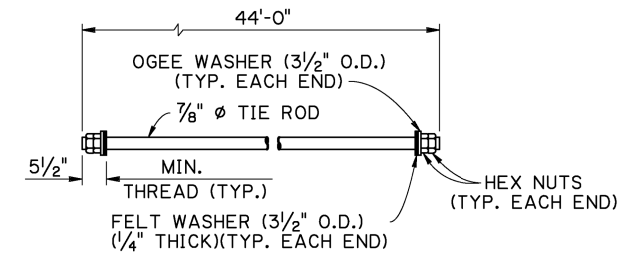


05/17/17



(FOR GUARD RAIL DETAILS, SEE
STANDARD PLAN BD.1.1.1.0.01 (GR-200).)
SCALE : $\frac{3}{4}" = 1'-0"$

THE NUTS & WASHERS FOR THE TIE ROD SHALL BE ZINC COATED AND THE EXPOSED ENDS TO THE TIE RODS SHALL BE PAINTED WITH AN APPROVED COATING. AS A FINAL OPERATION THE CONTRACTOR SHALL BE REQUIRED TO TORQUE THE INSTALLED TIE ROD TO 170 FT. LBS. JUST PRIOR TO PAINTING. ALL EXPOSED ENDS SHALL BE PAINTED WITH AN APPROVED COATING AFTER STRESSING. ONE (1) MECHANICAL SPLICE MAY BE USED IN SPLICING THE 7/8" Ø TIE ROD. THE SPLICE SHALL DEVELOP AT LEAST 125% OF THE SPECIFIED YIELD STRENGTH OF THE TIE ROD IN TENSION. THE MECHANICAL SPLICE SHALL BE ZINC COATED OR PAINTED WITH AN APPROVED COLD GALVANIZING REPAIR COMPOUND FROM AML PRIOR TO PLACING THE TIE ROD IN THE STRUCTURE.



AL, 20'-0"

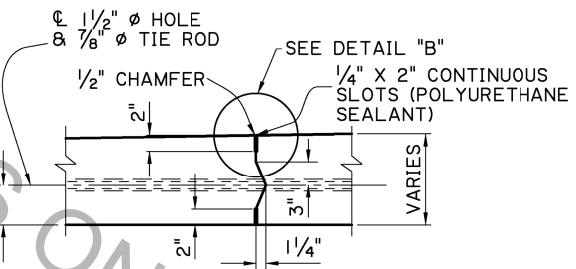
4" 29 SPS. @ 8" = 19'-4" (50I, 50IA, & 50IB) 4"

40I A

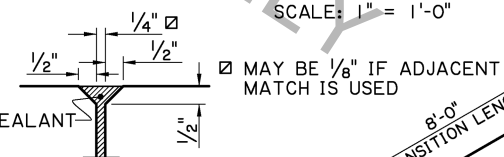
2'-8" (RAILING) 4" 3 SPS @ 7" = 1'-9" 7"

10" AB A

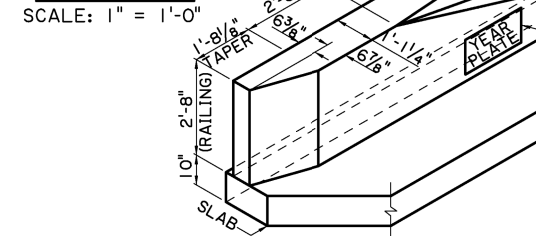
(SHOWING BARRIER RAILING ALONG BRIDGE END)
SCALE : $\frac{3}{8}" = 1'-0"$



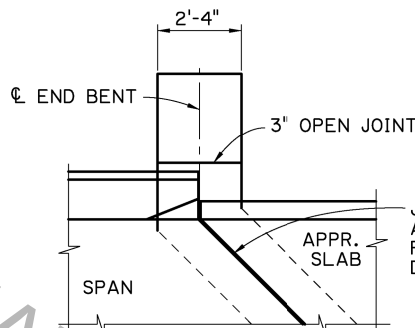
SCALE: 1" = 1'-0"



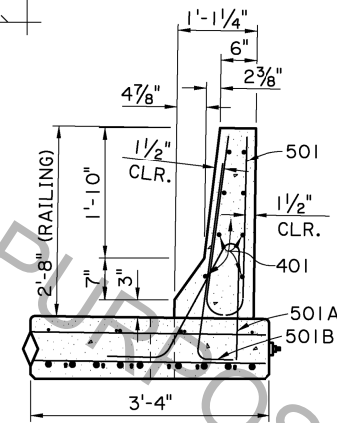
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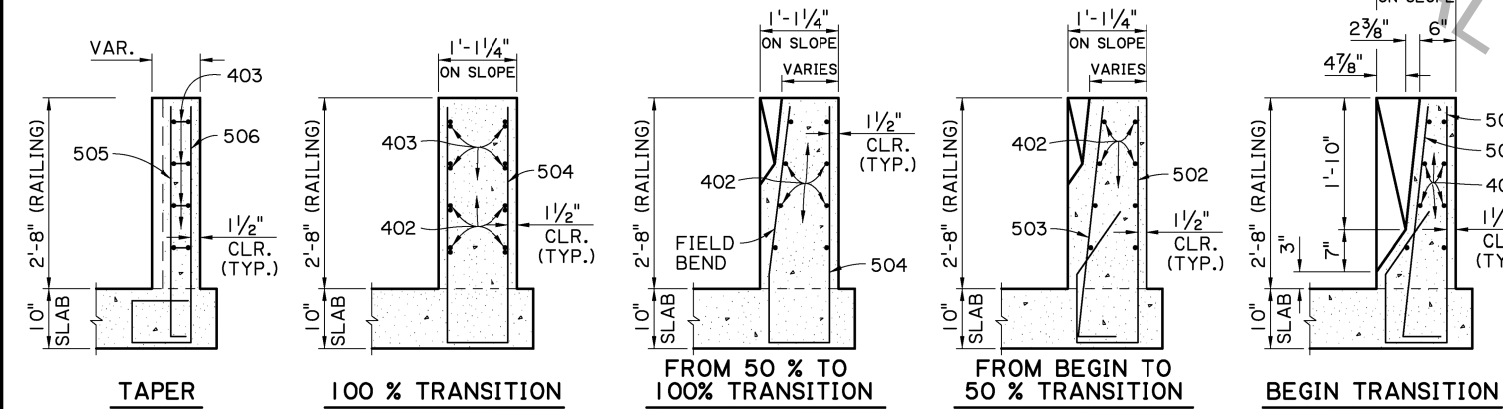
SCALE : $\frac{3}{8}'' = 1'-0''$



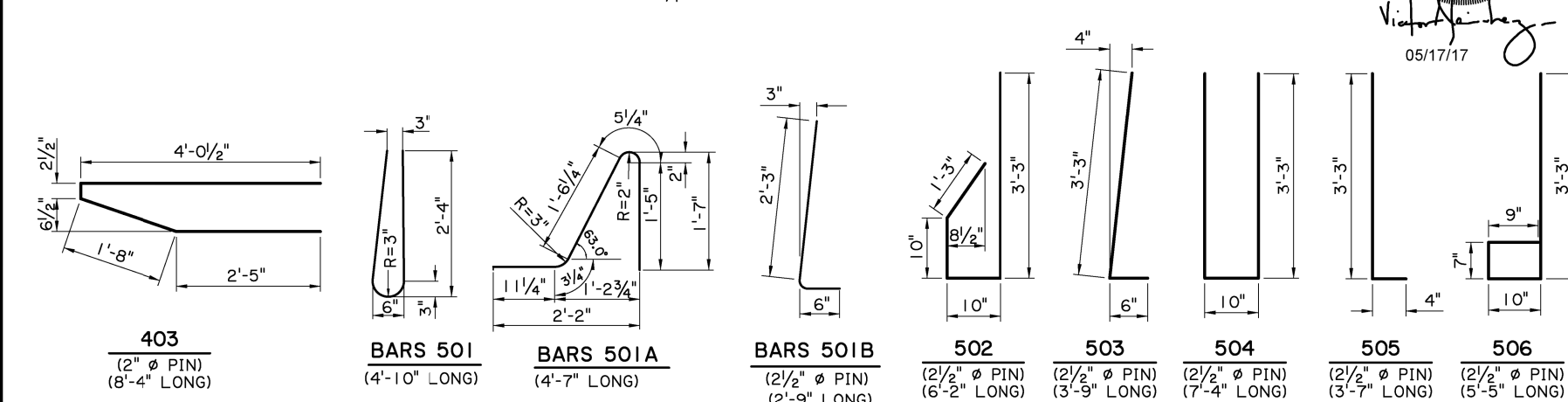
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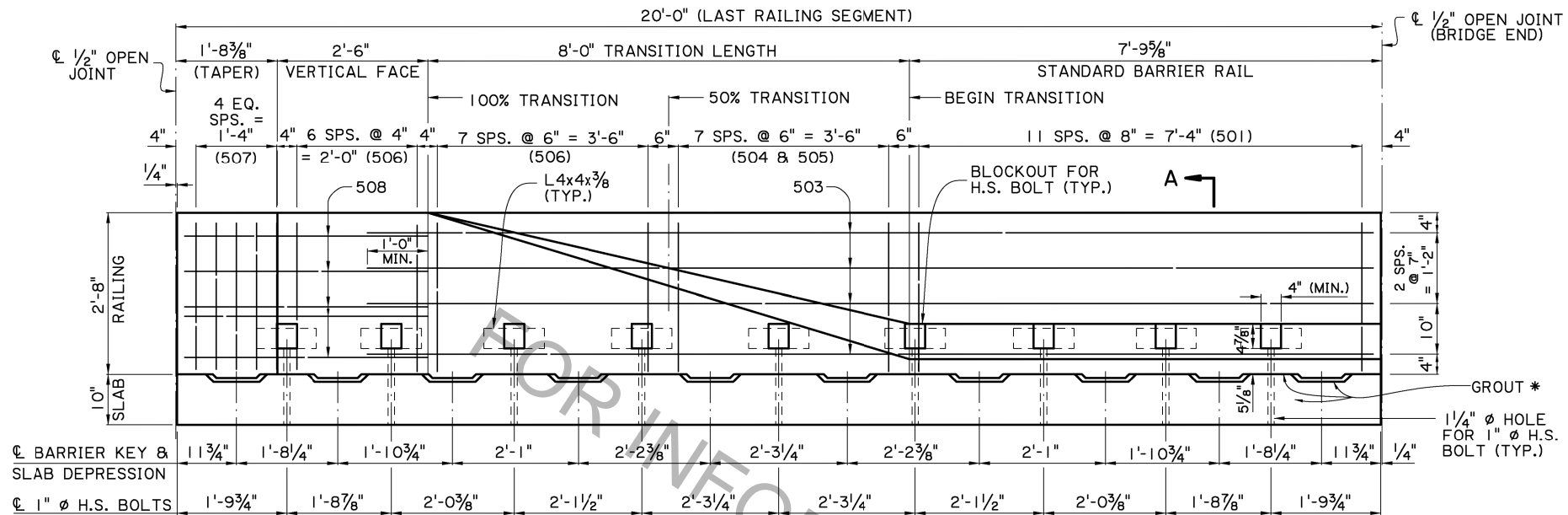


SCALE : $\frac{3}{4}" = 1'-0"$



SCALE : $\frac{3}{4}" = 1'-0"$

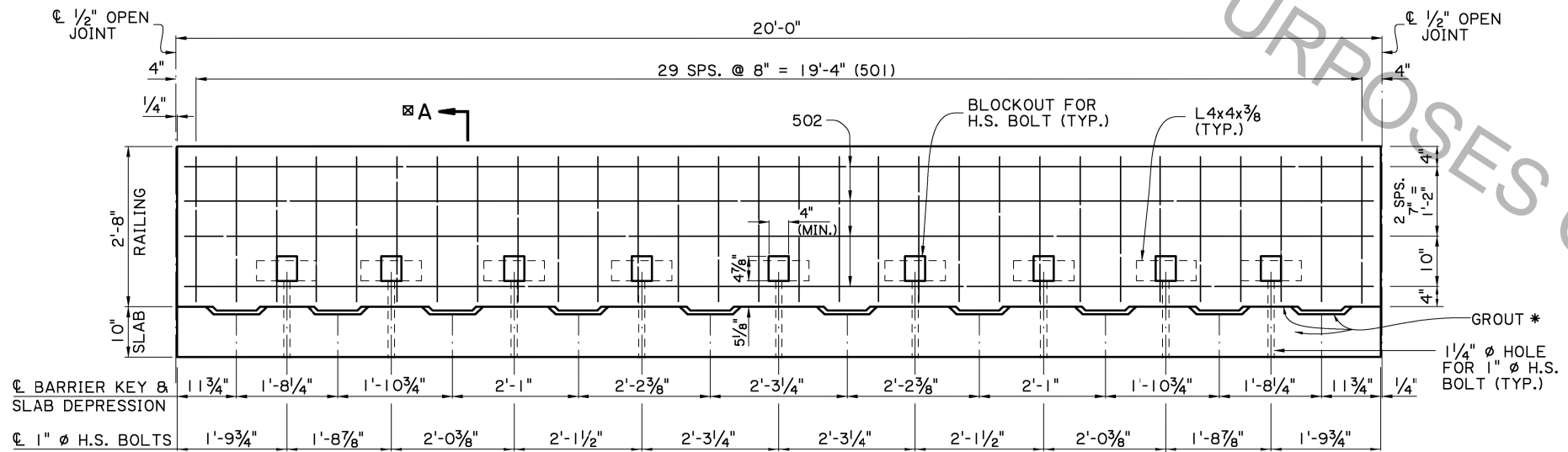




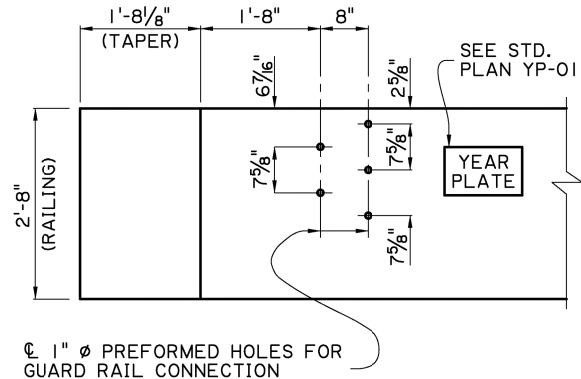
FOR SECTION A-A & TRANSITION SECTIONS
SEE ALTERNATE SPAN (3 OF 4)

PRECAST BARRIER RAILING TRANSITION ELEVATION
(SHOWING BARRIER RAILING AT END OF BRIDGE)
SCALE: $\frac{3}{4}" = 1'-0"$

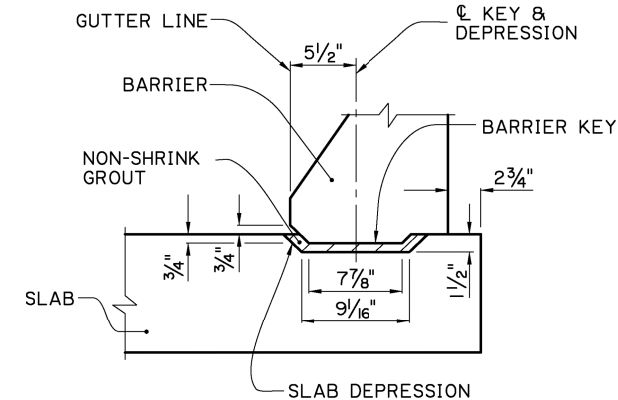
* PLACE OR INJECT NON-SHRINK GROUT AS REQUIRED IN BETWEEN SLAB DEPRESSIONS TO FILL ALL VOIDS AND GAPS FOR FULL EVEN BEARING OF THE BARRIER ON THE SLAB. SEE NOTE 3, SHEET 9 OF 11.



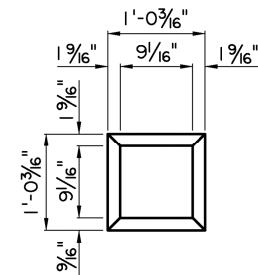
STANDARD PRECAST BARRIER RAILING ELEVATION
(SHOWING BARRIER RAILING ALONG BRIDGE SLAB)
SCALE: $\frac{3}{4}" = 1'-0"$



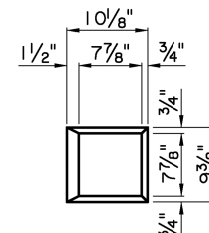
GUARD RAIL CONNECTION DETAIL
(FOR GUARD RAIL DETAILS,
SEE STANDARD PLAN BD.1.1.1.0.01 (GR-200).)
SCALE: $\frac{3}{4}" = 1'-0"$



ELEVATION
SCALE: $1\frac{1}{2}" = 1'-0"$



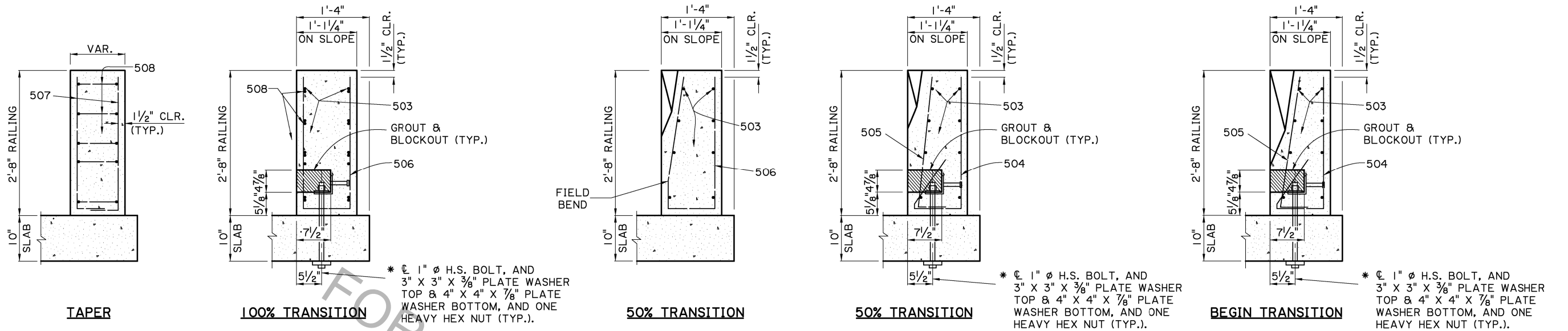
PLAN-DEPRESSION
SCALE: $1" = 1'-0"$



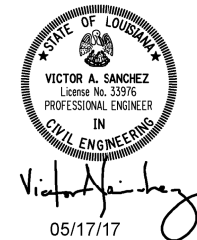
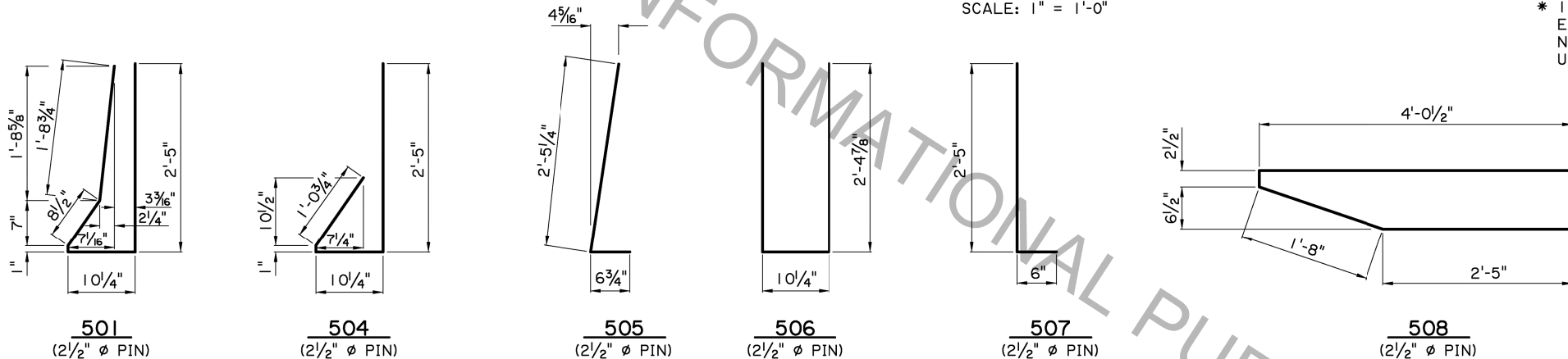
PLAN-KEY
SCALE: $1" = 1'-0"$

**BARRIER KEY AND
PANEL DEPRESSION DETAILS**



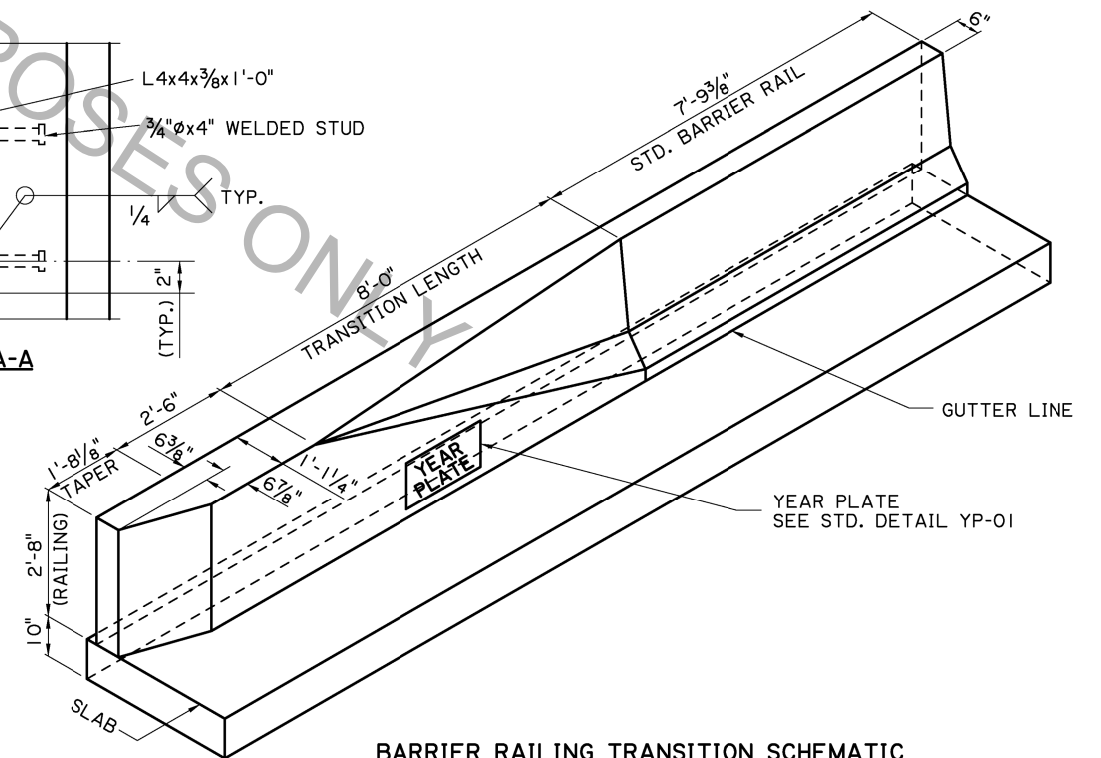
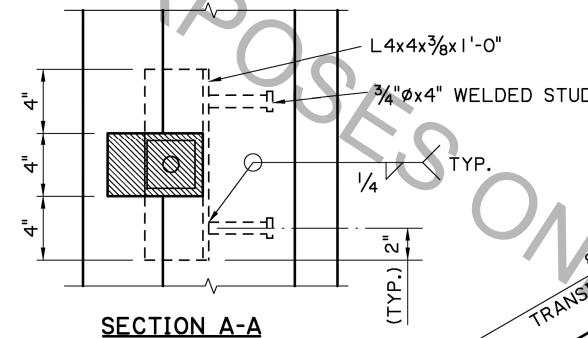
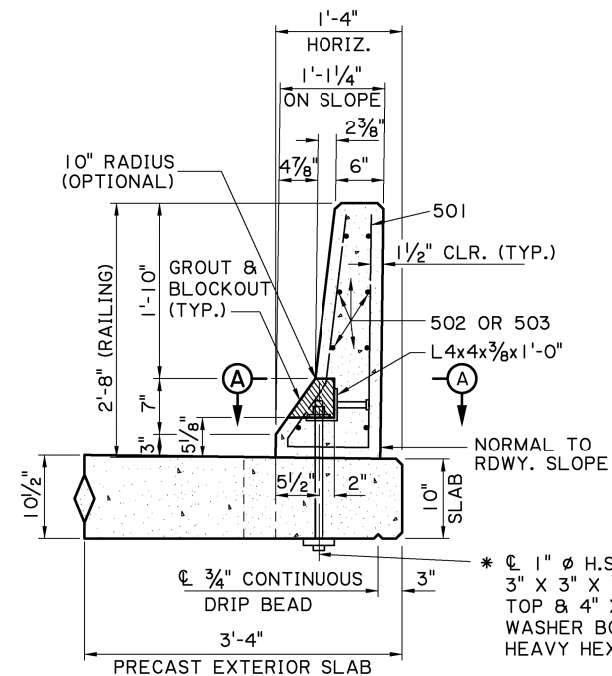


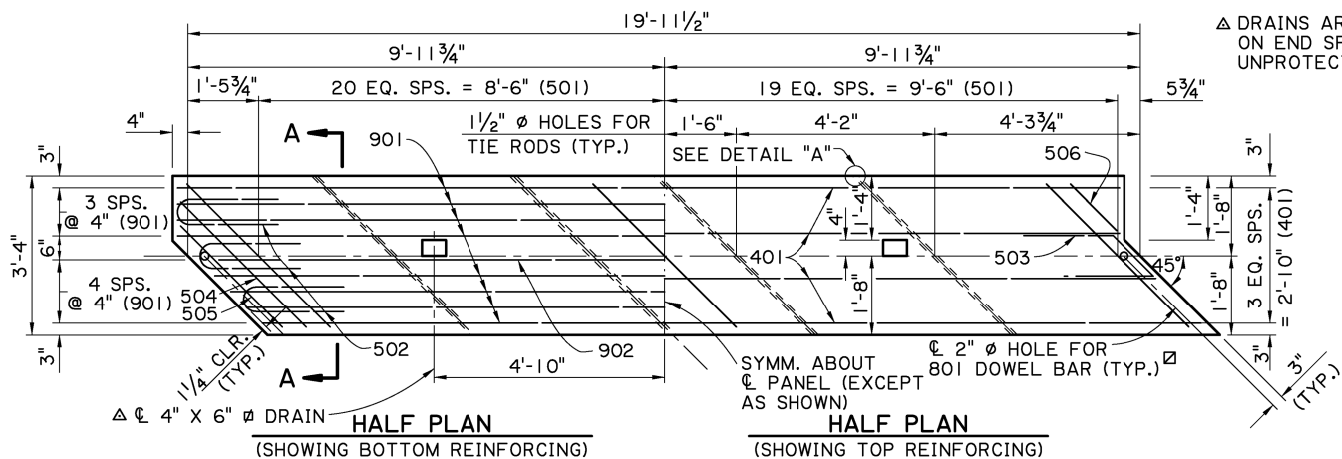
BARRIER RAILING TRANSITION SECTIONS
SCALE: 1" = 1'-0"



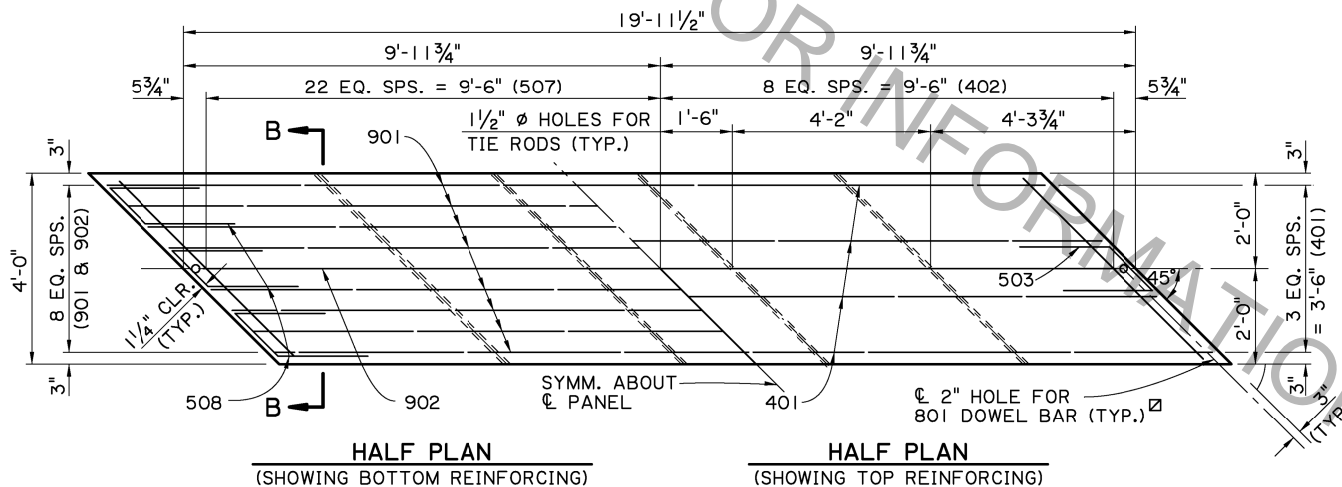
NOTES:

- 1) ALL BARRIER RAIL SURFACES ARE TO RECEIVE A CLASS 3 SPECIAL FINISH.
- 2) ALL SURFACES OF THE BLOCKOUTS EXCEPT THE BOTTOM MAY BE TAPERED AND ALL CORNERS MAY BE ROUNDED TO A RADIUS TO ALLOW FOR EASY REMOVAL OF PLUGS OR FORMS. AFTER PLACING AND TIGHTENING THE ANCHOR BOLTS, THE BLOCKOUTS SHALL BE FILLED WITH AN APPROVED NON-SHRINK GROUT FROM AML AND TROWELED TO THE REQUIRED FINISH AND TO THE SATISFACTION OF THE ENGINEER.
- 3) AFTER BARRIER IS PLACED AND ALIGNED, ALL GAPS UNDER BARRIER AND TOP OF SLAB SHALL BE FILLED WITH NON-SHRINK GROUT FROM AML AND ALLOWED TO SET PRIOR TO TIGHTENING OF BOLTS. IT IS IMPORTANT TO FILL ALL VOIDS AND GAPS UNDER THE BARRIER TO ENSURE EVEN BEARING ON DECK WHEN THE ANCHOR BOLTS ARE LOADED.
- 4) ALL 1" \varnothing BOLTS SHALL BE HIGH STRENGTH A325 OR APPROVED EQUAL. BOLT, NUT & WASHER TO BE GALVANIZED AS PER ASTM A-153. BOLTS SHALL BE TENSIONED TO 36 KIPS, OR APPROXIMATELY 540 FOOT-LB. OF TORQUE (LUBRICATED CONNECTION).

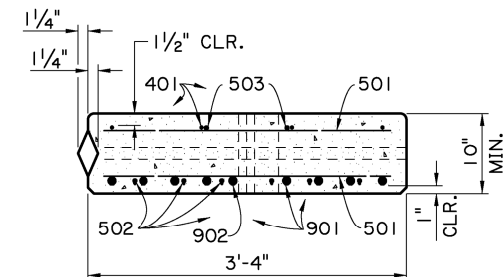




EXTERIOR UNIT
SCALE 1/2" = 1'-0"

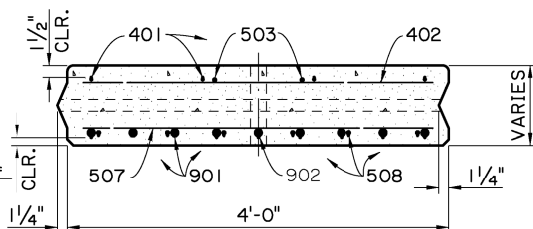


INTERIOR UNIT
SCALE 1/2" = 1'-0"

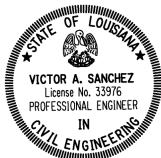


SECTION A-A
EXTERIOR UNIT
SCALE 1" = 1'-0"

NOTE:
FOR EACH SPAN, ONE EXTERIOR
UNIT WILL HAVE A TONGUE AND
ONE WILL HAVE A GROOVE.



SECTION B-B
INTERIOR UNIT
SCALE 1" = 1'-0"



05/17/17

THREE (3) LAYERS OF ASPHALT
SATURATED FELT SHALL BE
PLACED ON TOP OF BENT CAP.
CUT TAR PAPER TO EXPOSE ALL
OBLONG HOLES ON TOP OF CAP
PRIOR TO ERECTING THE SLAB.

2 1/2" X 5" OBLONG HOLES.
HOLES SHALL BE FILLED WITH
GROUT AFTER DOWEL BARS
ARE IN PLACE.

TYPICAL JOINT DETAIL
SCALE : 1" = 1'-0"

AS-DESIGNED RATING		
VEHICLE	RATING FACTOR	NOTES
HL-93 (INV)	1.687	
HL-93 (OPR)	2.187	
LADV-11 (INV)	1.298	MAGNIFICATION FACTOR = 1.3

△ DRAINS ARE NOT REQUIRED
ON END SPANS OVER
UNPROTECTED SLOPES.

ALTERNATE SPAN NOTES:

CONSTRUCTION SPECIFICATIONS : LATEST APPROVED LOUISIANA
STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLE-
MENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS : AASHTO LRFD BRIDGE DESIGN
SPECIFICATIONS, WITH 2008 & 2009 INTERIMS.

DESIGN LOAD : THE BRIDGE DECK IS DESIGNED FOR A FUTURE
WEARING COURSE OF 19 PSF. THE LIVE LOAD IS HL-93, AND
LADV-11 (LOUISIANA DESIGN VEHICLE LIVE LOAD 2011).

STRUCTURAL CONCRETE : ALL CONCRETE SHALL BE CLASS PI.
THE BRIDGE RAIL CONCRETE SHALL BE CLASS AI IF RAIL IS
CAST IN PLACE. STEEL SIDE FORMS AND STEEL OR CONCRETE
BOTTOM FORMS SHALL BE USED FOR PRECAST COMPONENTS.
EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER, UNLESS
OTHERWISE NOTED. ALL SURFACES SHALL RECEIVE A CLASS I
ORDINARY SURFACE FINISH UPON REMOVAL OF THE FORMS.
THE FINAL FINISH SHALL BE A TINE FINISH IN ACCORDANCE
WITH SUB-SECTION 805.08.5.3 OF THE LOUISIANA
STANDARD SPECIFICATIONS.

REINFORCING STEEL : ALL REINFORCING STEEL SHALL BE
GRADE 60. DIMENSIONS RELATING TO FABRICATION ARE OUT
TO OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS
RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE
NOTED. ALL REINFORCING BARS SHALL BE PLACED TO PROVIDE
A MINIMUM COVER OF 1" FROM THE DRAIN HOLES. REINFORCING
STEEL MAY BE TACK WELDED FOR A DISTANCE OF NOT MORE THAN
4'-0" FROM EACH END OF UNIT. NO OTHER WELDING SHALL BE
PERMITTED.

MISCELLANEOUS STEEL : HIGH STRENGTH BOLTS SHALL CONFORM
TO ASTM DESIGNATION A-325. PRESTRESSING STRANDS SHALL
CONFORM TO ASTM DESIGNATION A-416, GRADE 270. PLATES,
TIE RODS, AND DRIFT BOLTS SHALL CONFORM TO ASTM DESIG-
NATION A709, GRADE 36. STEEL SPECIFIED TO BE ZINC
COATED SHALL BE IN CONFORMANCE WITH ASTM DESIGNATION
A-123.

GROUT : THE GROUT SHALL BE AN APPROVED FLOWABLE NON-
SHRINK GROUT LISTED ON AML. THE GROUT SHALL BE TESTED
FOR ACCEPTANCE PRIOR TO USAGE. THE GROUT SHALL ATTAIN
A MINIMUM COMPRESSIVE STRENGTH OF 3500 PSI PRIOR TO
LOADING SLABS. SURFACES SHALL BE THOROUGHLY SATURATED
WITH WATER BY FLOODING THE HOLES FOR APPROXIMATELY
FIVE (5) MINUTES IMMEDIATELY BEFORE THE GROUT IS PLACED.
ONLY POTABLE WATER SHALL BE USED FOR SATURATION AND MIXING
PURPOSES.

PATCHING MATERIAL : THE PATCHING MATERIAL SHALL BE AN
APPROVED PATCHING MATERIAL FOR PRECAST OR PRESTRESSED
CONCRETE PRODUCTS LISTED ON AML. SURFACE PREPARATION,
MIXING AND PLACEMENT SHALL BE IN ACCORDANCE WITH THE MANU-
FACTURERS' RECOMMENDATIONS. ONLY POTABLE WATER SHALL BE
USED FOR SATURATION AND MIXING PURPOSES.

PRECAST UNITS : THE PLANS FOR AN ONGOING OPERATION OF
FABRICATION FACILITIES SHALL BE APPROVED BY THE DEPARTMENT.
EACH UNIT SHALL HAVE "LIVE LOAD HL-93 AND LADV-11", THE FABRI-
CATOR'S MARK, AND UNIQUE NUMBER, MEETING THE APPROVAL OF
THE ENGINEER STAMPED OR INSCRIBED IN THE PLASTIC CONCRETE.
PRECAST UNITS MAY BE CAST WITH OR WITHOUT CAMBER. IF CAMBER
IS PROVIDED IT SHALL NOT EXCEED 1/4" AT THE CENTERLINE OF
SPAN. ALL UNITS SHALL BE HELD AT THE PLANT FOR A MINIMUM
OF TEN(10) DAYS AFTER CASTING. THE CONCRETE SHALL REACH
A MINIMUM STRENGTH OF 3,000 PSI BEFORE HANDLING IS PERMITTED.
THE LIFTING INSERTS SHALL BE 1", TYPE S INSERTS AS MANU-
FACTURED BY DAYTON-SUPERIOR CORPORATION OR AN APPROVED
EQUAL. EACH INSERT SHALL HAVE A MINIMUM LOAD CAPACITY OF
10,000 POUNDS. FOUR(4) INSERTS WITH 1" X 5" LONG COIL BOLTS
SHALL BE PLACED IN THE TOP OF THE UNIT AND LOCATED 1'-3"
FROM ITS ENDS AND 1'-0" FROM ITS EDGES. INSERT HOLES SHALL
BE GROUT FILLED AFTER PLACEMENT OF UNIT. AT THE CONTRAC-
TOR'S OPTION A SLING OF SUFFICIENT CAPACITY MAY BE USED FOR
LIFTING, PROVIDED THE SAME PICKUP LOCATION FROM THE ENDS ARE
USED. FABRICATION TOLERANCES SHALL BE AS FOLLOWS:

UNIT DEPTH ± 3/16"
UNIT LENGTH + 1/8" AND -1/2"
OVERALL SPAN WIDTH ± 2"

ALL PRECAST UNITS IN EACH BRIDGE SPAN SHALL BE MATCH CAST
IN THE SAME CASTING BED TO ENSURE A PROPER FIT DURING
INSTALLATION.

GUARDRAIL : REFER TO GENERAL PLAN FOR GUARDRAIL REQUIRE-
MENTS. PROVIDE HOLES FOR GUARDRAIL CONNECTIONS ACCORDING
TO STANDARD PLAN BD.1.1.1.0.01 (GR-200) ON ALL FOUR(4) BRIDGE ENDS.

BASIS OF PAYMENT : ALL MATERIALS SHALL BE PAID FOR UNDER
"BRIDGE SUPERSTRUCTURE AND SUBSTRUCTURE" ACCORDING TO
THE SPECIFICATIONS.

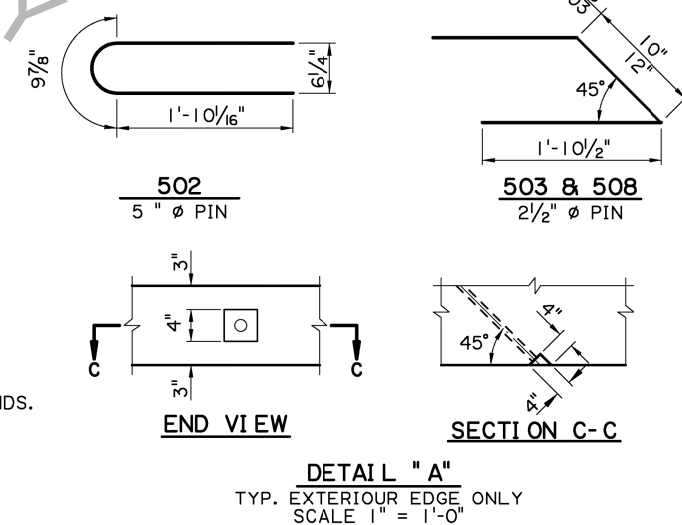
ESTIMATED QUANTITIES (ONE EXTERIOR UNIT)

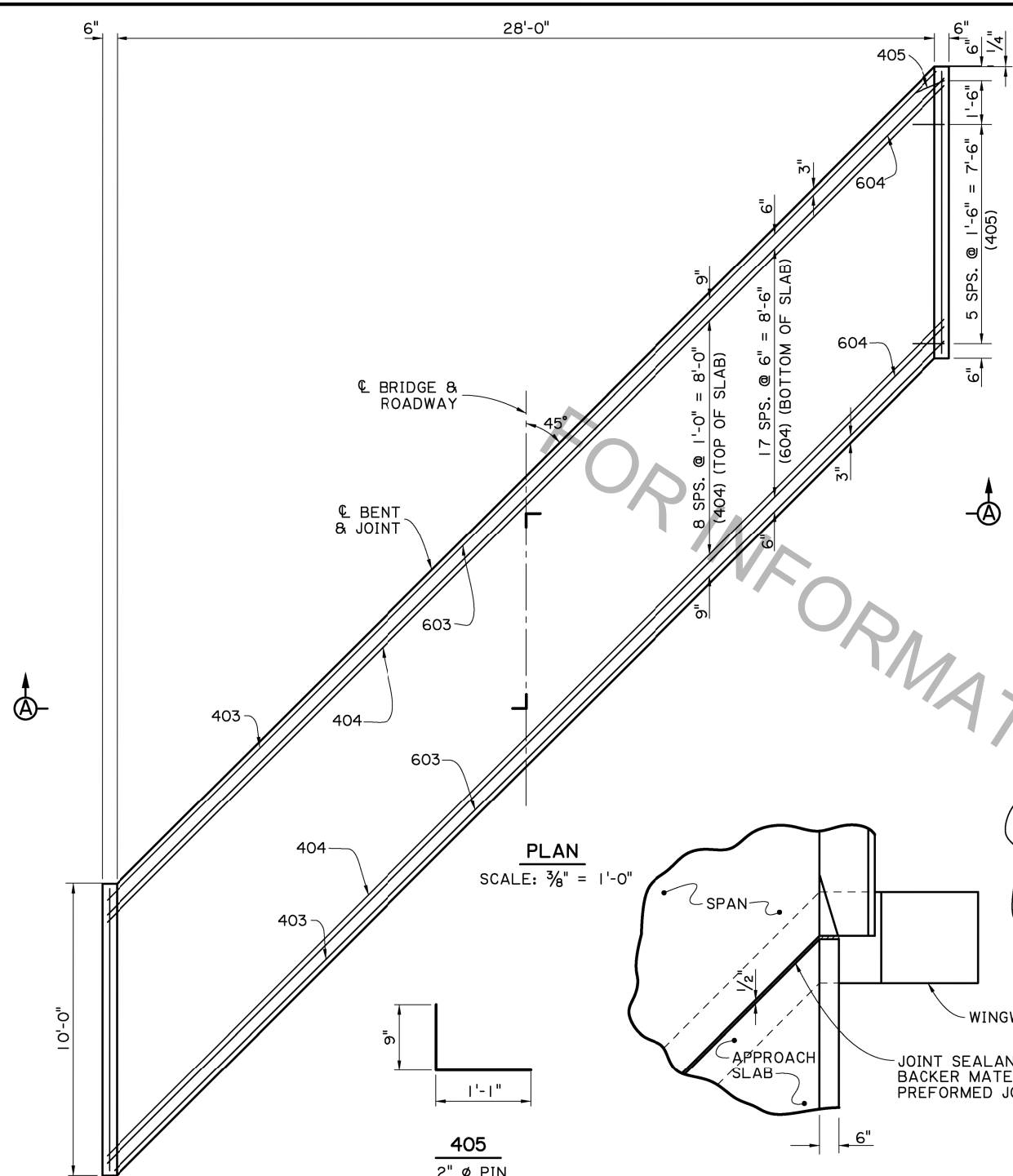
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
901	8	19'-7"	156'-8"	LONGIT. BOT. OF SLAB
902	1	18'-11"	18'-11"	LONGIT. BOT. OF SLAB
TOTAL NO. 9 BARS = 175'-7" = 597 LBS.				
801	1	1'-0"	1'-0"	DOWELS
TOTAL NO. 8 BARS = 1'-0" = 3 LBS.				
501	80	4'-2"	333'-4"	TRANS. TOP & BOT. OF SLAB
502	6	4'-6"	27'-0"	TOP & BOT. END OF SLAB
503	2	4'-9"	9'-6"	TOP & BOT. END OF SLAB
504	2	3'-9"	7'-6"	TOP & BOT. END OF SLAB
505	2	3'-0"	6'-0"	TOP & BOT. END OF SLAB
506	2	1'-4"	2'-8"	TOP & BOT. END OF SLAB
TOTAL NO. 5 BARS = 386'-0" = 403 LBS.				
401	4	19'-7"	78'-4"	LONGIT. TOP OF SLAB
TOTAL NO. 4 BARS = 78'-4" = 52 LBS.				
DEFORMED REINFORCING STEEL = 1055 LBS.				
CLASS PI CONCRETE = 2.05 CU. YDS.				
CONCRETE RAILING (PER SPAN) = 40.00 LIN. FT.				

ESTIMATED QUANTITIES (ONE INTERIOR UNIT)

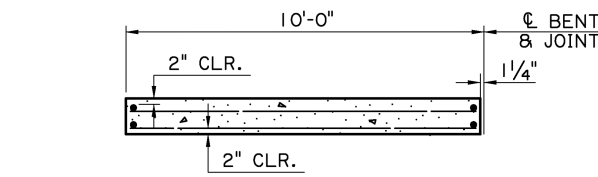
BAR	NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION
901	8	19'-7"	156'-8"	LONGIT. BOT. OF SLAB
902	1	18'-11"	18'-11"	LONGIT. BOT. OF SLAB
TOTAL NO. 9 BARS = 175'-7" = 597 LBS.				
801	1	1'-0"	1'-0"	DOWELS
TOTAL NO. 8 BARS = 1'-0" = 3 LBS.				
503	2	4'-9"	9'-6"	TOP END OF SLAB
507	45	5'-2"	232'-6"	TRANS. BOT. OF SLAB
508	6	4'-7"	27'-6"	BOT. END OF SLAB
TOTAL NO. 5 BARS = 269'-6" = 282 LBS.				
401	4	19'-7"	78'-4"	LONGIT. TOP OF SLAB
402	17	4'-2"	70'-10"	TRANS. TOP OF SLAB
TOTAL NO. 4 BARS = 149'-2" = 100 LBS.				
DEFORMED REINFORCING STEEL = 982 LBS.				
CLASS PI CONCRETE = 2.46 CU. YDS.				

○ BASED ON A 10" SLAB THICKNESS

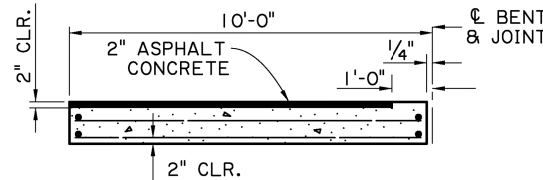




PLAN
SCALE: 3/8" = 1'-0"

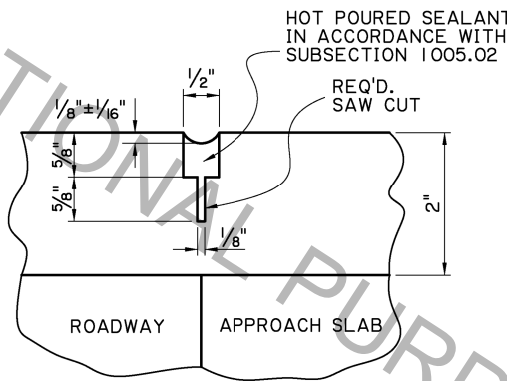


(FOR PORTLAND CEMENT CONCRETE ROADWAY PAVEMENT)

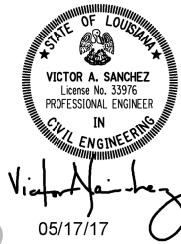


(FOR ASPHALT CONCRETE ROADWAY PAVEMENT)

SECTION ALONG C ROADWAY
SCALE: 1/4" = 1'-0"



SAWING & SEALING
JOINT DETAIL
N.T.S.

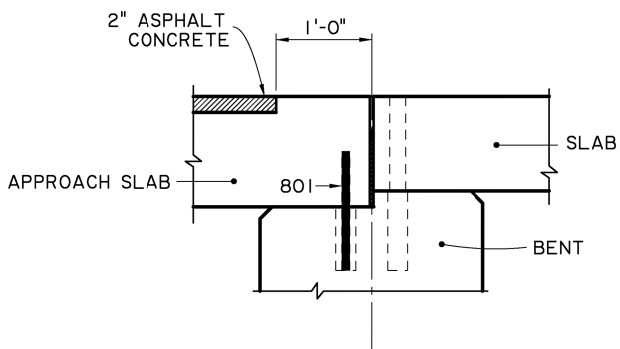


APPROACH SLAB NOTES:

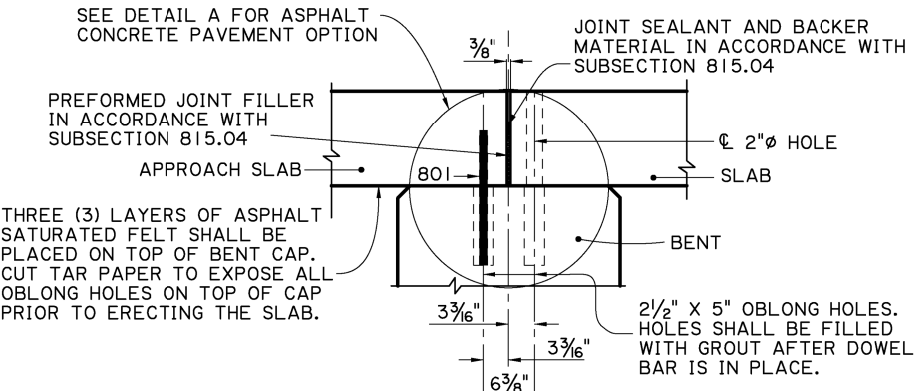
CONSTRUCTION SPECIFICATIONS: LATEST APPROVED LOUISIANA STANDARD SPECIFICATIONS FOR ROADS AND BRIDGES, SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.
DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 4th EDITION, WITH 2008 & 2009 INTERIMS.
STRUCTURAL CONCRETE: ALL CONCRETE SHALL BE CLASS A1. EXPOSED EDGES SHALL HAVE A 3/4" CHAMFER, UNLESS OTHERWISE NOTED.
REINFORCING STEEL: ALL REINFORCING STEEL SHALL BE GRADE 60. DIMENSIONS RELATING TO THE FABRICATION ARE OUT-TO-OUT OF BARS, UNLESS OTHERWISE NOTED. DIMENSIONS RELATING TO SPACING ARE TO BAR CENTERS, UNLESS OTHERWISE NOTED.
BEDDING MATERIAL: FOR DETAILS OF BEDDING MATERIAL AND UNDERDRAINS, SEE STANDARD DETAIL BD.2.10.1.0.07.
SAWING & SEALING: THE ASPHALT CONCRETE SHALL BE SAW CUT AT THE END OF THE CONCRETE APPROACH SLAB THE ENTIRE ROADWAY WIDTH AND SEALED, COST TO BE INCLUDED WITH CONCRETE APPROACH SLABS.
BASIS OF PAYMENT: ALL MATERIAL SHALL BE PAID FOR UNDER 'CONCRETE APPROACH SLABS' ACCORDING TO THE SPECIFICATIONS.

ESTIMATED QUANTITIES (ONE SLAB)				
BAR NO.	UNIT LENGTH	TOTAL LENGTH	LOCATION	
801	8	1'-0"	8'-0"	DOWELS
TOTAL NO. 8 BARS = 8'-0" = 21 LBS.				
601	2	9'-7"	19'-2"	LONGIT. BOT. OF SLAB
602	56	9'-6"	532'-0"	LONGIT. BOT. OF SLAB
603	2	40'-1"	80'-2"	TRANSV. BOT. OF SLAB
604	18	40'-6"	729'-0"	TRANSV. BOT. OF SLAB
TOTAL NO. 6 BARS = 1,360'-4" = 2,043 LBS.				
401	4	9'-7"	38'-4"	LONGIT. TOP OF SLAB & CURB
402	28	9'-6"	266'-0"	LONGIT. TOP OF SLAB
* 403	2	41'-9"	83'-6"	TRANSV. TOP OF SLAB
* 404	9	42'-2"	379'-6"	TRANSV. TOP OF SLAB
405	14	1'-10"	25'-8"	DOWELS IN CURB
TOTAL NO. 4 BARS = 793'-0" = 530 LBS.				
TOTAL DEFORMED REINFORCING STEEL = 2,594 LBS.				
CONCRETE APPROACH SLAB = 32.22 SQ. YDS.				
ASPHALT CONCRETE = 3.0 TONS				
SAW CUT & SEAL = 38 LIN. FT.				

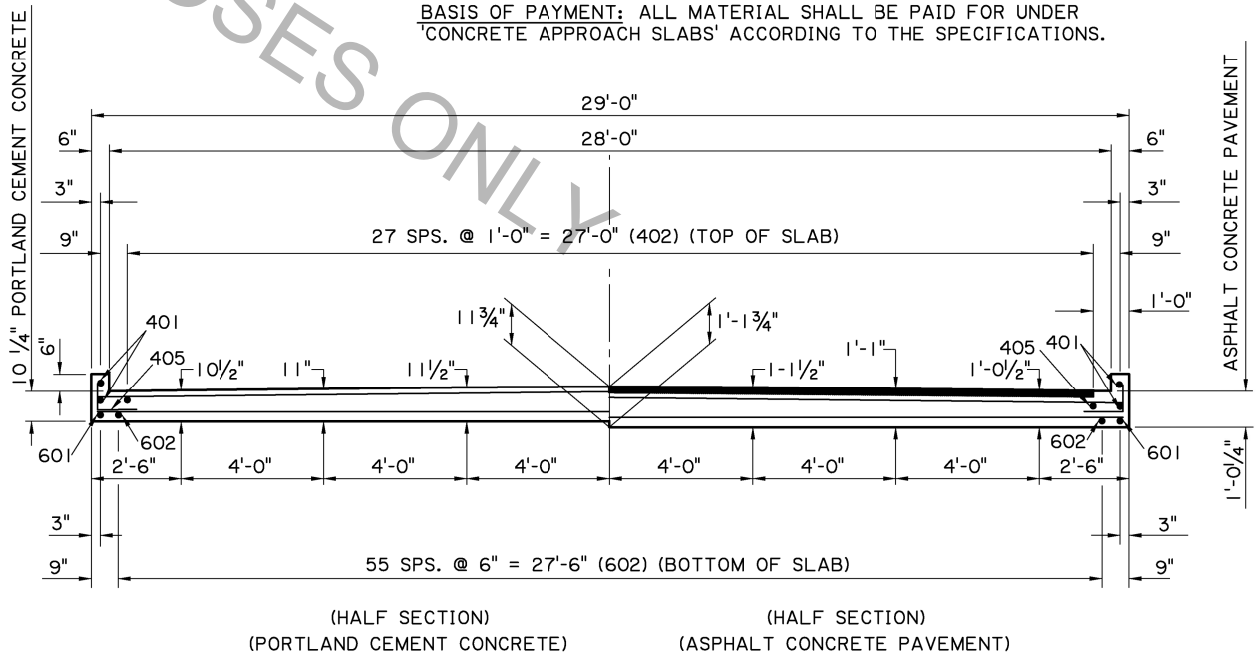
- TO BE PAID FOR UNDER ITEM CONCRETE APPROACH SLABS.
- * INCLUDES ONE (1) 1'-8" MINIMUM LAP SPLICE. ALL LAP SPLICES ARE TO BE STAGGERED.
- REQUIRED WHEN APPROACH SLAB IS ADJACENT TO ASPHALT CONCRETE PAVEMENT.



DETAIL A
SCALE: 1" = 1'-0"
(ASPHALT CONCRETE PAVEMENT OPTION)



TYPICAL JOINT DETAIL
SCALE: 1" = 1'-0"



(HALF SECTION) (PORTLAND CEMENT CONCRETE) (HALF SECTION) (ASPHALT CONCRETE PAVEMENT)

SECTION A-A
SCALE: 3/8" = 1'-0"

DESIGNED BY: B. DELATTE
CHECKED BY: J. NAKHLEH
DATE: 05/17/17

PARISH: B. DELATTE
CONTROL SECTION: J. NAKHLEH
REVIEWED BY: J. NAKHLEH
DATE: 05/17/17

STATE: LA
PROJECT: 45-28-20SL
SERIES: 13 OF 13

ALTERNATE APPROACH SLAB
10'-0" CAST-IN-PLACE APPROACH SLAB
28'-0" CLEAR ROADWAY
45° CROSSING TWO WAY TANGENT

DOTD
DOTD BRIDGE DESIGN